

PLANNING COMMISSION AGENDA

CHAIRPERSON:

Liz Wynn



VICE CHAIRPERSON:

Chris Gomez

COMMISSIONERS: Liz Wynn, Chris Gomez, Brett Taylor, Marvin Hansen, Sarrah Peariso

TUESDAY NOVEMBER 12, 2019; 7:00 P.M., COUNCIL CHAMBERS, 707 W. ACEQUIA, VISALIA CA

1. THE PLEDGE OF ALLEGIANCE –
2. CITIZEN'S COMMENTS – This is the time for citizens to comment on subject matters that are not on the agenda but are within the jurisdiction of the Visalia Planning Commission. The Commission requests that a 5-minute time limit be observed for comments. Please begin your comments by stating and spelling your name and city. Please note that issues raised under Citizen's Comments are informational only and the Commission will not take action at this time.
3. CHANGES OR COMMENTS TO THE AGENDA –
4. CONSENT CALENDAR - All items under the consent calendar are to be considered routine and will be enacted by one motion. For any discussion of an item on the consent calendar, it will be removed at the request of the Commission and made a part of the regular agenda.
5. PUBLIC HEARING – Paul Scheibel
Conditional Use Permit No. 2019-38: A request by Rise Church, to add 58 parking spaces to an existing 60-space parking area within an existing church facility located in the R-1-20 (Single-family Residential 20,000 sq. ft. minimum lot size) at 5702 West Caldwell Avenue (APN: 119-690-049). The project is Categorically Exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15301, Categorical Exemption No. 2019-71.
6. PUBLIC HEARING – Josh Dan
Conditional Use Permit 2019-36: A request by Donahue Schriber Realty Group to develop three pad sites with fast food dining, retail shops, on-site parking and drive-thrus across 3.72 acres of the Orchard Walk West development area. The site is zoned C-MU (Mixed Use Commercial Zone) and located at the northwest corner of north Dinuba Blvd and west Riggan Ave. (APN: 078-120-030 & 078-120-032). The project is Categorically Exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15303, Categorical Exemption No. 2019-70.

7. PUBLIC HEARING – Brandon Smith

- Conditional Use Permit No. 2018-03: A request by Omni Land Development LLC / Daygo Properties LLC to allow a master-planned commercial development over five parcels on 5.88 undeveloped acres, consisting of approximately 56,395 sq. ft. of commercial and office uses in the Mixed Use Commercial (CMU) Zone. The project site is located on the northeast corner of Dinuba Boulevard (State Route 63) and Shannon Parkway. (APN: 079-071-029) A Mitigated Negative Declaration (MND No. 2018-12) has been prepared for the project.
- Tentative Parcel Map No. 2018-01: A request by Omni Land Development LLC / Daygo Properties LLC to subdivide a 5.88-acre parcel into five parcels to facilitate commercial development on land in the Mixed Use Commercial (CMU) Zone. The project site is located on the northeast corner of Dinuba Boulevard (State Route 63) and Shannon Parkway. (APN: 079-071-029) A Mitigated Negative Declaration (MND No. 2018-12) has been prepared for the project.

8. PUBLIC HEARING – Brandon Smith

Mid-cycle Update to 5th Cycle General Plan Housing Element (2020-2023). A request by the City of Visalia to adopt the mid-cycle update to the 5th Cycle General Plan Housing Element in accordance with State Government Code Section 65580, et seq. The update is a comprehensive review and select update of the 5th Cycle Housing Element's background information report, goals, policies, and implementation programs. The mid-cycle update will serve a four-year planning period from December 31, 2019 to December 31, 2023. The project area is contained within the City of Visalia's Urban Development Boundaries that are illustrated in the Visalia General Plan. An Initial Study was prepared for this project, consistent with CEQA, which disclosed that environmental impacts are determined to be not significant with mitigation and that Negative Declaration No. 2019-63 was adopted.

9. DIRECTOR'S REPORT/ PLANNING COMMISSION DISCUSSION-

- Planning Commission Meeting November 25, 2019

The Planning Commission meeting may end no later than 11:00 P.M. Any unfinished business may be continued to a future date and time to be determined by the Commission at this meeting. The Planning Commission routinely visits the project sites listed on the agenda.

For Hearing Impaired – Call (559) 713-4900 (TTY) 48-hours in advance of the scheduled meeting time to request signing services.

Any written materials relating to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the City Office, 315 E. Acequia Visalia, CA 93291, during normal business hours.

APPEAL PROCEDURE

THE LAST DAY TO FILE AN APPEAL IS FRIDAY, NOVEMBER 22, 2019 BEFORE 5 PM

According to the City of Visalia Zoning Ordinance Section 17.02.145 and Subdivision Ordinance Section 16.04.040, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal form with applicable fees shall be filed with the City Clerk at 220 N. Santa Fe, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.visalia.city or from the City Clerk.

THE NEXT REGULAR MEETING WILL BE HELD ON MONDAY, DECEMBER 9, 2019



REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE: November 12, 2019

PROJECT PLANNER: Josh Dan, Associate Planner
Phone No.: (559) 713-4003
E-mail: josh.dan@visalia.city

SUBJECT: Conditional Use Permit 2019-36: A request by Donahue Schriber Realty Group to develop three pad sites with fast food dining, retail shops, on-site parking and drive-thrus across 3.72 acres of the Orchard Walk West development area. The site is zoned C-MU (Mixed Use Commercial Zone) and located at the northwest corner of north Dinuba Blvd and west Riggan Ave. (APN: 078-120-030 & 078-120-032).

STAFF RECOMMENDATION

Staff recommends approval of Conditional Use Permit No. 2019-36 based upon the project's consistency with the Orchard Walk Specific Plan, the General Plan, and the Zoning Ordinance, by adopting the findings and conditions in Resolution No. 2019-66.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No. 2019-36 based on the findings and conditions in Resolution No. 2019-66.

PROJECT DESCRIPTION

The Orchard Walk Specific Plan is comprised of two large commercial components located on the east and west sides of Dinuba Blvd. north of Riggan Ave. The initial commercial development was built on the northeast corner of Dinuba Blvd. and Riggan Ave. Both commercial components include out-pad buildings that are required to comply with the master site plan placement, size, and architectural theme approved with the Specific Plan. The applicant has submitted this Conditional Use Permit (CUP) proposing to develop the west side of the project area by requesting the following:

- A. Amending CUP No. 2007-13 of the Orchard Walk Specific Plan to rearrange building placement in Phase One;
- B. CUP, as required by the Visalia Municipal Code (VMC), for Building Pad No. 2 and Building Pad No. 3 due to the drive-thru lanes proximity to residential development (i.e., within 250-feet); and,
- C. Amendment to building architectural requirements and wall signage for In-N-Out Burger development.

The applicant is requesting to amend CUP No. 2007-13 which approved a home improvement store (Home Depot) for Phase One of the Orchard Walk West development area. Phase One is now proposing to develop 3.72 acres of the overall 16.5 acre master plan development. Phase One is located on the southeast corner of the Orchard Walk West development and is proposed to include three out-pad sites with fast food dining, retail shops, on-site parking, and drive-thru lanes (see Exhibit "A"). This design differs from the previously approved Specific Plan showing

two retail buildings, without drive-thrus, on the southeast corner of the project site area (See Exhibit “H”).

This CUP is necessary as the drive-thru lanes for Pads No. 2 and No. 3 do not meet the separation from residences requirement of Section 17.32.162.B.1 “Drive-thru lanes Performance Standards” of the Municipal Code, which requires a minimum 250-foot separation from property planned and/or zoned residential.

In addition, the Specific Plan was approved with rustic, bucolic architectural requirements for tenant buildings. Building Pad No. 2 will consist of a stand-alone fast food tenant with a drive-thru lane for an In-N-Out Burger restaurant. The In-N-Out architect, with the owner’s support, has submitted building elevations that are consistent with In-N-Out’s corporate building design found in all of their buildings. Although the building elevations for In-N-Out Burger do not meet the architectural requirements of the Orchard Walk Specific Plan, the In-N-Out Burger site will incorporate the amenities adopted for the specific plan including the wood split-rail fencing, bucolic landscaping, trellis covers, etc., as required by the Specific Plan. Staff has reviewed the applicant’s request and recommends that the changes can be supported subject to the development complying with all other requirements of the Orchard Walk Specific Plan.

BACKGROUND INFORMATION

General Plan Land Use Designation:	Commercial Mixed Use	
Zoning:	C-MU (Commercial Mixed Use)	
Surrounding Zoning and Land Use:	North:	C-MU (Commercial Mixed Use), R-M-3 (Multi-Family Residential) - Vacant
	South:	Riggin Avenue, R-1-5 (Single Family Residential) – existing Fairview Village Subdivision
	East:	Dinuba Blvd., C-MU (Commercial Mixed Use) – Orchard Walk East
	West:	North Conyer Street, R-M-2 (Multi-Family Residential) – Vacant
Environmental Review:	Categorical Exemption No. 2019-70	
Special Districts:	Orchard Walk Specific Plan	
Site Plan:	Site Plan Review: 2019-076	
	Parcel Map: 2007-05 (West)	
	CUPs: 2007-13, 2007-14, 2007-58, 2007-59	

RELATED PLANS AND POLICIES

All related plans and policies are reprinted in the attachment to this staff report entitled “Related Plans and Policies”.

RELATED PROJECTS

The subject site is part of a larger shopping center with a Target store and other major tenants as the main anchors and added retail space and related out-pads for retail and food services. The development plan for the shopping center was approved under the Orchard Walk Specific Plan, which was reviewed by the Planning Commission on May 29, 2007 and adopted by the City Council on June 18, 2007.

Two Conditional Use Permits on the May 29, 2007 Planning Commission agenda were approved for the development of the first phase of each of the commercial centers with a Home Depot home improvement store and a Target retail store as the anchor tenants (CUPs 2007-13 and 2007-14, respectively). However, the Home Depot project was never developed.

Two other Conditional Use Permits No. 2007-58 and 2007-59 on the December 10, 2007 Planning Commission agenda were approved for a McDonalds and Starbucks, both with drive-thrus, located in the Orchard Walk East development area.

PROJECT EVALUATION

Staff recommends approval of the requested Conditional Use Permit based on project consistency with the Orchard Walk Specific Plan, the General Plan and the Zoning Ordinance.

Consistency with Adopted Plans

The project is consistent with the Orchard Walk Specific Plan, the City's General Plan, and Zoning Ordinance. The Specific Plan provides a number of policies regarding the development of this area. These plans and policies cover issues including streets, pedestrian pathways, infrastructure, aesthetics, setbacks, and parking. Staff has reviewed the policies in the Specific Plan and concludes that the proposed site configuration and elevations are consistent with the applicable policies in the Specific Plan.

Staff further concludes the proposed drive-thru lanes are consistent with the existing and future commercial land uses along Dinuba Blvd. and Riggan Ave. Both of these roadways are major roadways and the future development of this commercial property will provide additional commercial shopping opportunities to the surrounding residential neighborhoods and regional sports park. With the development of this commercial node, and the installation of frontage improvements, the catalyst for the remaining balance of the site to be developed could be initiated by this new construction.

Access and Circulation

During the Site Plan Review process, the applicant was informed that improvements done for the Orchard Walk West commercial development has off-site improvement permits which have not received a final or have since expired, and therefore could not be accepted by the City. In addition, all installed public infrastructure and hardscape improvements that were installed, will need to be repaired, re-tested, reconditioned, etc. Coordination with the City Engineer is required to have the public right-of-way improvements complete and accepted by the City prior to any permit being "finalized" for any portion of the Phase One development.

Conversely, Sedona and Conyer are local streets that are currently fenced-off / barricaded due to the previously unfinished development. Completion of the outstanding improvements for these two local streets is required based on the use of Sedona Ave. to provide access to the commercial development from the residential neighborhoods to the west and the sports park to the north. Local street improvements include the construction of curb returns at the southeast and northeast corner of Sedona and Conyer. Coordination with the City Engineer is required to establish an acceptable plan prior to Phase One being complete.

The requirement to have the off-site and local street improvements completed and accepted by the City Engineer is included as Condition No. 8 of this CUP.

Public Frontage Improvements

Frontage improvements for this project include construction of sidewalks, planting of street trees, construction of median island along Riggan Ave., street lights, and curb returns. In

addition, the developer is required to construct a bus turn-out just west of the Dinuba/Riggin intersection. The applicant is also responsible for replanting any vegetation including street trees along Dinuba Blvd. that are dead as described in Condition No. 5.

Out-Pad Tenants

Pad No. 1 is shown on the northeast side of Phase One, along Dinuba Blvd. Pad No. 1 will be developed with a one-story retail / fast food building, with drive-thru, consisting of a total building area of 7,800 square-feet. There are currently no tenants identified at this time.

Pad No. 2 is the proposed In-N-Out Burger restaurant (with drive-thru) shown in Exhibit "B". The fast food business is proposed to be a sole occupant of a 3,867 square-foot building at the southeast corner of Phase One.

Pad No. 3 is shown on the south side of Phase One, along Riggin Ave. Pad No. 3 will be developed with a one-story retail / fast food building, with drive-thru, consisting of a total building area of 6,440 square-feet. There are currently no tenants identified at this time.

The buildings will be buffered with landscaping along both street frontages and from the parking and drive-thru areas (refer to Landscape Plan, Exhibit "C").

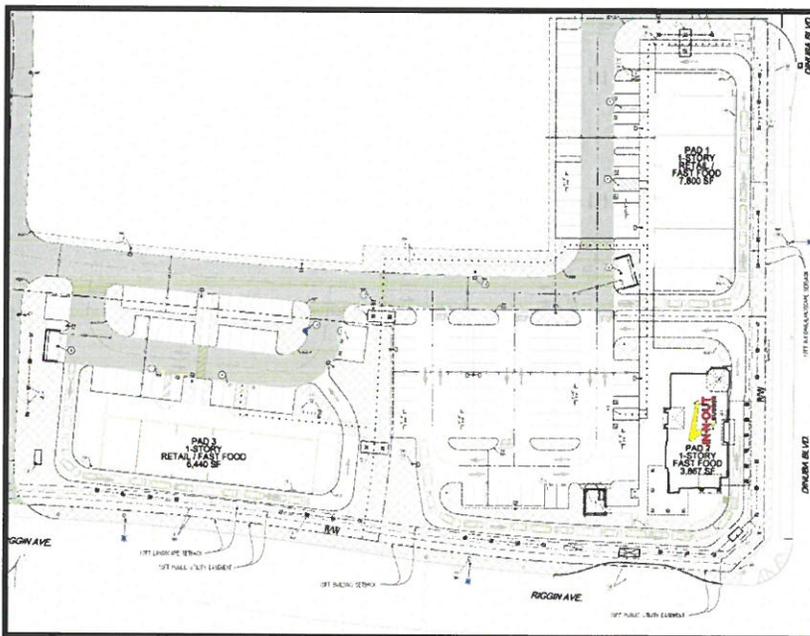
General vehicular access will be provided from Dinuba Blvd., Riggin Ave. via a private drive (see Exhibit "E"). Access located on Riggin Ave.

will have restricted three-quarter access with no "Left Out" allowed. This three-quarter median opening will require adjustment of the left turn pocket to align with the new driveway location. An unnamed private road is proposed to be developed as a part of Phase One and will provide connection from Riggin Ave. to Sedona Ave (see Exhibit "E").

Anticipated hours of operation for In-N-Out Burger will be consistent with the other In-N-Out drive-thru operation in Visalia, which is seven days a week from 10:30 a.m. to 1:00 a.m. Sunday through Wednesday and 10:30 a.m. to 1:30 a.m. on Friday and Saturday. Anticipated hours of operation for the other drive-thrus and retail shops have not yet been described as there are no other tenants listed. All drive-thru lanes are required to comply with the City of Visalia Noise Ordinance as described in Condition No. 6.

Trip Generation

The applicant has provided a trip generation analysis. The analysis was prepared by QK and is provided as an attachment to the report (see Exhibit "F"). The trip generation analyzed the estimated vehicular trips generated for the proposed project and compared it with the original development proposal in 2007 to determine if there was a need for a new traffic impact study (TIS) or any additional traffic mitigation measures for the site at this time. Based on the Phase One area of development, and uses proposed, the consultant found that the trip generation was comparable and would not warrant an additional TIS or any additional mitigation measures for Phase One of this development. The City's Traffic Engineer reviewed the analysis prepared by QK and agreed with their assessment that the trip generation for the revised commercial



development is consistent with the trip generation analyzed in the previous TIS for the entire Orchard Walk development.

In-N-Out Elevations

The Pad No. 2 elevation plans submitted for the In-N-Out Burger are not consistent with the architecture design guidelines written in the Specific Plan, which require the building facades to retain a rustic and pastoral theme. The applicant is therefore proposing an amendment to the Specific Plan's architecture standards for the In-N-Out Burger restaurant that would give them flexibility to allow for this building pad to be developed with their prototype building design that has strict corporate building elevation and material requirements for their In-N-Out Burger restaurants buildings. In-N-Out buildings are required to maintain their form of architecture that is found in every In-N-Out building located throughout the State.

Based on the amendment to the architecture standards to accommodate the In-N-Out Burger restaurant, staff recommends the Planning Commission approve this request based on the remaining balance of the In-N-Out site being developed with the amenities as required per the specific plan, including the wood split-rail fencing to be installed along the Dinuba and Riggan street frontages, bucolic landscaping, trellis covers, on-site lighting poles, etc. to maintain the overall cohesive appearance as required per the Orchard Walk Specific Plan.

Condition No. 3 has been included for the Planning Commission's consideration permitting Pad No. 2 to be developed with the elevations provided by In-N-Out as found in Exhibit "D", subject to the remaining balance of the Pad No. 2 site incorporating the Orchard Walk amenities including the wood split-rail fencing, bucolic landscaping, trellis covers, and other site amenities as contained in the Orchard Walk Specific Plan.

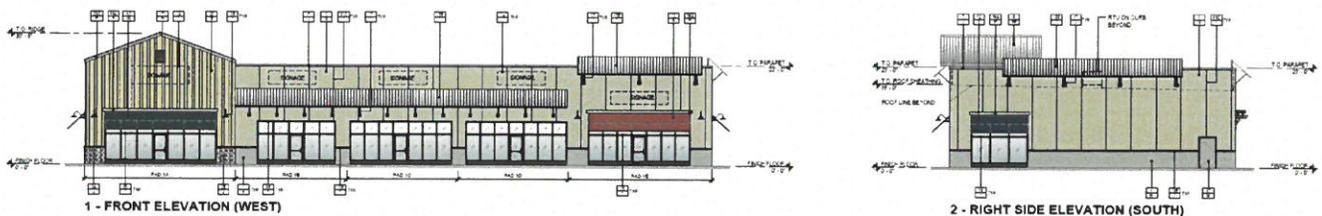
Pad No.2 – Front Elevation



Building Elevations for Pad No. 1 and Pad No. 3

The proposed buildings for Pad No. 1 and Pad No. 3 will be constructed in a rustic, bucolic fashion which complies with the Orchard Walk Specific Plan architectural requirements.

(See inset below).



Pad No.1 – Front and Right Side Elevations



Pad No.3 – Front and Right Side Elevations

Staff has included Condition No. 4 of the Conditions of Approval for the Conditional Use Permit requiring that the building elevations be developed consistent with those provided in Exhibits “C” and “D”.

Drive-thru Operations

The Site Plan Review Committee concluded that the drive-thru lanes provide adequate stacking area and traffic will not obstruct any other drive-aisles on-site or the public street. Any significant changes to the drive-through configuration will require an additional amendment to the Conditional Use Permit. Significant changes can include, but are not limited to, an increase in the number of drive-thru lanes, relocating the lanes to be adjacent to public right-of-way, or the reduction of the vehicle stacking areas.

The applicant has provided information for the In-N-Out Burger restaurant. There are residential subdivisions located to the south across Riggan Ave. The In-N-Out site plan (see Exhibit “B”) depicts the menu board and speaker box location along Riggan Ave. The distance from the speaker box to the closest residential structure is over approximately 140-ft. Given the distance of the speaker box to sensitive noise receptors, the anticipated noise levels will be less than significant. However, noise levels emitted from the speaker box of the drive-thru are still required to meet the community noise standards as set forth in Chapter 8.36 (Noise Ordinance) of the Municipal Code and pursuant to Section 17.32.162 of the Drive-thru Performance Standards. The requirement to comply with the City’s Noise requirements is included as Condition No. 6 of the Conditions of Project of Approval for all three drive-thrus.

Parking

The parking standards for the Orchard Walk Specific Plan are based on the Zoning Ordinance requirements for specific uses. For shopping centers, a flat ratio of one space per 225 square feet of building area is applied. Based on this ratio, the total parking requirement for all three Pads would be 82 spaces; however, 132 spaces have been provided. The site plan shows five of the parking spaces as accessible parking spaces, and eight (8) of the parking spaces as electric vehicle (EV) parking spaces. Staff has determined the proposed quantity and location of parking spaces to serve the buildings developed in Phase One is consistent with the Orchard Walk Specific Plan.

Sign Program

The Orchard Walk Specific Plan includes a separate sign program that establishes the sign allowance for each business within the shopping center. In-N-Out proposes to comply with the development’s sign requirements regarding the monument signs and tenant panel. This includes the placement of their logo on corrugated metal consistent with the monument sign specifications and block letters on wooden boards as required for the tenant panels (refer to Exhibit “G”). In-N-Out Burger, however, is seeking to amend the sign program for their wall and directional signage, which the owner of the shopping center supports. Staff supports the deviation on the wall signs and directional signs based on the remaining balance of the site installing all site amenities as required per the specific plan.

Environmental Review

This project is considered Categorical Exempt under Section 15303 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA) for new construction less than 10,000 square feet in urban areas (Categorical Exemption No. 2019-70).

RECOMMENDED FINDINGS

1. That the site is located within The Orchard Walk Specific Plan, and is consistent with the goals and policies of The Orchard Walk Specific Plan.
2. That the proposed Conditional Use Permit is consistent with the intent, objectives, and policies of the General Plan and Zoning Ordinance.
3. That the proposed location of the Conditional Use Permit is compatible with adjacent land uses.
4. That the proposed Conditional Use Permit is not detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
5. That the project is considered Categorical Exempt under Section 15303 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA), for new construction less than 10,000 square feet in urban areas (Categorical Exemption No. 2019-70).

RECOMMENDED CONDITIONS OF APPROVAL

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2007-076.
2. That the site be developed in substantial compliance with the Site Plan in Exhibits "A" and "B".
3. That Pad No. 2 be developed with the elevations provided by In-N-Out in Exhibit "B" subject to the remaining balance of the Pad No. 2 site incorporating the Orchard Walk amenities including the wood split-rail fencing, bucolic landscaping, trellis covers, and other items as contained in the Orchard Walk Specific Plan.
4. That the building elevations be developed in substantial compliance with the elevations shown in Exhibit "C" and "D".
5. That the applicant replants any vegetation, including street trees along Dinuba Blvd. that are dead.
6. That the noise emitted from the drive-thru teller speaker box shall meet all community noise standards.
7. That all signs shall require a separate building permit.
8. That the applicant shall have all installed public infrastructure and hardscape improvements repaired, re-tested, and reconditioned, prior to acceptance of these improvements by the City Engineer. In addition, the improvements for the right-of-way of W. Sedona Ave. between N. Conyer St. and N. Dinuba Blvd. shall be completed as required per the City's Engineering Development Standards for this street, and as deemed necessary by the City Engineer.
9. That all other federal, state and city codes, ordinances and laws be met.

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.02.145, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission on a conditional use permit application. An appeal shall be in writing and shall be filed with the City Clerk at 220 N. Santa Fe Street, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record.

Attachments:

- Related Plans and Policies
- Resolution
- Exhibit "A" – Phase One Site Plan "Overall"
- Exhibit "B" – Pad No. 2 "In-n-Out Burger"
- Exhibit "C" – Landscaping and Site Amenities Plan
- Exhibit "D" – Building Elevations
- Exhibit "E" – Vehicular Access
- Exhibit "F" – Trip Generation Comparison
- Exhibit "G" – Sign Package (In-N-Out Signage)
- Exhibit "H" – 2007 Orchard Walk Site Plan
- Exhibit "I" – Acoustical Analysis
- Site Plan Review Comments
- General Plan Land Use Map
- Zoning Map
- Aerial Photo
- Location Map

Related Plans and Policies

Chapter 17.18: PLANNED COMMERCIAL ZONES

17.19.10 Purpose and intent.

- A. The several types of mixed zones included in this chapter are designed to achieve the following:
 1. Encourage a wide mix of commercial, service, office, and residential land uses in horizontal or vertical mixed use development projects, or on adjacent lots, at key activity nodes and along corridors.
 2. Maintain Visalia's downtown Conyer Street to Tipton and Murray Street to Mineral King Avenue including the Court-Locust corridor to the Lincoln Oval area) as the traditional, medical, professional, retail, government and cultural center;
 3. Provide zone districts that encourage and maintain vibrant, walkable environments.
- B. The purposes of the individual mixed use zones are as follows:
 1. Mixed Use Commercial Zone—(C-MU). The purpose and intent of the mixed use commercial zone district is to allow for either horizontal or vertical mixed use development, and permit commercial, service, office, and residential uses at both at key activity nodes and along corridors. Any combination of these uses, including a single use, is permitted.
 2. Mixed Use Downtown Zone—(D-MU). The purpose and intent of the mixed use downtown zone district is to promote the continued vitality of the core of the community by providing for the continuing commercial development of the downtown and maintaining and enhancing its historic character. The zone is designed to accommodate a wide mix of land uses ranging from commercial and office to residential and public spaces, both active and passive. The zone is intended to be compatible with and support adjacent residential uses, along with meeting the needs of the city and region as the urban center of the city; to provide for neighborhood, local, and regional commercial and office needs; to accommodate the changing needs of transportation and integrate new modes of transportation and related facilities; and to maintain and enhance the historic character of the city through the application of architectural design features that complement the existing historic core of the city. (Ord. 2017-01 (part), 2017)

17.32.162 Drive-thru lanes performance standards.

- A. Purpose and Intent. It is the purpose of this section to specify performance standards applicable to uses that seek to incorporate a drive-thru lane in association with a specified use. This section does not apply to carwashes and lube and oil changing stations.
- B. Performance standards:
 1. Separation from residences. The drive-thru lane shall be no less than two hundred fifty (250) feet from the nearest residence or residentially zoned property.
 2. Stacking. The drive-thru lane shall contain no less than ten (10) vehicle stacking, measured from pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicle spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.
 3. Circulation. No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive-thru shall not take ingress or egress from a local residential road.
 4. Noise. No component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60 dB between the hours of 7:00 p.m. and 6:00 a.m. daily.
 5. Screening. The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.
 6. Menu boards and signage. Shall be oriented or screened to avoid direct visibility from adjacent public streets. (Ord. 2017-01 (part), 2017; Ord. 2014-07 § 3, 2014)

Table 17.25.030

Commercial, Mixed Use, Office, and Industrial Zones Use Matrix

P = Use is Permitted by Right
 T = Use Requires Temporary Use Permit
 C = Use Requires Conditional Use Permit
 Blank = Use is Not Allowed

USE	Commercial and Mixed Use Zones					Office Zones			Industrial Zones		Special Use Standards (See identified Chapter or Section)
	C-N	C-R	C-S	C-MU	D-MU	O-PA	O-C	BRP	I-L	I	
D											
DAYCARE, LICENSED											
D1	P	P	P	P	P	P	P	P	P	P	P
D2	P	P	P	P	P	P	P	P	P	P	P
D3	C	C	C	C	C	C	C	C	C	C	C
D4	P	P	P	P	P	P	P	P	P	P	P
D5	P	P	P	P	P	P	P	P	P	P	P
D6	C	C	C	C	C	C	C	C	C	C	C
D7	P	P	P	P	P	P	P	P	P	P	P
D8	P	P	P	P	P	P	P	P	P	P	17.32.162
D9	C	C	C	C	C	C	C	C	C	C	17.32.162
D10									C	C	17.32.161
E											
EATING & DRINKING ESTABLISHMENTS											
E1	C	C		C							
E2		P		C							

RESOLUTION NO. 2019-66

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT NO. 2019-36, A REQUEST BY DONAHUE SCHRIBER GROUP TO DEVELOP THREE PAD SITES WITH FAST FOOD DINING, RETAIL SHOPS, ON-SITE PARKING AND DRIVE-THRUS ACROSS 3.72 ACRES OF THE ORCHARD WALK WEST DEVELOPMENT AREA. THE SITE IS ZONED C-MU (MIXED USE COMMERCIAL ZONE) AND IS LOCATED AT THE NORTHWEST CORNER OF NORTH DINUBA BLVD AND WEST RIGGIN AVE. (APN: 078-120-030 & 078-120-032)

WHEREAS, Conditional Use Permit No. 2019-36, is a request by Donahue Schriber Realty Group to develop three pad sites with fast food dining, retail shops, on-site parking and drive-thrus across 3.72 acres of the Orchard Walk West development area. The site is zoned C-MU (Mixed Use Commercial Zone) and located at the northwest corner of N. Dinuba Blvd. and W. Riggan Ave. (APN: 078-120-030 & 078-120-032); and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on November 12, 2019; and

WHEREAS, the Planning Commission of the City of Visalia finds the Conditional Use Permit to be in accordance with Chapter 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, the Planning Commission finds the project to be Categorically Exempt consistent with the California Environmental Quality Act (CEQA) and City of Visalia Environmental Guidelines.

NOW, THEREFORE, BE IT RESOLVED that the project is exempt from further environmental review pursuant to CEQA Section 15303.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

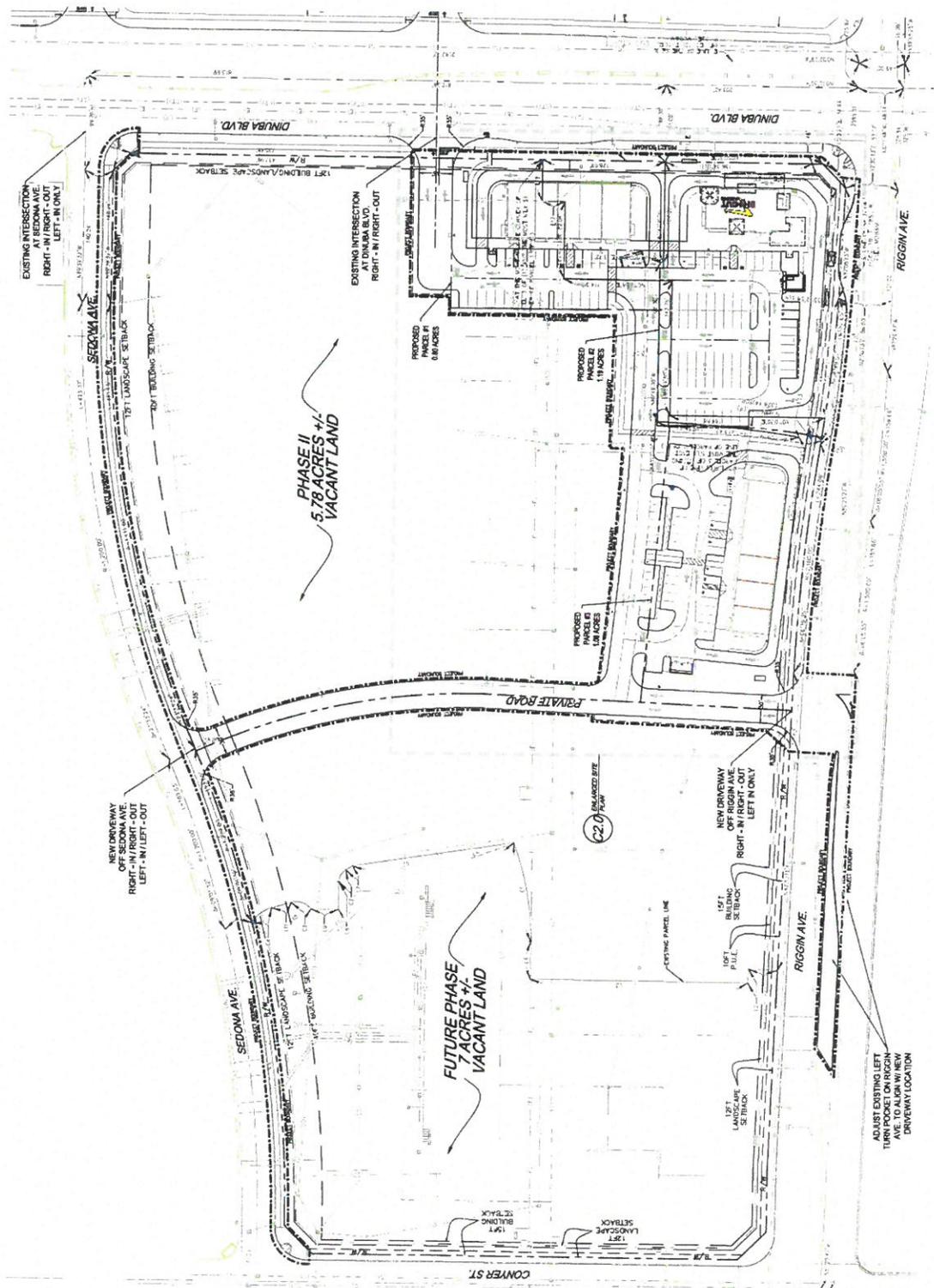
1. That the site is located within The Orchard Walk Specific Plan, and is consistent with the goals and policies of The Orchard Walk Specific Plan.
2. That the proposed Conditional Use Permit is consistent with the intent, objectives, and policies of the General Plan and Zoning Ordinance.
3. That the proposed location of the Conditional Use Permit is compatible with adjacent land uses.
4. That the proposed Conditional Use Permit is not detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
5. That the project is considered Categorically Exempt under Section 15303 of the Guidelines for the Implementation of the California Environmental Quality Act

(CEQA), for new construction less than 10,000 square feet in urban areas (Categorical Exemption No. 2019-70).

BE IT FURTHER RESOLVED that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the project be developed in substantial compliance with the comments from the approved Site Plan Review No. 2007-076.
2. That the site be developed in substantial compliance with the Site Plan in Exhibits "A" and "B".
3. That Pad No. 2 be developed with the elevations provided by In-N-Out in Exhibit "B" subject to the remaining balance of the Pad No. 2 site incorporating the Orchard Walk amenities including the wood split-rail fencing, bucolic landscaping, trellis covers, and other items as contained in the Orchard Walk Specific Plan.
4. That the building elevations be developed in substantial compliance with the elevations shown in Exhibit "C" and "D".
5. That the applicant replants any vegetation, including street trees along Dinuba Blvd. that are dead.
6. That the noise emitted from the drive-thru teller speaker box shall meet all community noise standards.
7. That all signs shall require a separate building permit.
8. That the applicant shall have all installed public infrastructure and hardscape improvements repaired, re-tested, and reconditioned, prior to acceptance of these improvements by the City Engineer In addition, the improvements for the right-of-way of W. Sedona Ave. between N. Conyer St. and N. Dinuba Blvd. shall be completed as required per the City's Engineering Development Standards for this street, and as deemed necessary by the City Engineer.
9. That all other federal, state and city codes, ordinances and laws be met.

EXHIBIT "A"

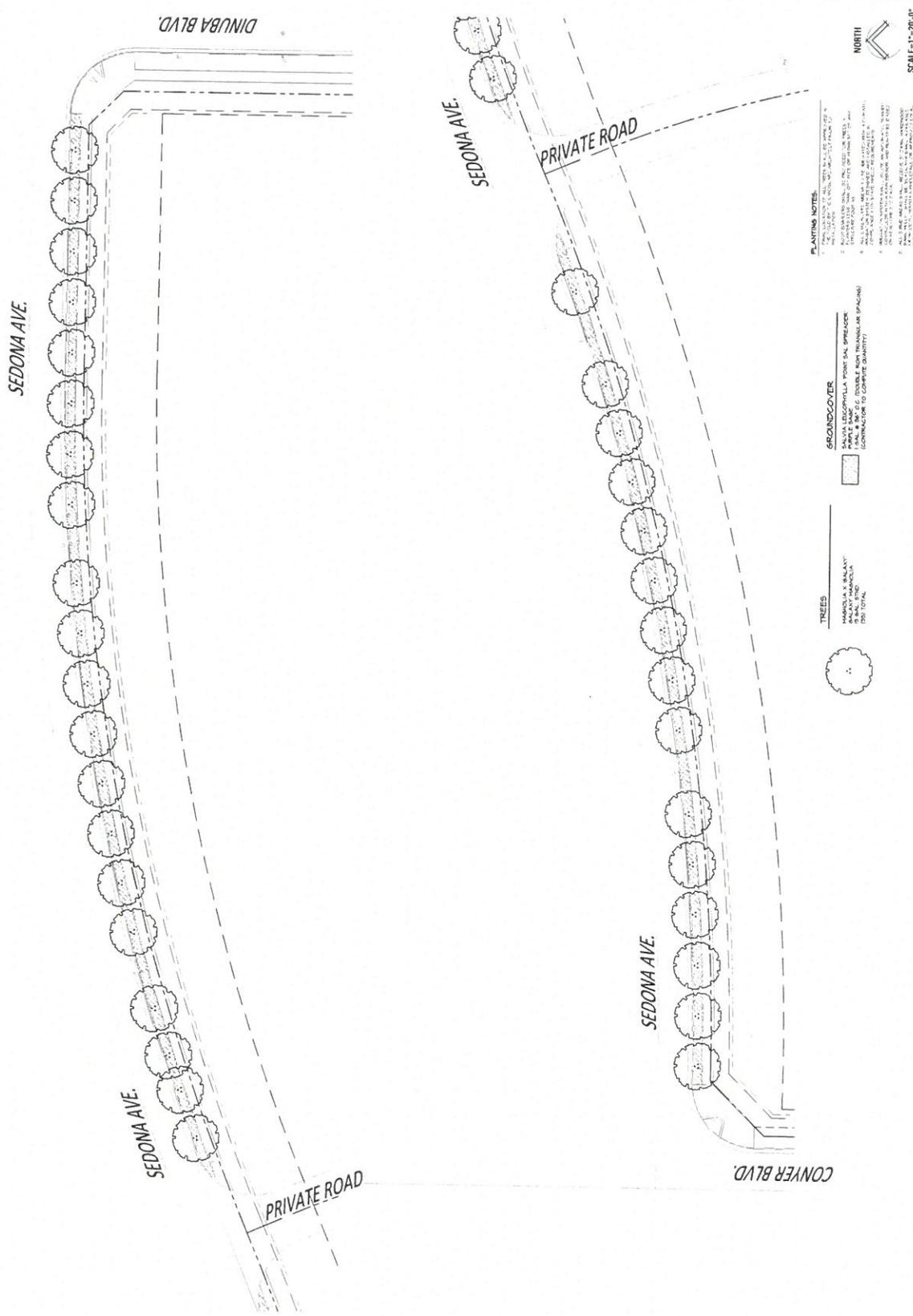




DATE: 06/11/10
PROJECT: ORCHARD WALK WEST

6011.1.10.07.00
PRELIMINARY
LANDSCAPE
PLAN

06/11/10 L2.1



PLANTING NOTES:
1. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
2. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
3. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
4. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
5. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
6. ALL PLANTING SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.

GROUND COVER:
1. ALL GROUND COVER SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
2. ALL GROUND COVER SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.

TREES:
1. ALL TREES SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.
2. ALL TREES SHALL BE DONE IN ACCORDANCE WITH THE CITY OF VISALIA PLANTING SPECIFICATIONS.

SCALE: 1"=20'-0"
NORTH

MASTER PLAN - CONCEPTUAL USE PERMIT SH-10-076

Exhibit "C"

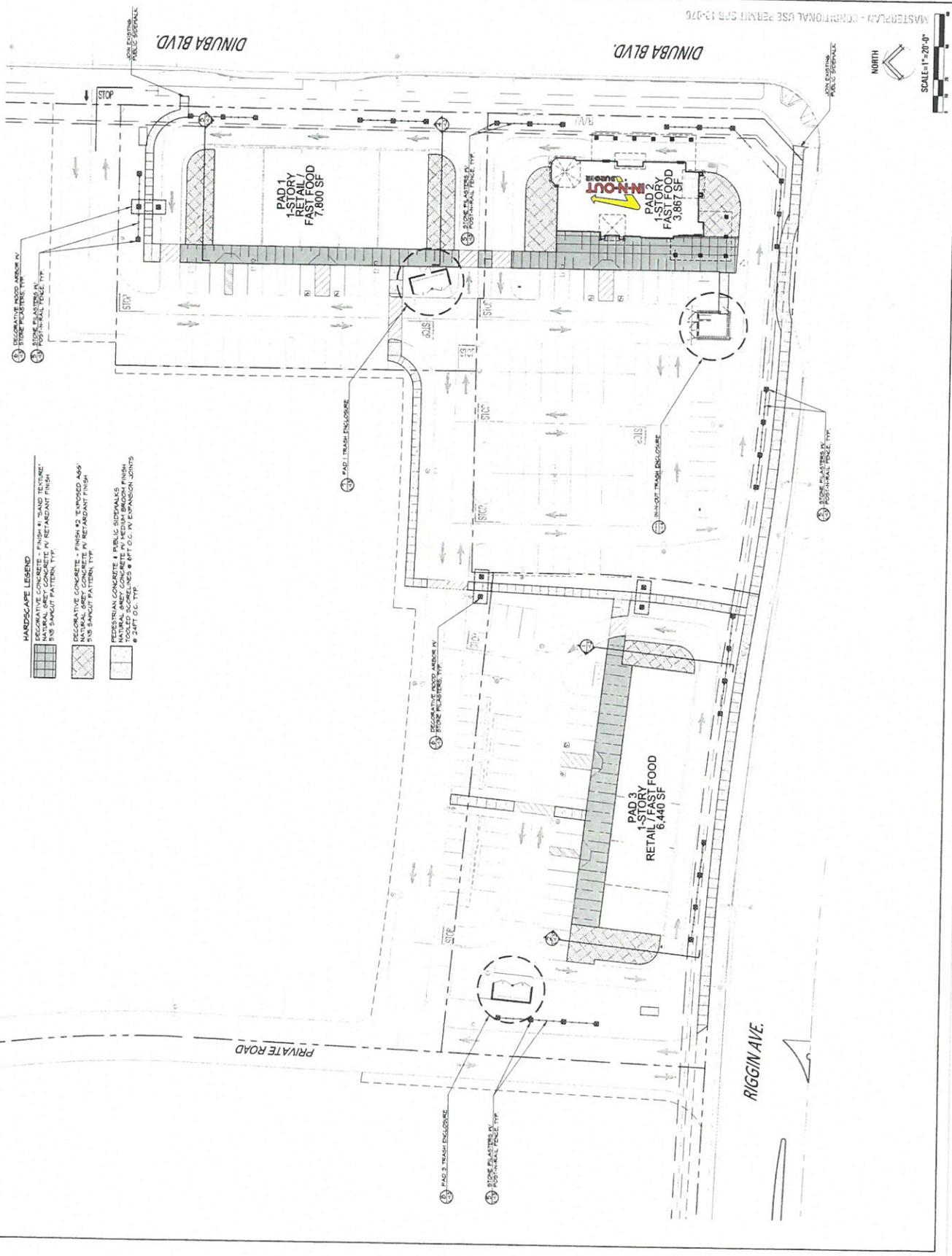
LITTLE
 ARCHITECTS
 1000 UNIVERSITY AVENUE
 SUITE 100
 BERKELEY, CA 94702
 TEL: 415.863.1100
 WWW.LITTLEARCHITECTS.COM

DOVARGANES
 ARCHITECTS
 1000 UNIVERSITY AVENUE
 SUITE 100
 BERKELEY, CA 94702
 TEL: 415.863.1100
 WWW.DOVARGANESARCHITECTS.COM

**ORCHARD WALK WEST
 PHASE ONE**
 NW CORNER - RIGGIN AVE. & DINUBA BLVD.
 VISALIA, CA 93291

DATE	DESCRIPTION
08/11/19	PRELIMINARY HARDSCAPE PLAN
08/11/19	1.0

60113467.00
**PRELIMINARY
 HARDSCAPE
 PLAN**



- HARDSCAPE LEGEND**
- DECORATIVE WOOD ANKOR IV
 - STONE PAVERS TYP.
 - 3/8" SAND/OUT PATERN TYP.
 - RETARDANT FINISH
 - 3/8" SAND/OUT PATERN TYP.
 - DECORATIVE CONCRETE - FINISH #5
 - EXPOSED AGG
 - NATURAL GREY CONCRETE IV
 - RETARDANT FINISH
 - 3/8" SAND/OUT PATERN TYP.
 - PEDESTAL CONCRETE
 - 1" REBAR SUBSTRALS
 - NATURAL GREY CONCRETE IV
 - MEDIUM BRUSH FINISH
 - 1/2" REBAR LINES @ 8FT O.C.
 - IV EXPANSION JOINTS @ 24FT O.C. TYP.

MASTERPLAN - CONCEPTUAL USE PERMIT SPR 19-076

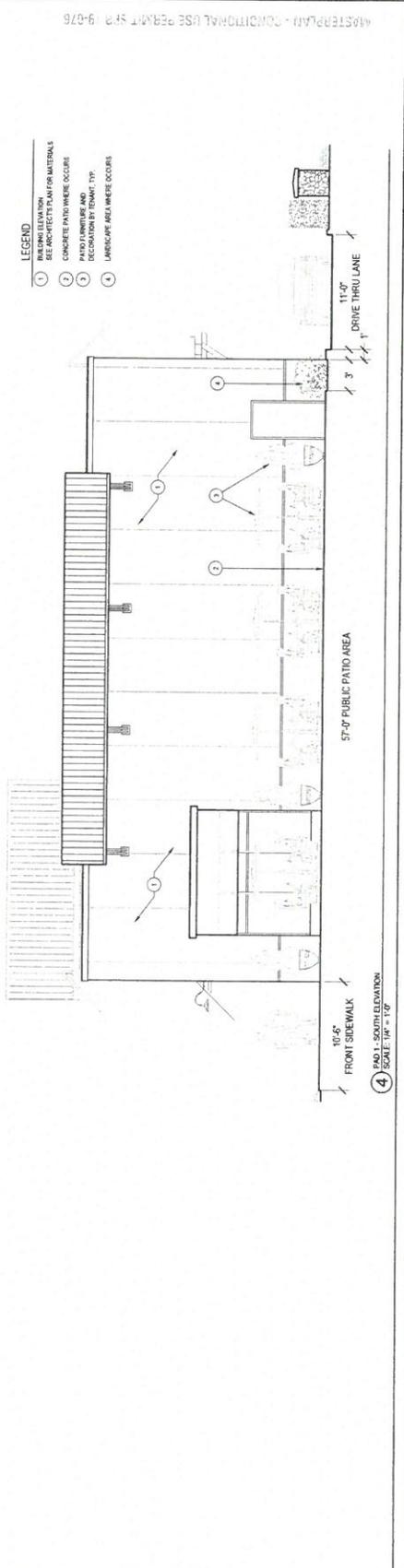
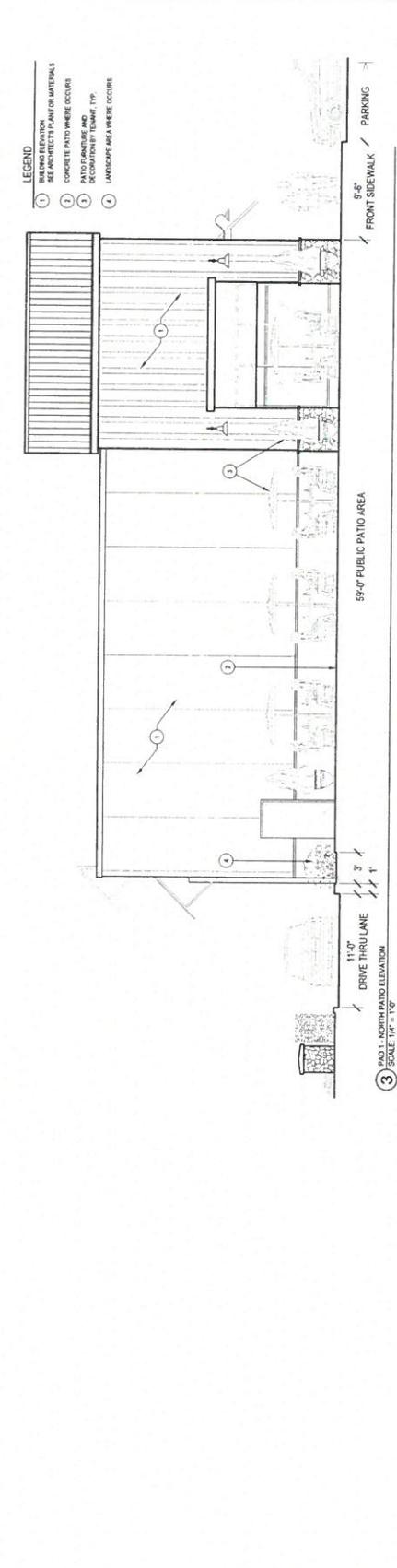
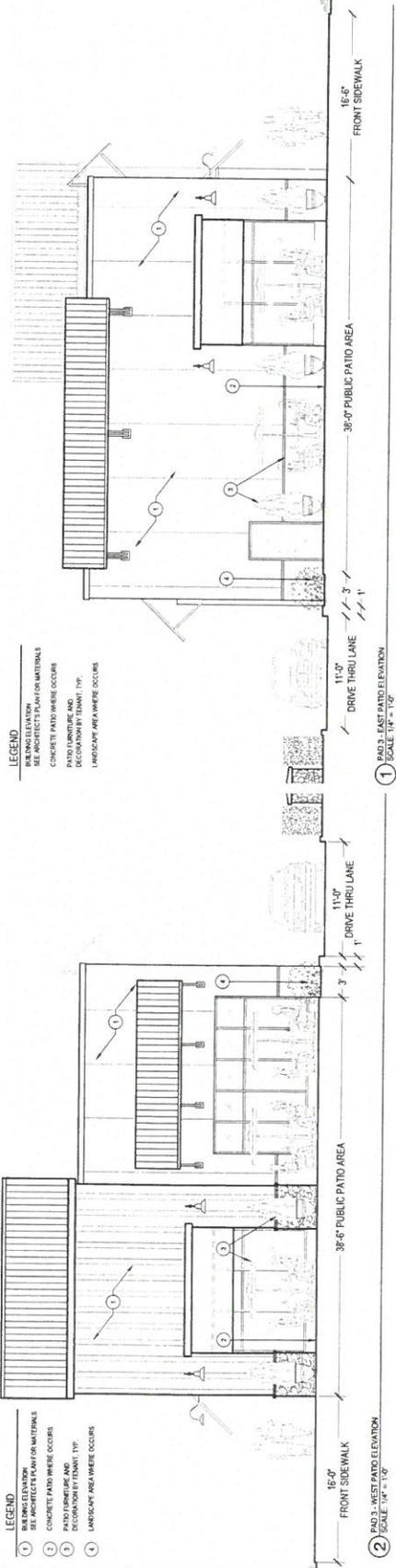
Exhibit "C"



ORCHARD WALK WEST PHASE ONE NW CORNER - RIGGIN AVE & DINUBA BLVD. VISALIA, CA 93291

DATE: 08/15/19	SCALE: 1/4" = 1'-0"
PROJECT: ORCHARD WALK WEST PHASE ONE	DATE: 08/15/19
DESIGNER: DONAHUE SCHAEFER ARCHITECTS, INC.	DATE: 08/15/19
CLIENT: LITTLE ARCHITECTS, INC.	DATE: 08/15/19
PROJECT NO: 601.310.6700	DATE: 08/15/19

COMMON AREA DETAILS
08/15/19 L1.2



MASTER PLAN - CONDITIONAL USE PERMIT SP. 9-076

Pad No. 1 Front & Rear Elevations

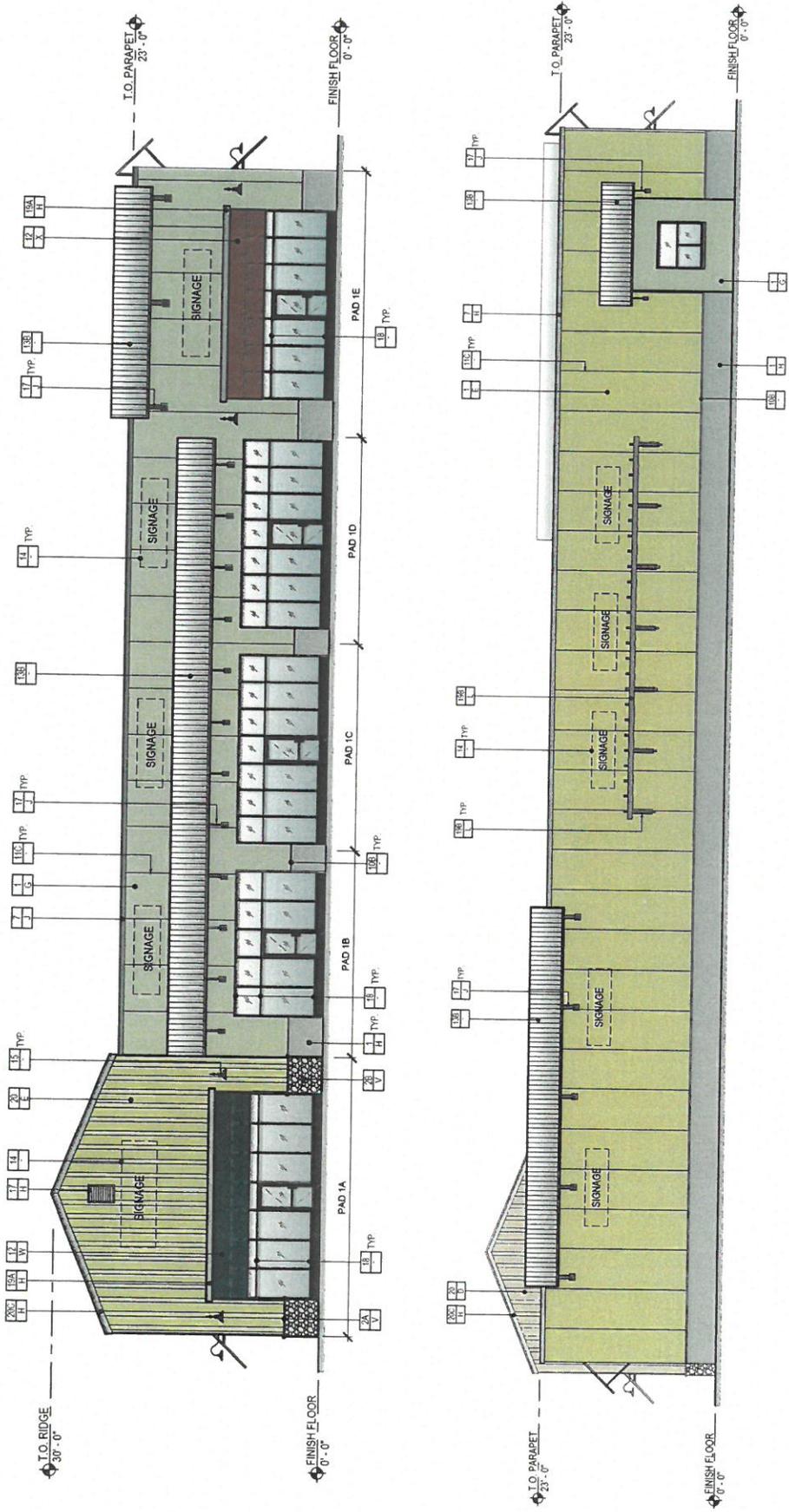


EXHIBIT "D - 1"

Pad No. 2 Front & Rear Elevations

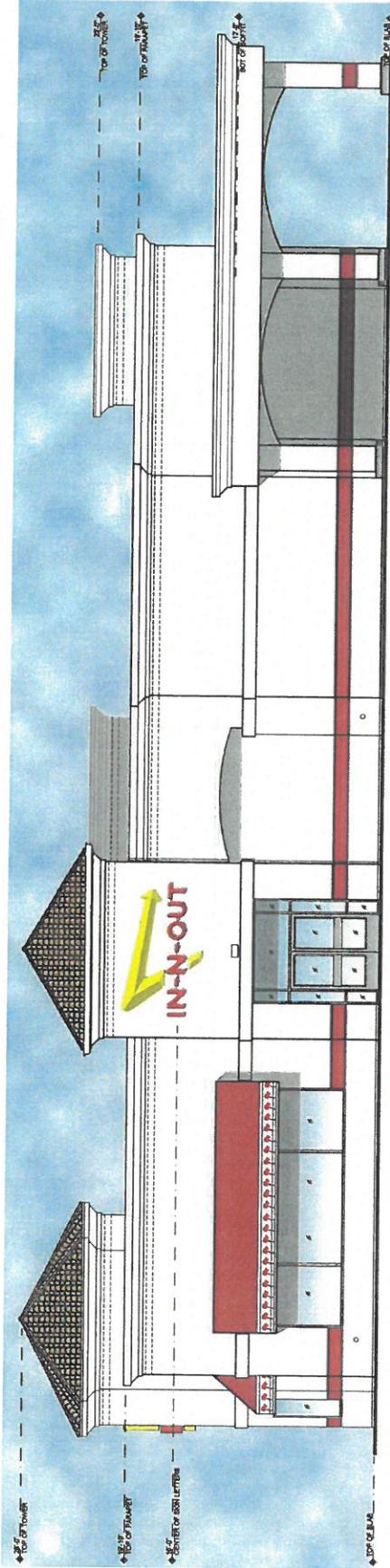


EXHIBIT "D - 3"

Pad No. 2 Side Elevations

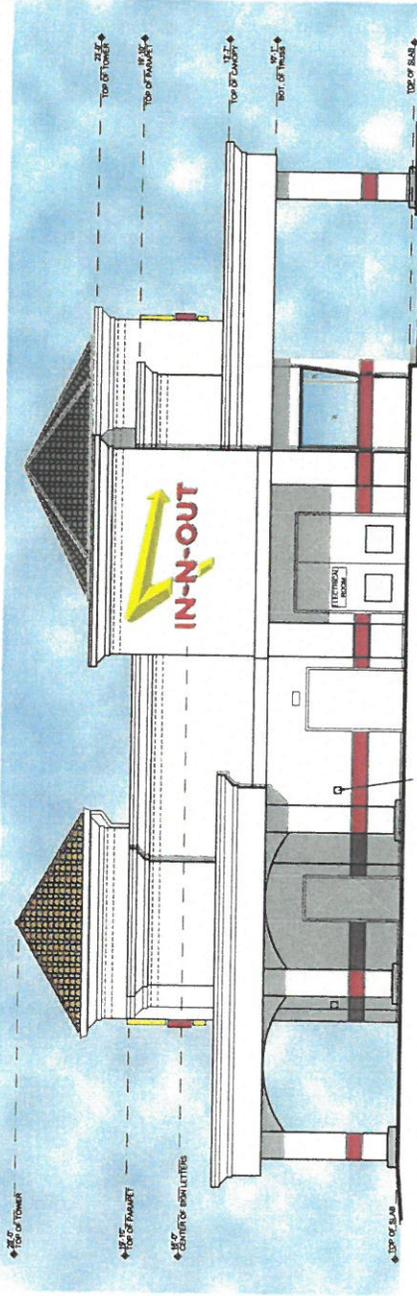


EXHIBIT "D - 4"

Pad No. 3 Front & Rear Elevations

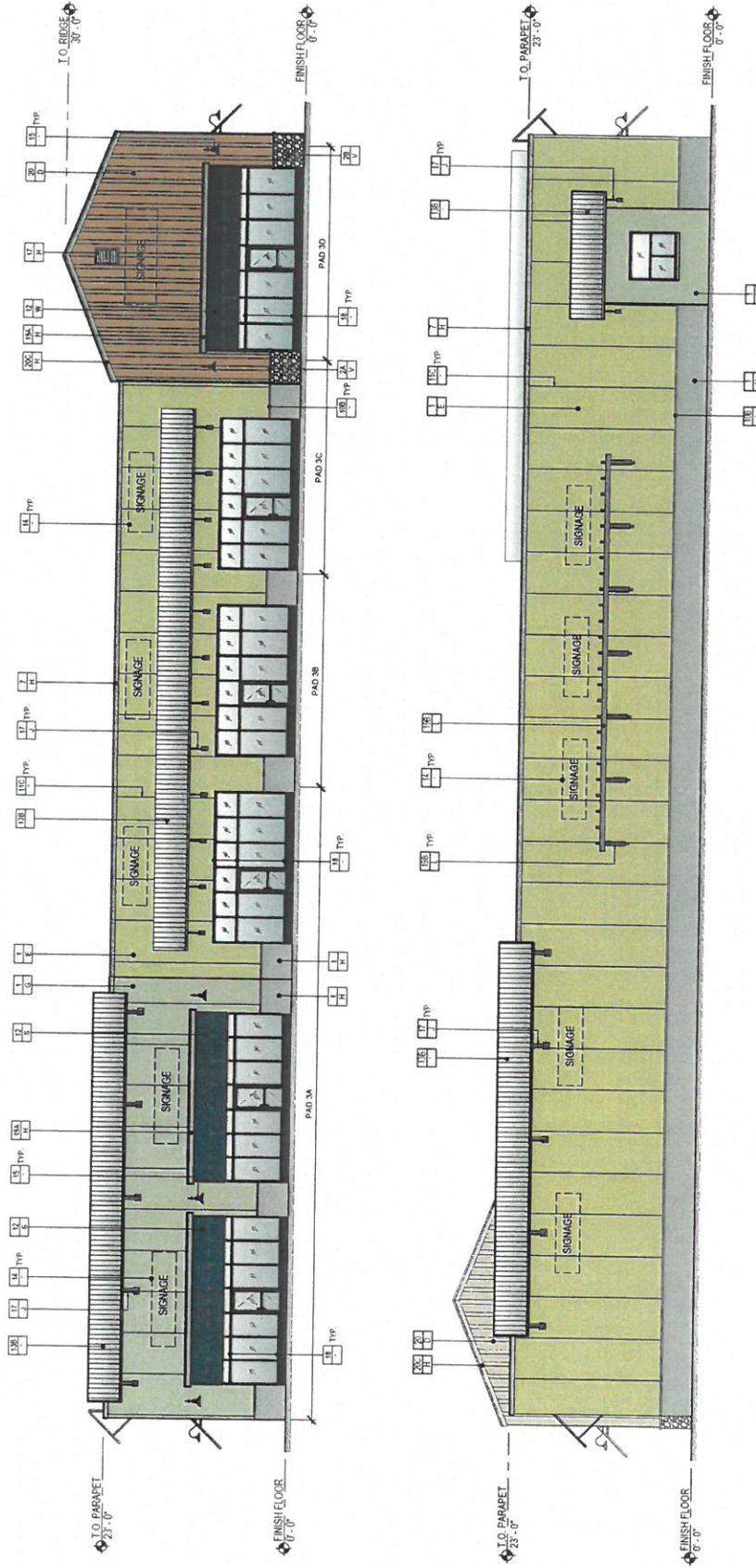


EXHIBIT "D - 5"

Pad No. 3 Side Elevations

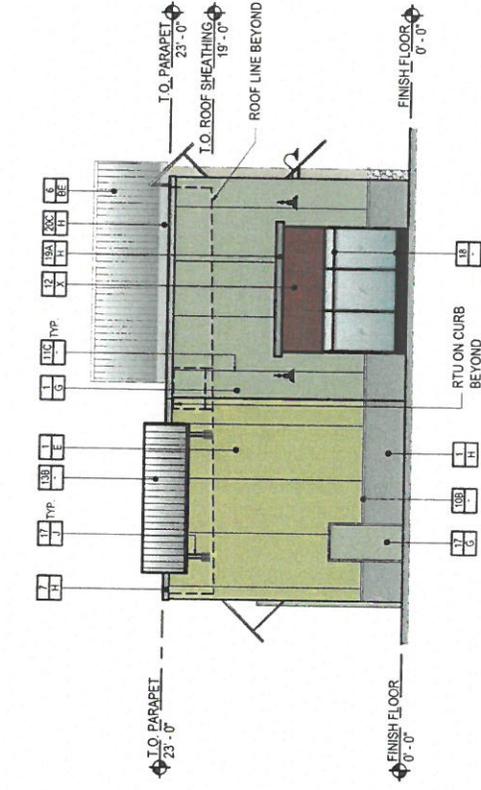
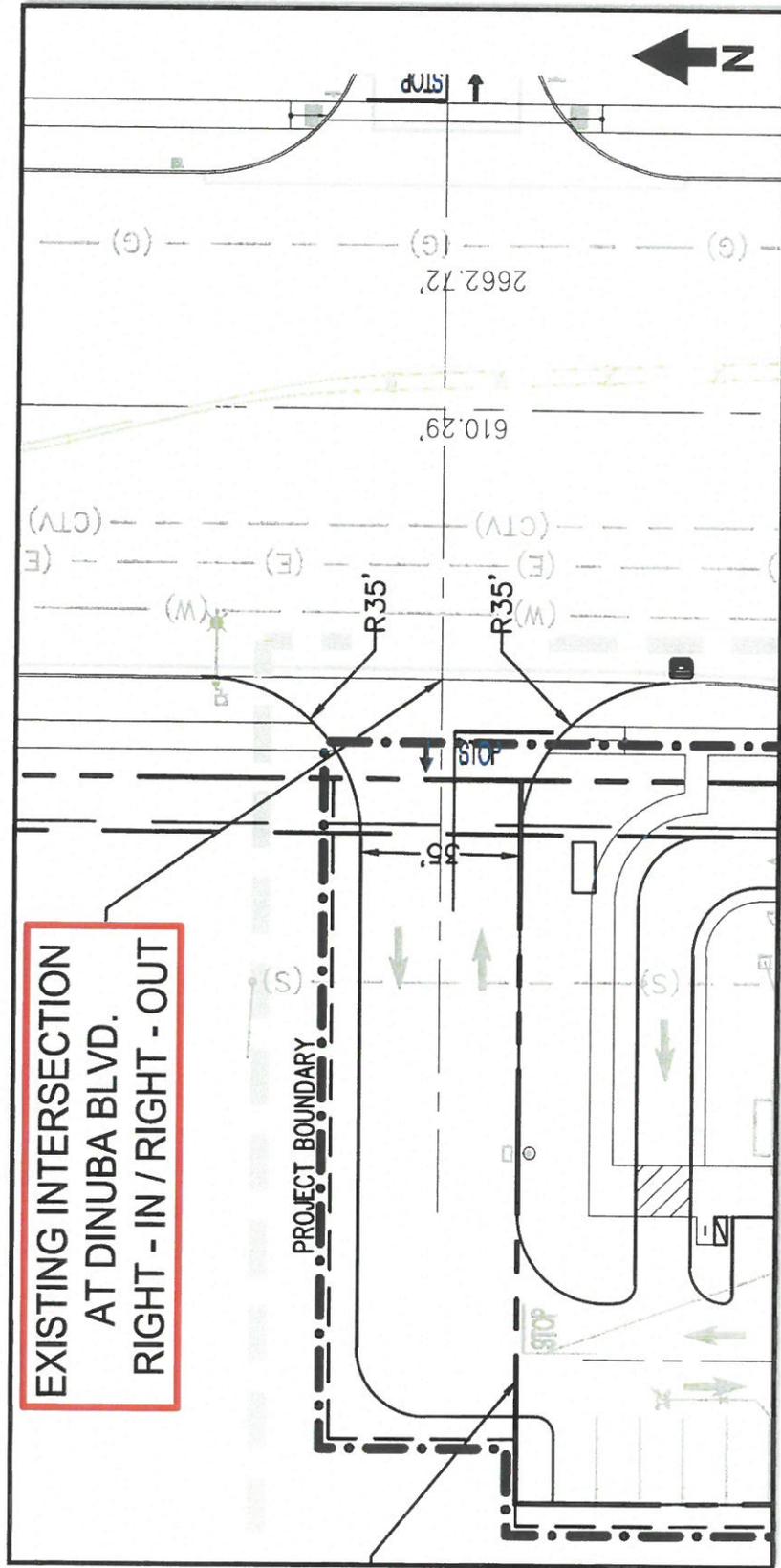


EXHIBIT "D - 6"

Vehicular Access from Dinuba Boulevard



**EXISTING INTERSECTION
AT DINUBA BLVD.
RIGHT - IN / RIGHT - OUT**

EXHIBIT "E - 2"

Vehicular Access from Sedona Avenue

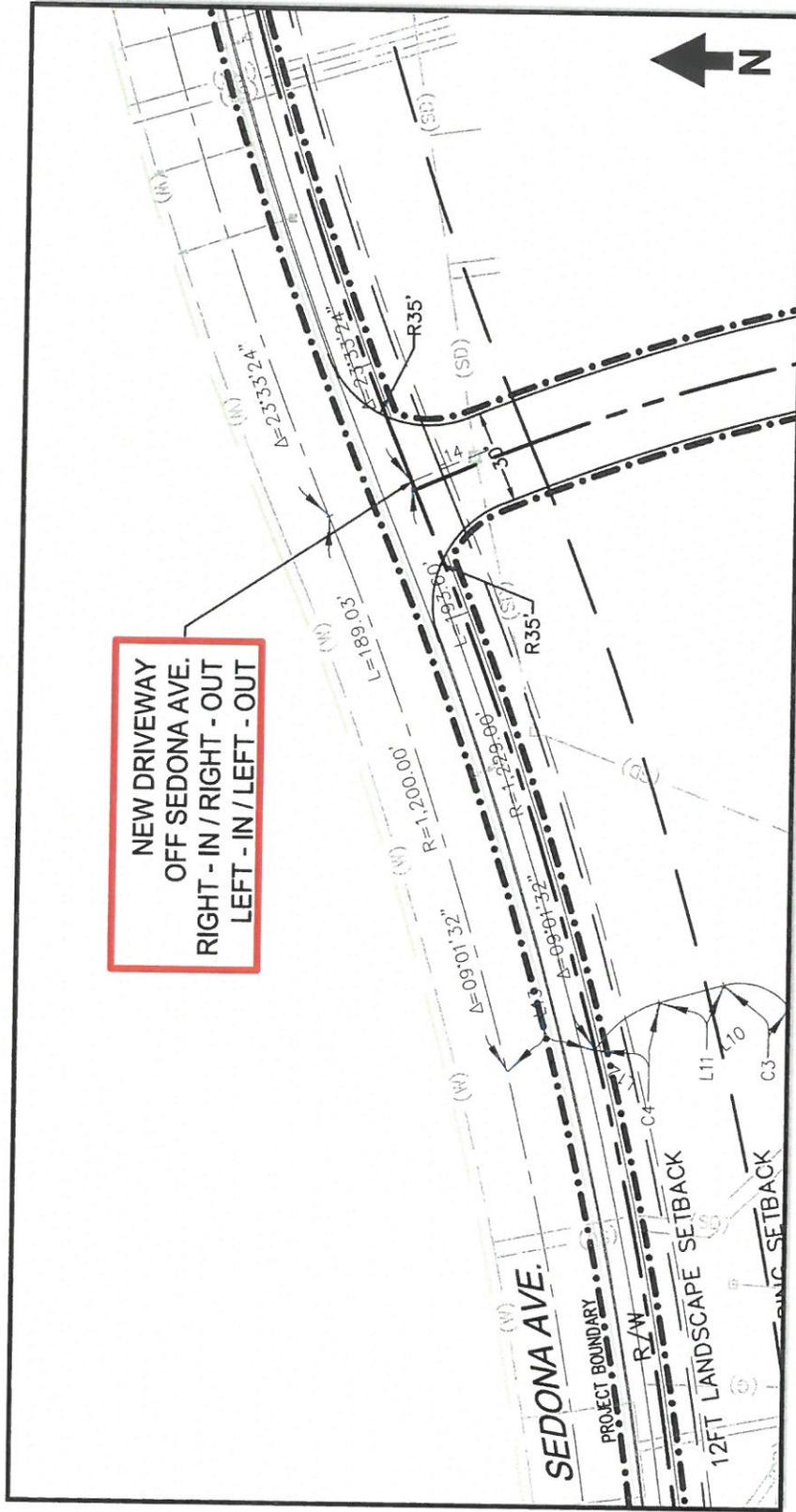


EXHIBIT "E - 3"

Private Road Connecting Riggan Avenue (south)
To Sedona Avenue (north)

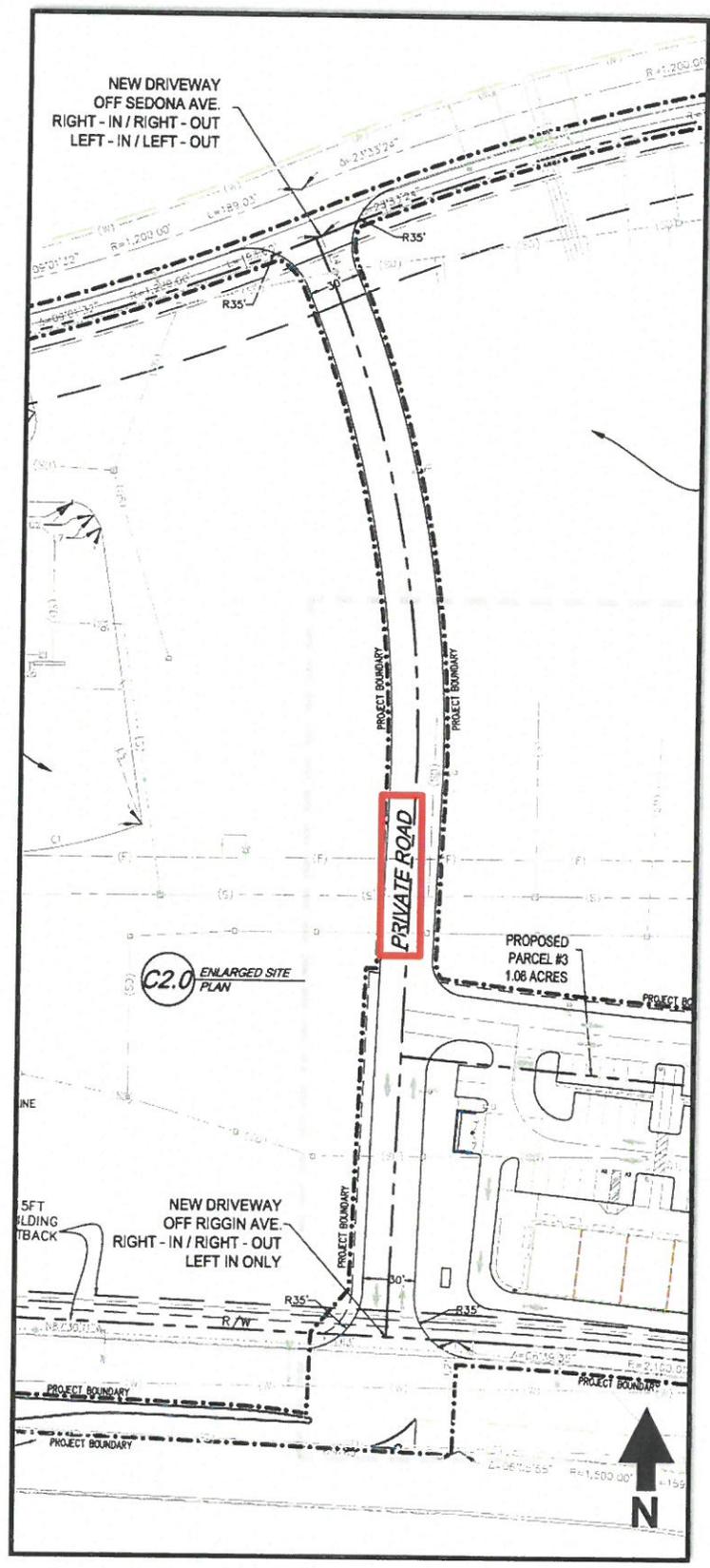


EXHIBIT "E - 4"



Table 1
2007 Traffic Generation Data

Land Use	Square Feet	24-Hour Two-Way Volume	AM Peak Hour Enter	AM Peak Hour Exit	PM Peak Hour Enter	PM Peak Hour Exit
Home Improvement Superstore	106,432	3,172	69	59	123	138
Nursery (Wholesale)	34,760	769	12	12	35	35
Shopping Center	41,280	3,821	56	36	168	181
High-Turnover (Sit-Down) Restaurant	14,900	1,894	89	83	99	64
Fast-Food Restaurant w/o Drive-Through	8,500	6,086	223	150	114	109
TOTALS	205,872	15,742	449	340	539	527

Source: 2007 Traffic Impact Study, Orchard Walk West, TPG Consulting

Table 2 estimates the vehicle trips generated for the proposed 2019 development on the same 15.85-acre site. The property is currently intended to be developed in multiple phases and parcels. Phase I has a combined development area of 3.07 acres and consists of Parcels #1, #2 and #3 per the site plan 19-076 (2nd submittal), dated July 24, 2019 (see attached). Phase II (5.78 acres) and future phases (7 acres) have not yet been assigned specific land uses; therefore, 30% of the remaining 12.78 acres of the development was designated as general shopping center in order to produce an estimated value for calculating daily trips generated by the entire development.

Table 2
Orchard Walk West Parcel – Multi-Use Trip Generation (2019)

Land Use	Square Feet	24-Hour Two-Way Volume	AM Peak Hour (Enter)	AM Peak Hour (Exit)	PM Peak Hour (Enter)	PM Peak Hour (Exit)
PHASE I - 3.07ac						
Fast-Food Restaurant w/ Drive-Through	10,967	5,165	225	216	186	172
Shopping Center	9,640	364	6	3	18	19
Subtotal	20,607	5,529	231	219	204	191
PHASE II & FUTURE						
PHASE - 12.78 ac						
Shopping Center ¹	167,009 ¹	6304	97	60	305	331
Subtotal	167,009	6304	97	60	305	331
OVERALL TOTALS	187,616	11,833	328	279	509	522

Source: Institute of Transportation Engineers (ITE) Trip General Manual, Volume 2 - Part 3 - 10th Edition

¹ Square footage estimated at 30% of 12.78 acres to match estimated buildout FAR from 2007 project buildout.



CONCLUSION

The original 2007 project had been previously approved by the City of Visalia and consisted of a high-volume home improvement superstore, a wholesale nursery, two (2) fast-food restaurants without drive-throughs, two (2) high-turnover restaurants and general shopping. This 15.85-acre site generated 15,742 daily, 789 AM peak hour and 1,066 PM peak hour vehicle trips.

The current 2019 proposal for the same site remains largely unprogrammed at this time except for the 3.07 acres of fast-food restaurants and shopping center uses that have already been proposed in Phase I. For purposes of this enquiry, the remaining 12.78 acres were calculated at the shopping center land use rates. The results show that the 2019 proposal generates 11,833 daily, 607 AM peak hour and 1031 PM peak hour trips per day. When compared to the 2007 proposed site calculations, there is an overall reduction of 3,909 daily, 26 AM peak hour, and 34 PM peak hour trips, significantly fewer trips than the previously approved 2007 proposal. It seems there is insufficient evidence to require an additional TIS or any additional mitigation measures for Phase I of this development.

If land uses for future phases are dramatically changed or altered, to better fit with the development, then it is understood that it may become necessary to initiate a revised or updated TIS at that time.



IN-N-OUT BURGER
NWC Dinuba Blvd. & Riggins Avenue, Visalia CA

- R1: Reduced letters to 18" with flat faces; Deleted all LED coping lighting - 03/20/19 (tt)
- R2: Chg A1 location and sign layout; Chg all letters to 18" with formed faces; Added LED coping lighting - 03/21/19 (tt)
- R3: Added A4 monument sign - 04/17/19 (tt)
- R4: Revised site plan and elevations - 08/09/19 (tt)
- R5: Revised bldg elevations; Added A4 monument sign - with stone base; Relocated all ground signs - 08/13/19 (tt)
- R6: Deleted Monument Sign A4 from package - 09/18/19 (tt)
- R7: Revised sign locations for A1, A2 and A3 - 10/17/19 (tt)

Exhibit "G"



4444 Federal Blvd San Diego, CA 92102
(619) 527-6100 signitech.com

In-N-Out

NWC Dinuba Blvd. & Riggins Avenue, Visalia CA

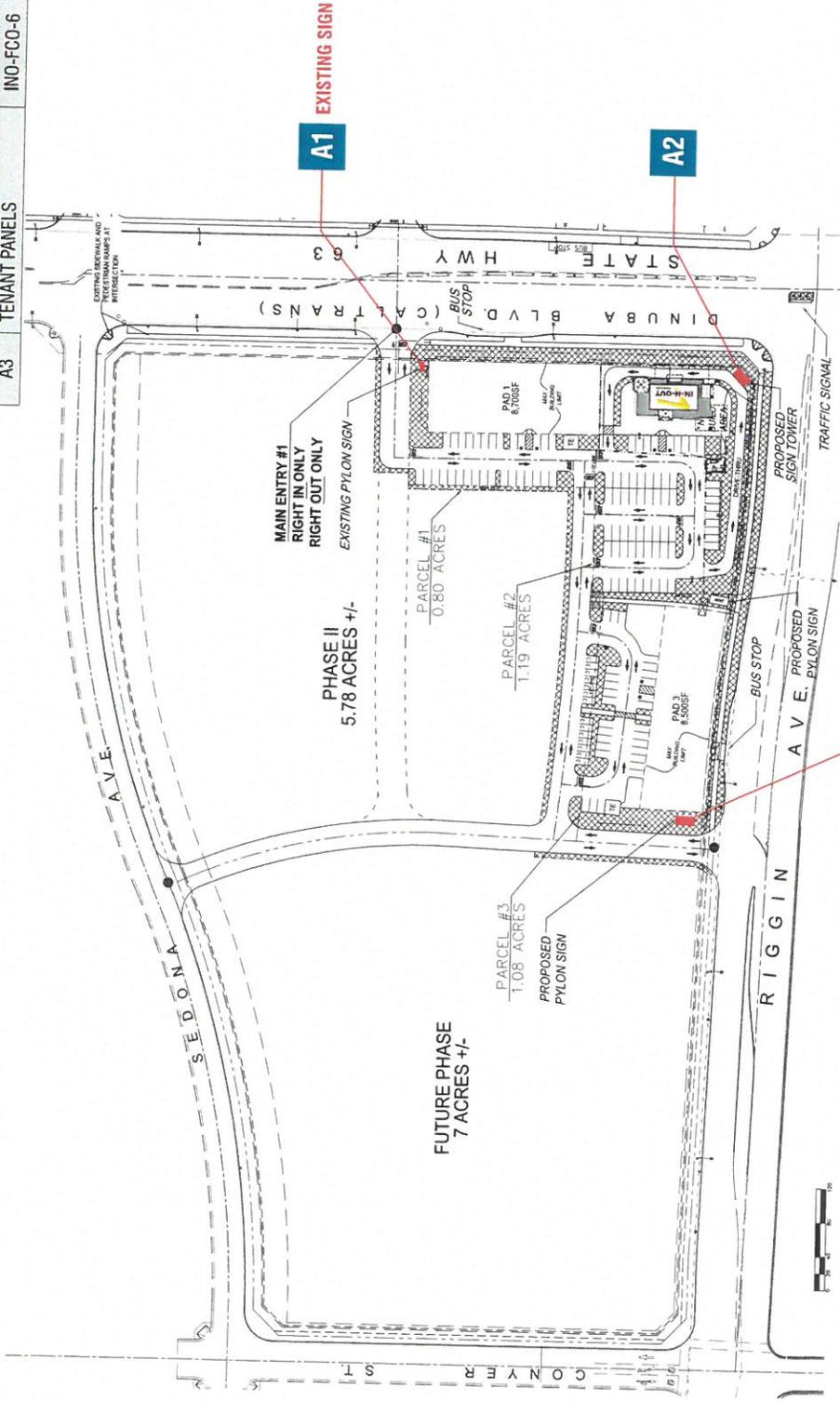
Sales: BMC
Coordinator: Garry Wilcox
Design: TThomsen
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba 93291_1

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Exhibit "G"

SIGN LEGEND				
SIGN #	DESCRIPTION	SIGN TYPE	SF	QTY
A1	MONUMENT SIGN	INO-CL-24-6	7.41	2
A2	TENANT PANELS	INO-FCO-6	1.82	2
A3	TENANT PANELS	INO-FCO-6	1.82	2



OVERALL PROJECT SITE PLAN SCALE: NONE



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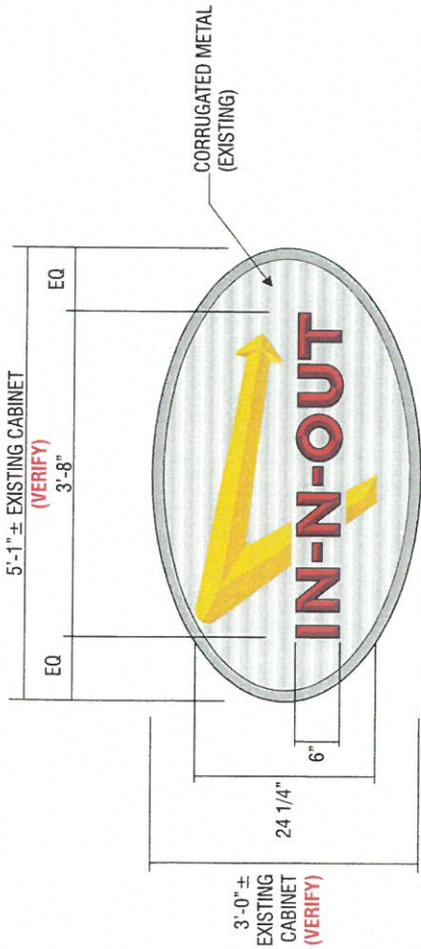
In-N-Out
NWC Dinuba Blvd. & Riggins Avenue, Visalia CA

Sales: BMC
Coordinator: Gary Wilcox
Design: TThomson
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba 93291_1

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Exhibit "G"



SCALE: 3/4" = 1'-0"

A1 SIGNTYPE INO-CL-24-6

MANUFACTURE AND INSTALL:

TWO (2) SETS OF INTERNALLY ILLUMINATED CHANNEL LETTERS

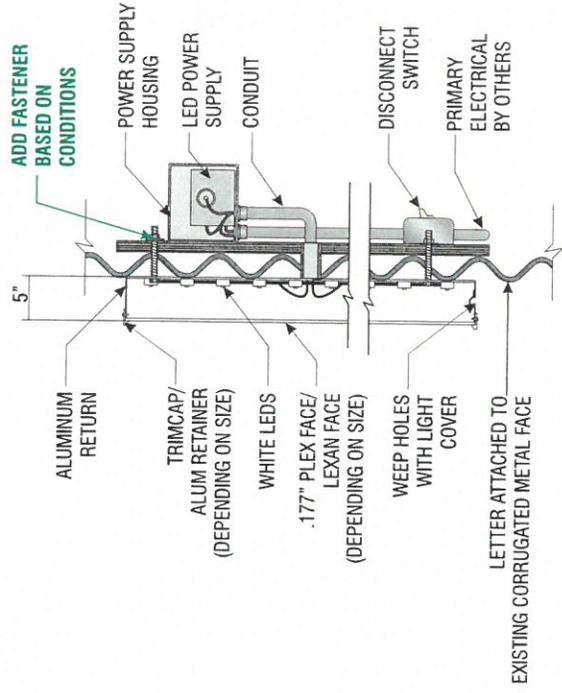
FACES: WHITE ACRYLIC WITH FIRST SURFACE APPLIED VINYL GRAPHICS

TRIMCAP: 3/4" GOLD

RETURNS: .040" ALUMINUM PAINTED DUNN-EDWARDS 'BONE CHINA'

ILLUMINATION: WHITE LEDS

- VINYL**
- V1** 3M 3630-015 'YELLOW' TRANSLUCENT
- V2** 3M 3630-235 'AUTUMN YELLOW' TRANSLUCENT
- V3** 3M 3630-73 'DARK RED' TRANSLUCENT
- V4** 3M 3630-33 'RED' TRANSLUCENT
- V5** 3M 7725-12 'BLACK' OPAQUE
- PAINT**
- P1** TO MATCH 'BONE CHINA' BY DUNN EDWARDS W/ SATIN FINISH



U.L. APPROVED LED FACE LIT CHANNEL LETTER DETAIL WITH REMOTE POWER SUPPLIES



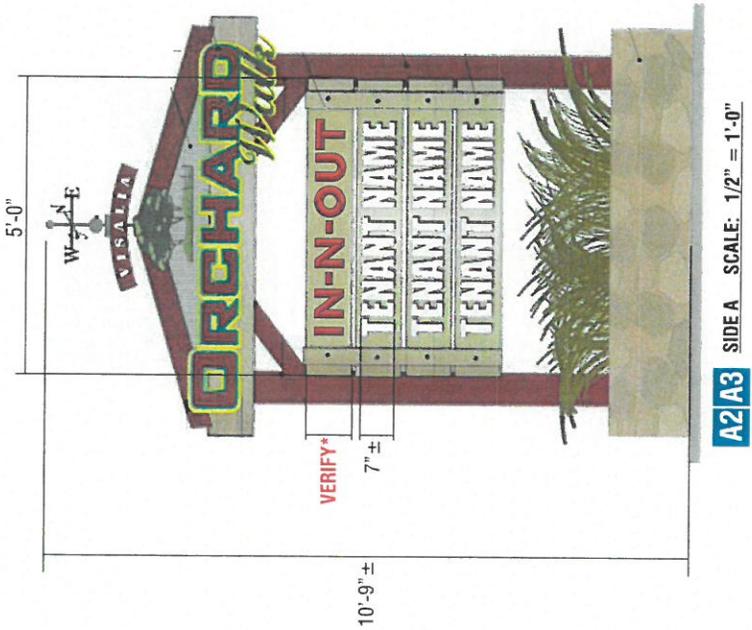
4444 Federal Blvd San Diego, CA 92102
(619) 527-6100 signitech.com

In-N-Out
NWC Dinuba Blvd. & Piggan Avenue, Visalia CA

Sales: BMC
Coordinator: Garry Wilcox
Design: TThomsen
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba_93291_1

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NOTES:
 -DRAWINGS CALL OUT TENANT PANEL HEIGHT AS 12" - SCALES TO 9 1/4"
 -VERIFY ALL DIMENSIONS
 -TENANT PANEL PLACEMENT MAY VARY



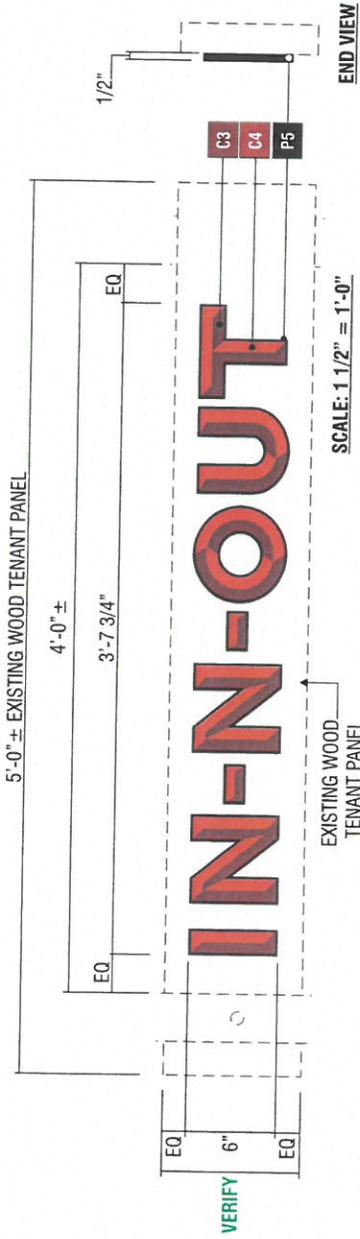
4444 Federal Blvd San Diego, CA 92102
 (619) 527-6100 signtech.com

In-N-Out
 NWC Dinuba Blvd. & Riggins Avenue, Visalia CA

Sales: BMC
 Coordinator: Garry Wilcox
 Design: TThomsen
 Engineering:

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date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote:
 project ID: IN-N-OUT_Dinuba 93291_1



A2/A3 SIGNTYPE INO-FCO-6
MANUFACTURE AND INSTALL FOUR (4) SETS OF FLAT CUT OUT LETTERS FOR EXISTING D/F MONUMENT SIGN
LETTERS : 1/2" ACRYLIC LETTERS - FACES & EDGES PAINTED BLACK
WITH FIRST SURFACE GRAPHICS DIGITALLY PRINTED ON WHITE OPAQUE VINYL
INSTALLATION: INSTALL ONE EXISTING WOOD TENANT PANEL WITH VHB TAPE & SILICONE

- COLORS**
- C3** DIGITALLY PRINTED COLORS TO MATCH PMS 187 C (3M #3630-73 "DARK RED" TRANSLUCENT VINYL)
 - C4** DIGITALLY PRINTED COLORS TO MATCH PMS 186 C (3M 3630-33 "RED" TRANSLUCENT VINYL)
- PAINT**
- P5** BLACK



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 (619) 527-6100 signtech.com

In-N-Out
 NWC Dinuba Blvd. & Riffin Avenue, Visalia CA

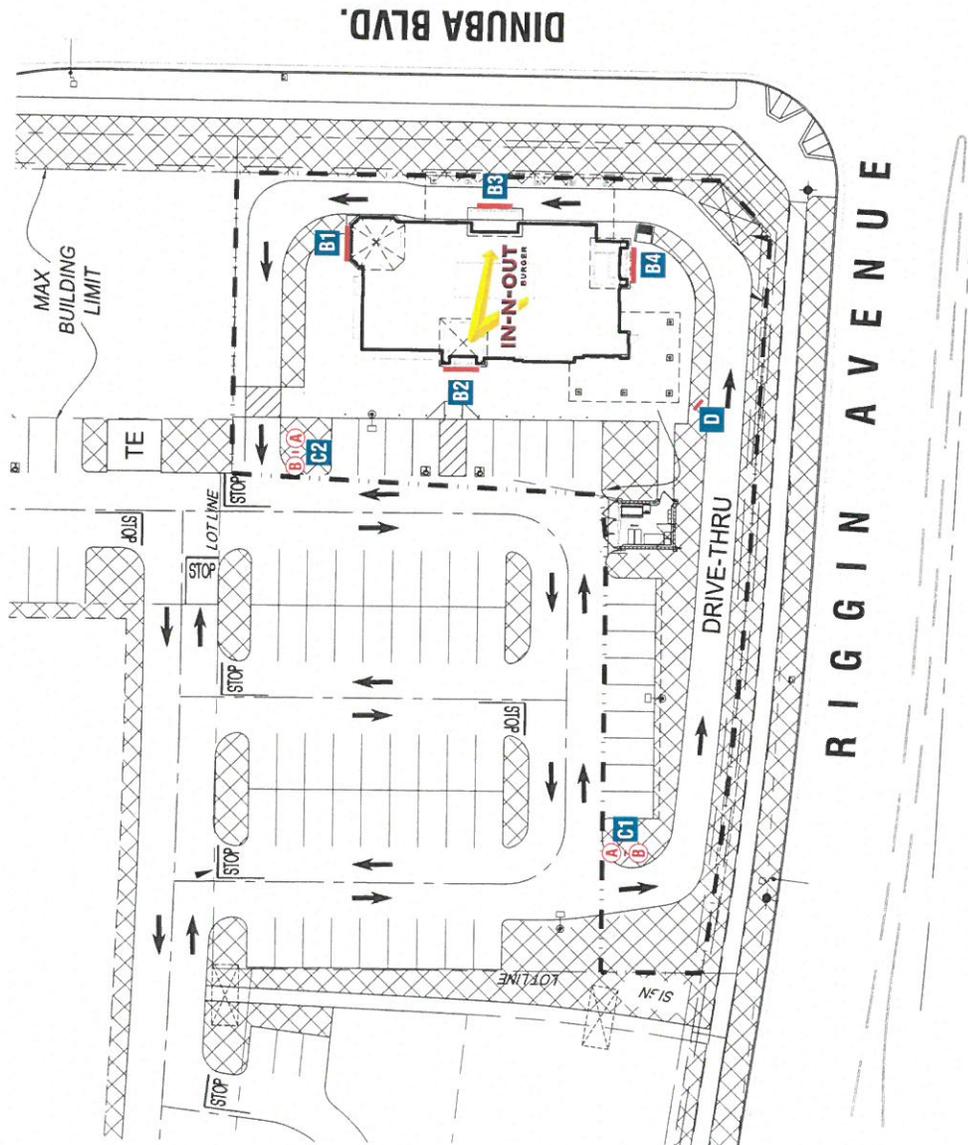
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Sales: BMC
 Coordinator: Garry Wilcox
 Designer: TThomsen
 Engineering:

date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote:
 project ID: IN-N-OUT_Dinuba 93291_1

Exhibit "G"

SIGN LEGEND				
SIGN #	DESCRIPTION	SIGN TYPE	SF	QTY
B1	WALL SIGN	INO-WS-72X141	55.9	1
B2	WALL SIGN	INO-WS-72X141	55.9	1
B3	WALL SIGN	INO-WS-72X141	55.9	1
B4	WALL SIGN	INO-WS-72X141	55.9	1
C1	DIRECTIONAL SIGN (DT)	INO-DIR-18x24x36	N/A	1
C2	DIRECTIONAL SIGN (TY/DNE)	INO-DIR-18x24x36	N/A	1
D	MENU BOARD	IN-MB-54x51x79	N/A	1
E	LED TUBE WALL ACCENT	INO-LED BORDER TUBE	N/A	1
F	ADDRESS NUMERALS	INO-CL-PL-12-ADDRESS-XXXX	N/A	1
G	INTERIOR NEON WALL SIGN	INO-OYCT-NEON-18x57	N/A	1



SITE PLAN SCALE: 1" = 40'-0"



4444 Federal Blvd San Diego, CA 92102
(619) 527-6100 signtech.com

In-N-Out
NWC Dinuba Blvd. & Riggan Avenue, Visalia CA

Sales: BMC
Coordinator: Garry Wilcox
Design: TThomsen
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba_93291_1

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NORTH - FRONT ELEVATION SCALE: 1/8" = 1'-0"



4444 Federal Blvd San Diego, CA 92102
(619) 527-6100 signtech.com

In-N-Out

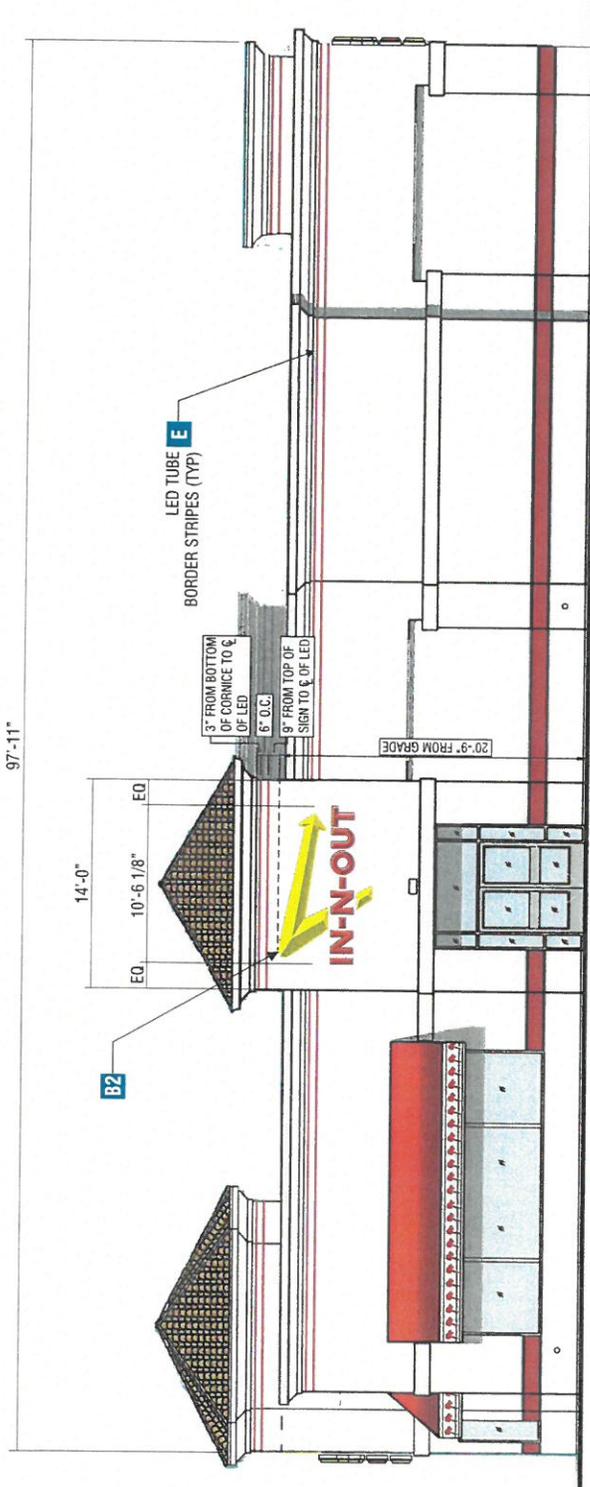
NWC Dinuba Blvd. & Riggins Avenue, Visalia CA

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Sales: BMC
Coordinator: Garry Wilcox
Designer: TThomsen
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba 93291_1

Exhibit "G"



WEST - RIGHT ELEVATION SCALE: 1/8" = 1'-0"



4444 Federal Blvd San Diego, CA 92102
 (619) 527-6100 signtech.com

In-N-Out

NWC Dinuba Blvd. & Regin Avenue, Visalia CA

Sales: BMC
 Coordinator: Garry Wilcox
 Design: TThomsen
 Engineering:

date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote:
 project ID: IN-N-OUT_Dinuba 93291_1

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EAST - DRIVE THRU ELEVATION SCALE: 1/8" = 1'-0"



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 (619) 527-6100 signtech.com

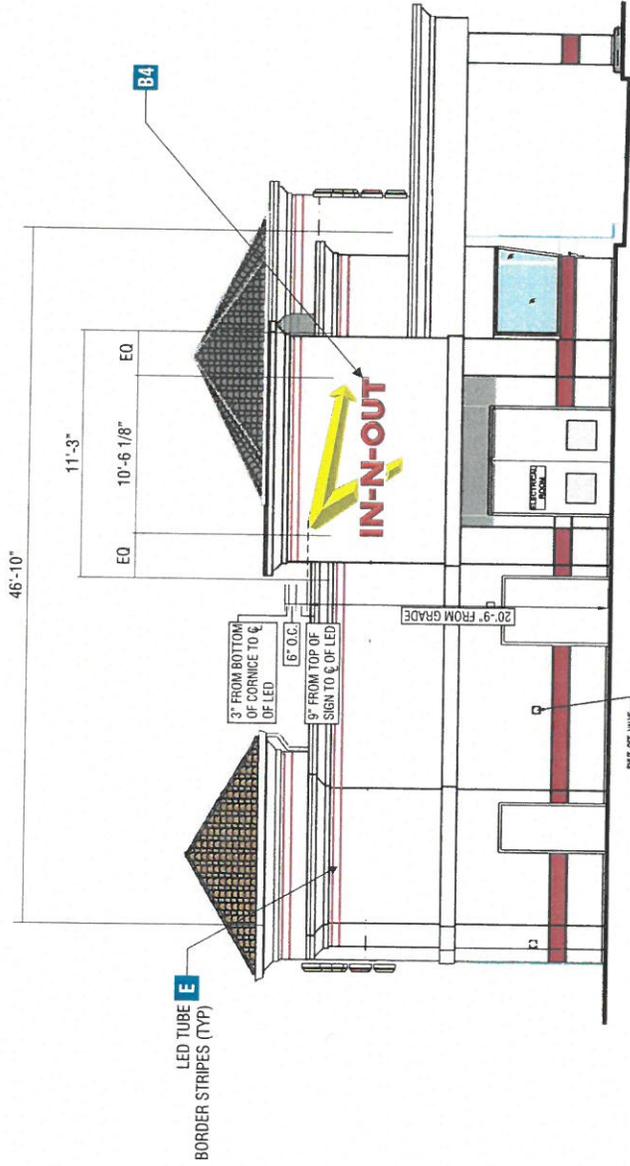
In-N-Out

NWC Dimuba Blvd. & Riggitt Avenue, Visalia CA

Sales: BMC
 Coordinator: Garry Wilcox
 Design: TThomsen
 Engineering:

date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote:
 project ID: IN-N-OUT_Dimuba 93291_1

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SOUTH - REAR ELEVATION SCALE: 1/8" = 1'-0"



4444 Federal Blvd San Diego, CA 92102
(619) 527-6100 signtech.com

In-N-Out

NWC Dinuba Blvd. & Riggim Avenue, Visalia CA

Sales: BMC
Coordinator: Garry Wilcox
Design: TTThomsen
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba 93291_1

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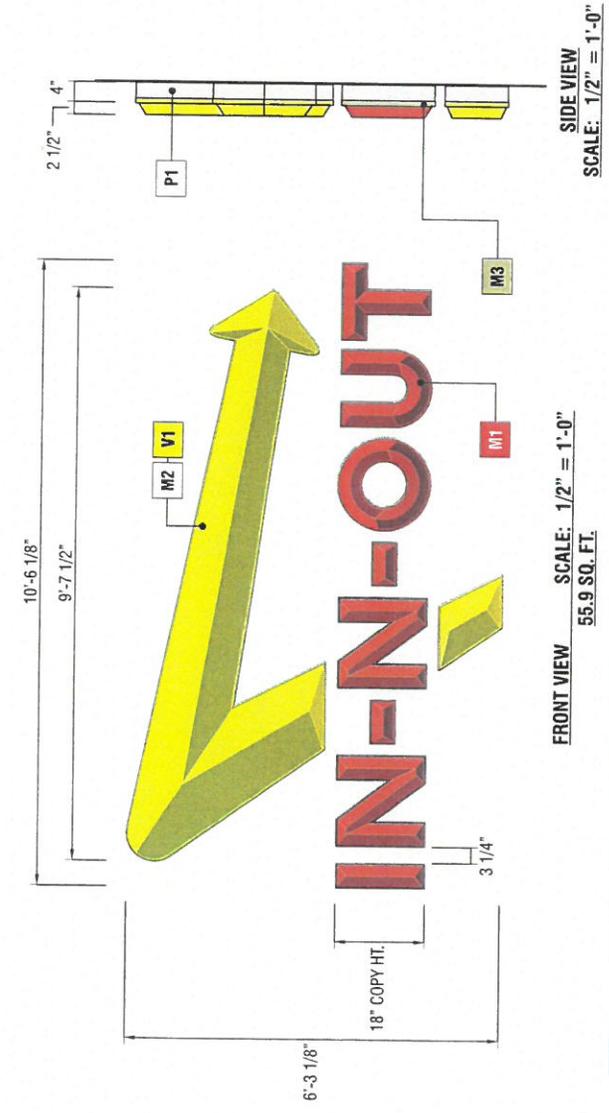
Exhibit "G"

NOTES:
 ALL LIGHTING COMPONENTS TO BE U.L. LISTED WITH DISCONNECT SWITCH @ POWER SUPPLY LOCATION (REMOTE). SIGNS PROVIDED WITH 3-WIRE 14 GAUGE JACK CABLE.

ARROW ILLUMINATION:
 GE TETRA® LED SYSTEMS - LED MODULES
 (2) GENERAL LED PS12-60W 120V/.08AMP POWER SUPPLY
 (TOTAL AMPS = 1.6)

COPY ILLUMINATION:
 SLOAN LED - LED MODULES
 (1) GENERAL LED PS12-60W 120V/.08AMP
 POWER SUPPLY

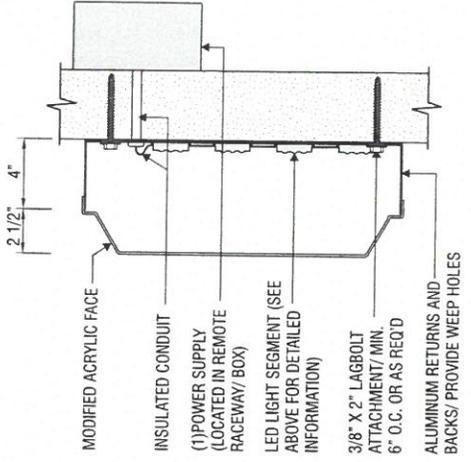
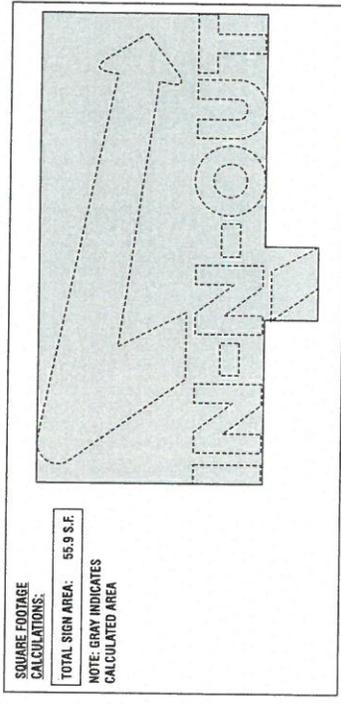
LIGHTING COMPONENT DETAIL SCALE: 1/2" = 1'-0"



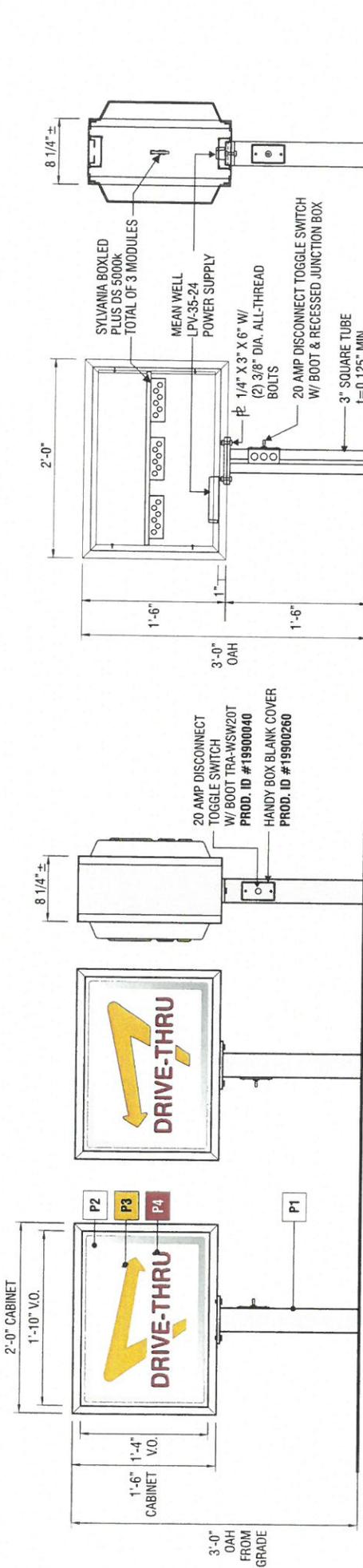
B1 B2 B3 B4 SIGNTYPE INO-WS-75x126

MANUFACTURE AND INSTALL FOUR (4) SINGLE-FACED INTERNALLY ILLUMINATED CHANNEL LETTER WALL SIGNS
ARROW: FABRICATED CHANNEL WITH FORMED CLEAR MODIFIED ACRYLIC FACE. LED ILLUMINATION.
COPY: FABRICATED CHANNEL WITH FORMED RED ACRYLIC FACE. LED ILLUMINATION.

- | | |
|------------------------------|--|
| MATERIALS | PAINT |
| M1 RED ACRYLIC #211-1 | P1 TO MATCH 'BONE CHINA' BY DUNN EDWARDS W/ SATIN FINISH |
| M2 CLEAR MODIFIED ACRYLIC | |
| M3 INO STANDARD GOLD TRIMCAP | VINYL |
| | V1 3M VINYL #3630-235 'AUTUMN YELLOW' |



SECTION DETAIL (NOT TO SCALE)



END VIEW
SCALE: 1" = 1'-0"

SIDE B
SCALE: 1" = 1'-0"

SIDE A
SCALE: 1" = 1'-0"

C1 SIGNTYPE INO-DIR-18x24x36

MANUFACTURE AND INSTALL ONE (1) DOUBLE-FACED INTERNALLY ILLUMINATED DIRECTIONAL SIGNS
FACES: PAN FORMED .093" CLEAR SG LEXAN WITH FORMED COPY & ARROW
CABINET: EXTRUDED ALUMINUM
POLE: 3" SQUARE TUBE, t=0.125" MIN.
ILLUMINATION: (3) DOUBLE SIDED SYLVANIA BOXLED PLUS 50000K MODULES (OR EQUIVALENT LUMENS AND COLOR TEMPERATURE) WITH MEANWELL LPV-35-24 POWER SUPPLY

PAINT

- P1** TO MATCH 'BONE CHINA' BY DUNN EDWARDS W/ SATIN FINISH
- P2** INO WHITE
- P3** TO MATCH 3M VINYL #3630-235 'AUTUMN YELLOW'
- P4** INO 443 RED / 25% CLEAR

ELEVATION DETAIL VIEW
SCALE: 1" = 1'-0"

SIDE DETAIL VIEW
SCALE: 1" = 1'-0"

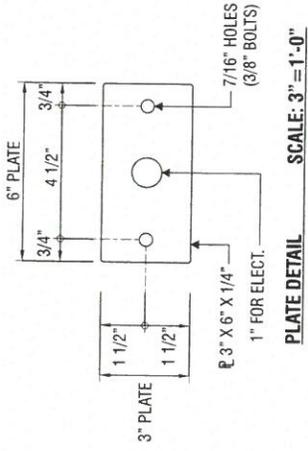


PLATE DETAIL
SCALE: 3" = 1'-0"



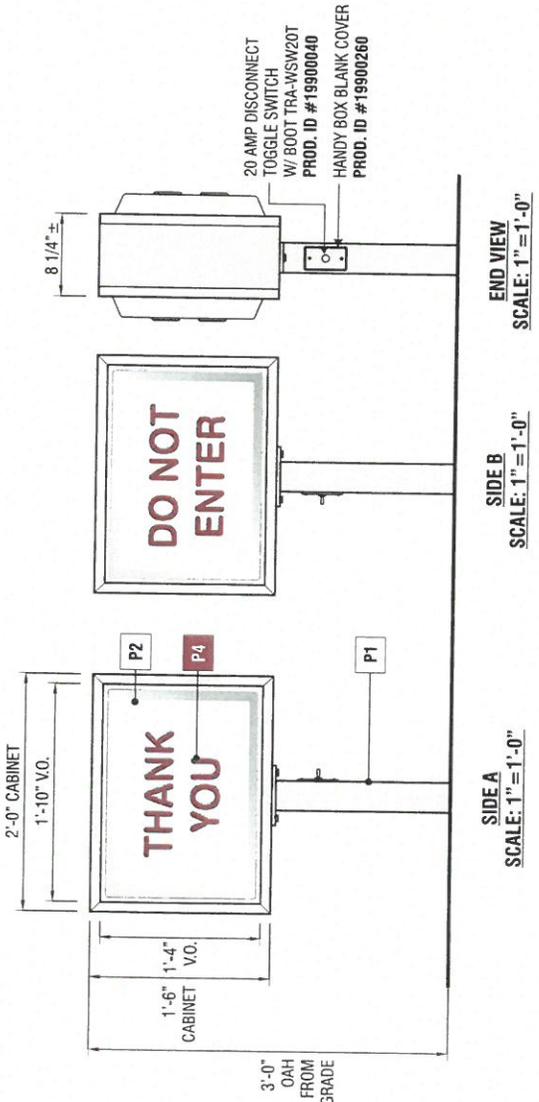
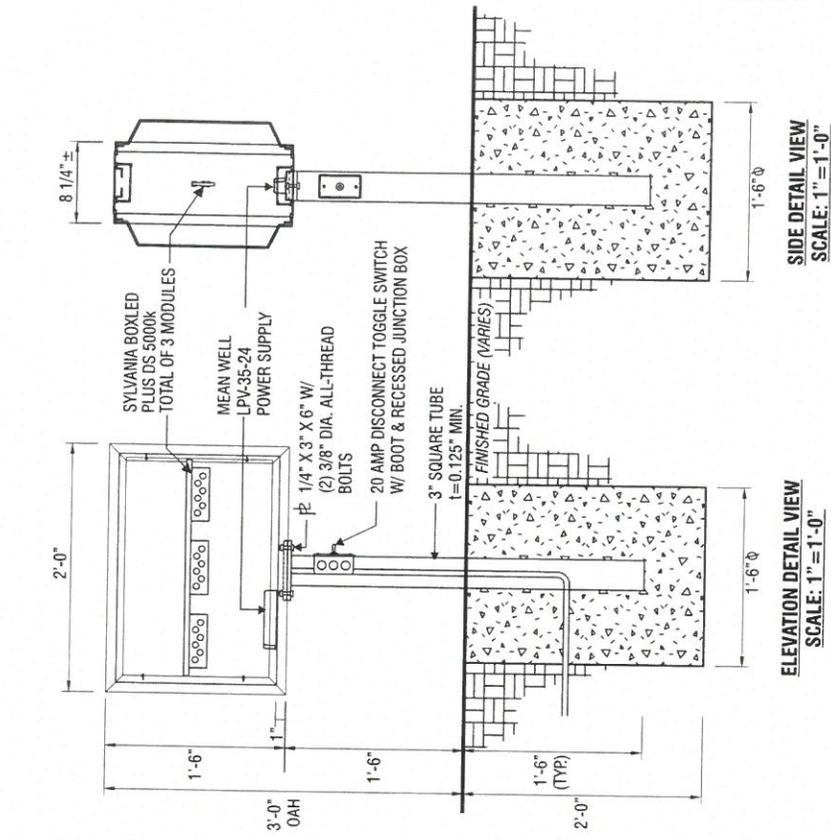
4444 Federal Blvd San Diego, CA 92102
(619) 527-6100 signitech.com

In-N-Out
NWC Dimuba Blvd. & Riggins Avenue, Visalia CA

Sales: BMC
Coordinator: Garry Wilcox
Design: TThomsen
Engineering:

date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dimuba 93291_1

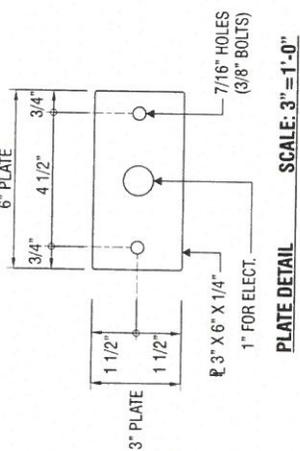
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C2 SIGNTYPE INO-DIR-18x24x36

MANUFACTURE AND INSTALL ONE (1) DOUBLE-FACED INTERNALLY ILLUMINATED DIRECTIONAL SIGN
 FACES: PAN FORMED .093" CLEAR SG LEXAN WITH SECOND SURFACE GRAPHICS
 CABINET: EXTRUDED ALUMINUM
 POLE: 3" SQUARE TUBE, t=0.125" MIN.
 ILLUMINATION: (3) DOUBLE SIDED SYLVANIA BOXED PLUS 5000K MODULES (OR EQUIVALENT LUMENS AND COLOR TEMPERATURE) WITH MEANWELL LPV-35-24 POWER SUPPLY

- PAINT**
- P1** TO MATCH 'BONE CHINA' BY DUNN EDWARDS W/ SATIN FINISH
 - P2** INO WHITE
 - P4** INO 443 RED / 25% CLEAR



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date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote: IN-N-OUT_Dimuba 93291_1
 project ID: IN-N-OUT_Dimuba 93291_1

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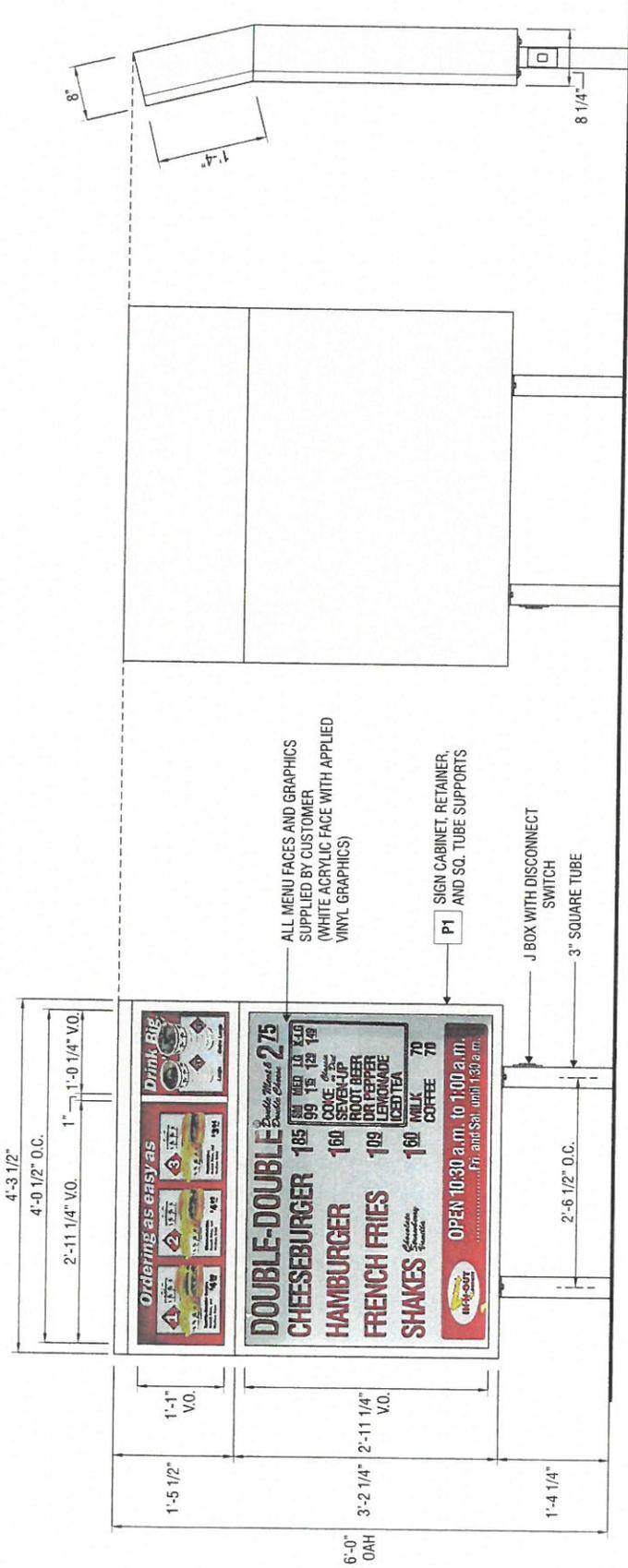


Exhibit "G"

D SIGNTYPE INO-MB-54x51x72

MANUFACTURE AND INSTALL ONE (1) SINGLE-FACED INTERNALLY ILLUMINATED MENU BOARD
 CABINET: EXTRUDED ALUMINUM W
 FACES: WHITE ACRYLIC WITH FIRST SURFACE APPLIED GRAPHICS (BY OTHERS)
 POLES: 3" SQUARE TUBE
 ILLUMINATION: SLOAN SIGN BOX II SINGLE SIDED 5000K LEDS

PAINT

P1 TO MATCH "BONE CHINA" SP 514 BY DUNN EDWARDS W/ SATIN FINISH

BACK VIEW SCALE: 3/4" = 1'-0"

SIDE VIEW SCALE: 3/4" = 1'-0"



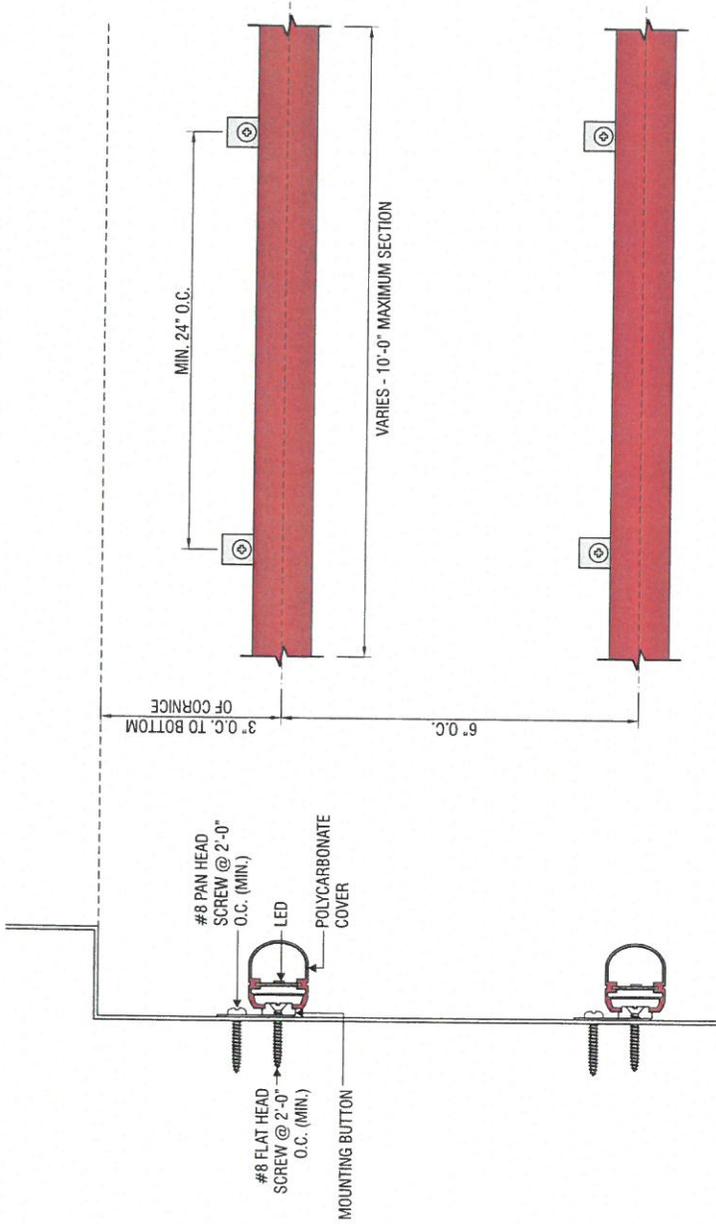
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 NWC Dimuba Blvd. & Riggan Avenue, Visalia CA

Sales: BMC
 Coordinator: Garry Wilcox
 Design: TThomsen
 Engineering:

date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote:
 project ID: IN-N-OUT_Dinuba 93291_1

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E SIGTYPE INO-LED BORDER TUBE

MANUFACTURE AND INSTALL LED TUBE BORDER STRIPES
 'SLOAN' LED RED LIGHTING SYSTEM. ALL ELECTRICAL COMPONENTS
 TO BE UL LISTED. ATTACH TO WALL AS REQUIRED.



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In-N-Out

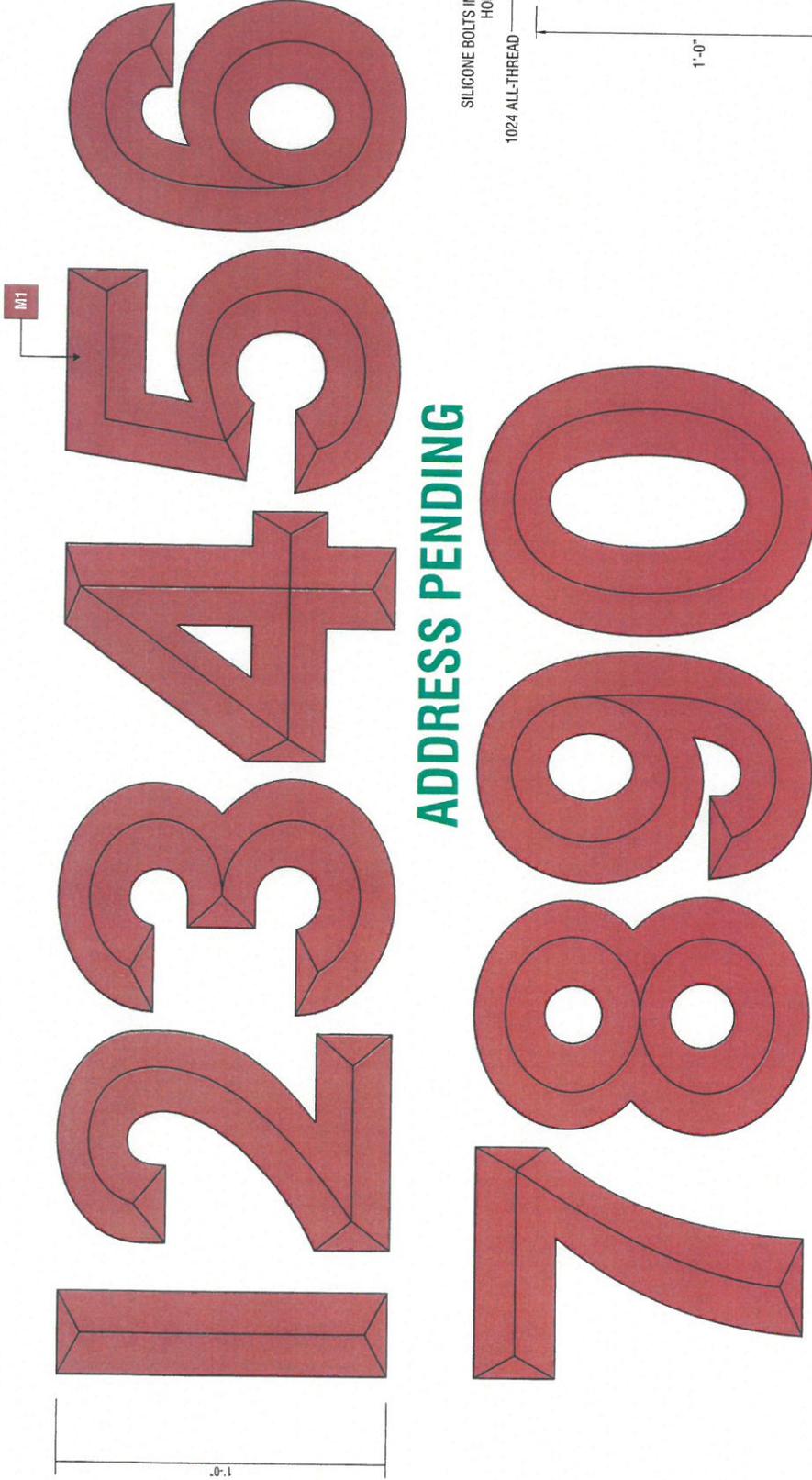
NWC Dinuba Blvd. & Riggins Avenue, Visalia CA

Sales: BMC
 Coordinator: Garry Wilcox
 Design: TThomsen
 Engineering:

date: 03/14/19
 drawing: 19-00727 rev: 7-10.17.19
 quote:
 project ID: IN-N-OUT_Dinuba 93291_1

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Exhibit "G"



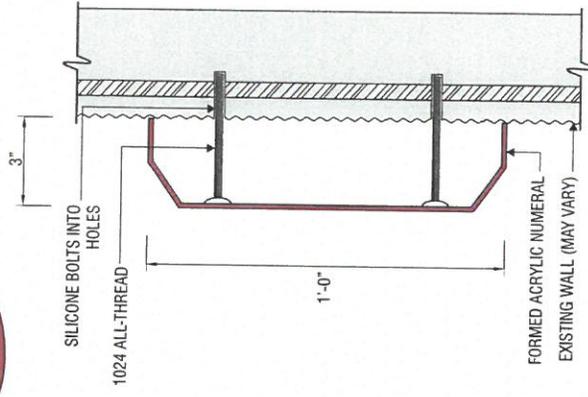
FRONT VIEW SCALE: 3" = 1'-0"

F SIGNTYPE INO-CL-PL-12-ADDRESS-XXXX

MANUFACTURE AND INSTALL ONE (1) SET OF NON-ILLUMINATED ADDRESS NUMERALS

MATERIAL

M1 RED ACRYLIC #211-1



INSTALLATION SECTION SCALE: 3" = 1'-0"



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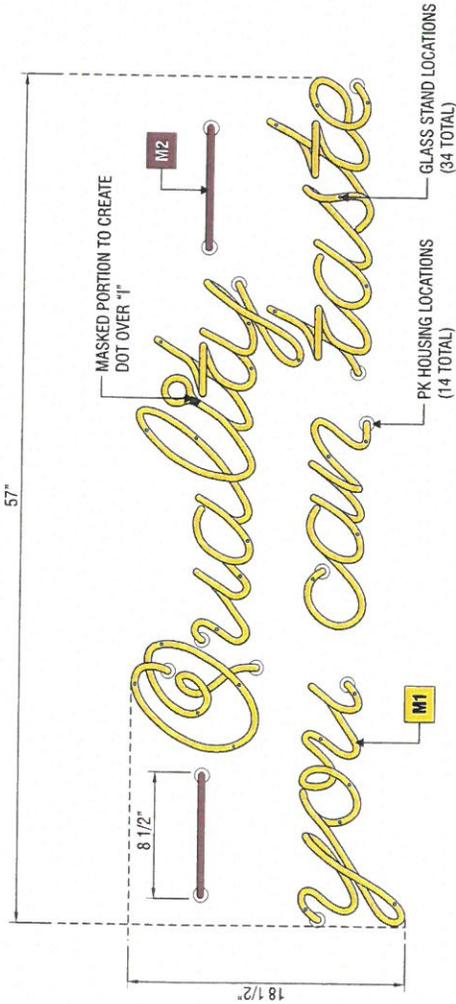
In-N-Out

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Sales: BMC
Coordinator: Garry Wilcox
Design: TThomsen
Engineering:

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drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba 93291_1



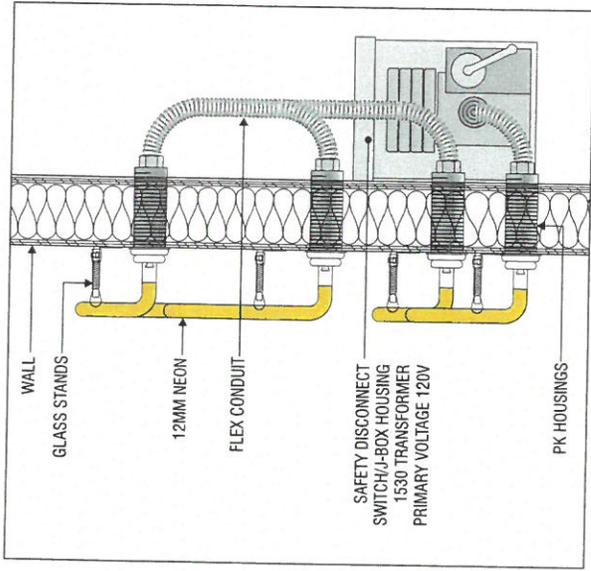
FRONT VIEW SCALE: 1 1/2" = 1'-0"

G SIGNTYPE INO-QYCT-NEON-18x57

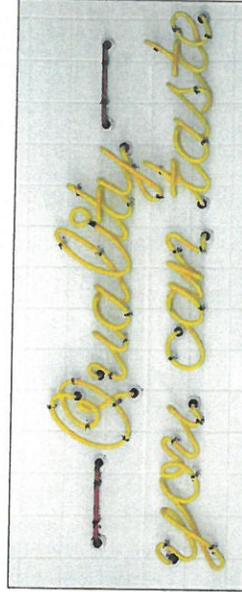
MANUFACTURE AND INSTALL ONE (1) INTERIOR EXPOSED NEON WALL SIGN

MATERIALS

- M1** EGL DOUBLE COATED 'NOVIOL GOLD' P73 EXPOSED NEON ILLUMINATION (12MM)
- M2** EGL DOUBLE COATED 'RUBY RED' EXPOSED NEON ILLUMINATION (12MM)



INSTALLATION SECTION SCALE: 3" = 1'-0"



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date: 03/14/19
drawing: 19-00727 rev: 7-10.17.19
quote:
project ID: IN-N-OUT_Dinuba 93291_1

Exhibit "I"

ACOUSTICAL ANALYSIS
ORCHARD WALK WEST
RIGGIN AVENUE & DINUBA BOULEVARD
VISALIA, CALIFORNIA

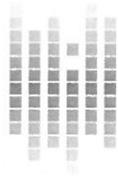
WJVA Project No. 19-037

PREPARED FOR

DONAHUE SCHRIBER REALTY GROUP
200 EAST BAKER STREET, SUITE 100
COSTA MESA, CA 92626

PREPARED BY

WJV ACOUSTICS, INC.
VISALIA, CALIFORNIA



wjv acoustics

AUGUST 29, 2019

INTRODUCTION

The project, Orchard Walk West, is a proposed commercial/retail development to be located at the northeast corner of Riggin Avenue and Dinuba Boulevard, in Visalia, California. The first phase of the development would include two drive-thru restaurants to be located within 250 feet of existing residential land uses. The residential land uses are located along the south side of Riggin Avenue. There is an existing 6-foot CMU block sound wall along all residential land uses along Riggin Avenue, in the vicinity of the proposed project. The City of Visalia has requested an acoustical analysis to determine if noise levels associated with drive-thru operations would exceed the City's applicable noise level standards.

This report is based upon the project site plan dated July 24, 2019, noise measurements obtained by WJV Acoustics, Inc. (WJVA) in the vicinity of the project site, reference noise measurements obtained at an existing drive-thru restaurant and information provided to WJVA by the project applicant concerning the proposed land uses and hours of operation. Revisions to the site plan or other project-related information available to WJVA at the time the analysis was prepared may require a reevaluation of the findings and/or recommendations of the report. The Project Site Plan is provided as Figure 1.

Appendix A provides definitions of the acoustical terminology used in this report. Unless otherwise stated, all sound levels reported in this analysis are A-weighted sound pressure levels in decibels (dB). A-weighting de-emphasizes the very low and very high frequencies of sound in a manner similar to the human ear. Most community noise standards utilize A-weighted sound levels, as they correlate well with public reaction to noise. Appendix B provides typical A-weighted sound levels for common noise sources.

CRITERIA FOR ACCEPTABLE NOISE EXPOSURE

The City of Visalia Noise Element of the General Plan (noise element) establishes noise level criteria in terms of the Day-Night Average Level (L_{dn}) metric, for transportation noise sources. The L_{dn} is the time-weighted energy average noise level for a 24-hour day, with a 10 dB penalty added to noise levels occurring during the nighttime hours (10:00 p.m.-7:00 a.m.). The L_{dn} represents cumulative exposure to noise over an extended period of time and is therefore calculated based upon *annual average* conditions.

The exterior noise level standard of the noise element is 65 dB L_{dn} for outdoor activity areas of residential uses. Outdoor activity areas generally include backyards of single-family residences and individual patios or decks and common outdoor activity areas of multi-family developments. The intent of the exterior noise level requirement is to provide an acceptable noise environment for outdoor activities and recreation.

The noise element also requires that interior noise levels attributable to exterior noise sources not exceed 45 dB L_{dn} . The intent of the interior noise level standard is to provide an acceptable noise environment for indoor communication and sleep.

Additionally, the noise element establishes hourly acoustical performance standards for non-transportation (stationary) noise sources. The standards are set in terms of the L_{eq} (hourly equivalent) and L_{max} (maximum) noise levels. The standards, provided in Table I, are made more restrictive during the nighttime hours of 10:00 p.m. to 7:00 a.m.

TABLE I			
NON-TRANSPORTATION NOISE LEVEL STANDARDS, dBA			
CITY OF VISALIA			
Daytime (7 a.m.-10 p.m.)		Nighttime (10 p.m.-7 a.m.)	
L_{eq}	L_{max}	L_{eq}	L_{max}
50	70	45	65

Source: City of Visalia Noise Element of General Plan

Section 8.36 of the City’s Municipal Code (noise ordinance) applies to noise sources that are not pre-empted from local control by existing state or federal regulations. Commercial activities are not pre-empted noise sources and are therefore subject to the provisions of the noise ordinance.

The noise ordinance addresses the statistical distribution of noise over time and allows for progressively shorter periods of exposure to levels of increasing loudness. Table II summarizes the exterior noise level standards of the ordinance. Note that the ordinance is to be applied during any one-hour time period of the day, and that the standards are 5 dB more restrictive between the hours of 7:00 p.m. and 6:00 a.m. Additionally, the noise ordinance states that the

noise standards provided in Table II shall be reduced by 5 dB for all noise sources consisting primarily of speech or music. This reduction of the standards would apply to noise levels associated with drive-thru loudspeaker operations.

TABLE II EXTERIOR NOISE LEVEL STANDARDS, dBA CITY OF VISALIA NOISE ORDINANCE			
Category	Cumulative # Min/Hr. (L _n)	Daytime ¹ (6am-7pm)	Nighttime ¹ (7pm-6am)
1	30 (L ₅₀)	50	45
2	15 (L ₂₅)	55	50
3	5 (L _{8.3})	60	55
4	1 (L _{1.7})	65	60
5	0 (L _{max})	70	65

Note: L_n is an abbreviation for the percentage of time that a certain noise level is exceeded during a specified time period (in this case, one hour). For example, an L₅₀ value of 50 dBA may not be exceeded during the hours of 6 am-7pm.

¹ Each of the noise level standards specified above shall be reduced by five dB for pure tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises.

Source: City of Visalia Municipal Code

The City's noise ordinance also establishes interior residential noise level standards that would apply to the project. The interior noise level standards are established in allowable exceedance limits over differing amounts of time, within residential land uses. Similar to the applicable exterior standards, the interior standards become 5 dB more restrictive during nighttime hours. The applicable interior noise level standards are provided in Table III.

TABLE III INTERIOR NOISE LEVEL STANDARDS, dBA CITY OF VISALIA NOISE ORDINANCE			
Category	Cumulative # Min/Hr.	Daytime (6am-7pm)	Nighttime (7pm-6am)
1	5	45	35
2	1	50	40
3	0	55	45

Source: City of Visalia Municipal Code

The City's noise ordinance also states *"In the event the measured ambient noise level without the alleged offensive source in operation exceeds an applicable noise level standard in any category above, the applicable standard or standards shall be adjusted so as to equal the ambient noise level"*.

PROJECT SITE NOISE EXPOSURE

The project site is currently an undeveloped lot located at the northeast corner of Dinuba Boulevard (SR 63) and Riggins Avenue. A site inspection and ambient noise monitoring were conducted on August 21st and 22nd, 2019, to evaluate the acoustical characteristics of the site and quantify existing ambient noise levels within the project area. The existing ambient noise environment is dominated by traffic noise associated with vehicles traveling on Dinuba Boulevard and Riggins Avenue. Additional sources of noise observed during the site inspection included noise associated with residential noise sources (HVAC, landscaping equipment, barking dogs) and retail/commercial activities to the west.

Ambient noise monitoring equipment consisted of a Larson-Davis Laboratories Model LDL 820 sound level analyzer equipped with a Bruel & Kjaer (B&K) Type 4176 ½" microphone. The monitor was calibrated with a B&K Type 4230 acoustical calibrator to ensure the accuracy of the measurements. The equipment complies with applicable specifications of the American National Standards Institute (ANSI) for Type 1 (precision) sound level meters.

WJVA collected 15-minute ambient noise level measurements at three (3) locations near the closest existing residential land uses to the proposed drive-thru operations. The noise monitoring sites were selected as they represent the closest off-site noise-sensitive receptors (residential land uses) to the proposed drive-thru operations. Three (3) individual noise measurements were collected at each site to document existing (without project) ambient noise levels during morning, midday and evening conditions. The findings of the noise measurements are provided in Table IV. The project vicinity and ambient noise monitoring site locations are provided as Figure 2.

TABLE IV

**SUMMARY OF SHORT-TERM AMBIENT NOISE MEASUREMENT DATA
ORCHARD WALK WEST, RIGGIN AVENUE & DINUBA BOULEVARD
AUGUST 21 & 22, 2019**

Site	Time	A-Weighted Decibels, dBA					
		L _{eq}	L ₅₀	L ₂₅	L ₈	L ₂	L _{max}
ST-1	7:40 a.m.	69.9	67.3	70.4	74.3	76.4	81.7
ST-1	11:45 a.m.	68.4	67.2	70.2	72.7	75.9	84.1
ST-1	7:20 p.m.	67.6	66.4	68.9	71.2	75.0	79.0
ST-2	8:00 a.m.	70.6	67.7	71.8	75.3	77.6	81.2
ST-2	12:05 p.m.	71.1	69.2	72.0	74.3	76.6	83.7
ST-2	7:40 p.m.	68.9	68.4	70.5	73.1	74.2	77.4
ST-3	8:20 a.m.	71.6	69.8	73.3	76.0	77.9	79.8
ST-3	12:25 p.m.	72.3	71.4	73.3	75.5	78.2	85.4
ST-3	8:00 p.m.	67.4	66.9	72.7	74.8	76.6	79.1

Source: WJV Acoustics, Inc.

From Table IV it can be determined that existing ambient (without project) noise levels at monitoring site ST-1 were in the range of approximately 68 to 70 dB L_{eq} with maximum noise levels in the range of 79 to 84 dB. Existing ambient noise levels at monitoring site ST-2 were in the range of approximately 69 to 71 dB L_{eq} with maximum noise levels in the range of approximately 77 to 84 dB. Existing ambient noise levels at monitoring site ST-3 were in the range of approximately 69 to 72 dB L_{eq} with maximum noise levels in the range of approximately 79 to 85 dB. Noise associated with vehicle traffic along Riggins Avenue (as well as Dinuba Boulevard at site ST-1) was the dominant noise source at all ambient noise monitoring sites during the measurement periods.

The noise levels described in Table IV exceed the City's exterior noise level standards for stationary noise sources in all statistical categories during each of the noise measurement periods. Such existing ambient noise levels would warrant an adjustment (increase) in the noise level standards described above.

PROJECT RELATED NOISE LEVELS

The project, Orchard Walk West, is a proposed commercial/retail development to be located at the northeast corner of Riggan Avenue and Dinuba Boulevard, in Visalia, California. The project would include two drive-thru restaurants located within 250 feet of existing residential land uses. Existing residential land uses are located approximately 150 feet south of the proposed drive-thru lanes. There is an existing 6-foot CMU block sound wall along all residential property lines at the residential land uses located along the south side of Riggan Avenue, in the vicinity of the proposed project.

According to the project applicant, In N Out Burger would occupy the drive-thru restaurant located adjacent to the corner of Riggan Avenue and Dinuba Boulevard, at Parcel 2. (see Figure 1). According to the project applicant, the hours of operation for the proposed In N Out Burger would be between 10:00 a.m. to 1:00 a.m. Sunday through Thursday and between 10:00 a.m. to 1:30 a.m. Friday and Saturday. At the time this analysis was prepared, the tenant and the hours of operation for the proposed drive-thru restaurant located at Parcel 3 were not known.

In order to assess potential project noise levels associated with drive-thru operations, WJVA utilized reference noise levels measured at a Wendy's drive-thru restaurant located on South Mooney Boulevard in Visalia. Measurements were conducted during the early afternoon of July 11, 2011 between 12:45 p.m. and 1:45 p.m. using the previously-described noise monitoring equipment.

The microphone used by customers to order food and the loudspeaker used by employees to confirm orders are both integrated into a menu board that is located a few feet from the drive-thru lane at the approximate height of a typical car window. Vehicles would enter the drive-thru lane from the west and then turn to the north along the east side of the restaurant.

Reference noise measurements were obtained at a distance of approximately 40 feet from the menu board containing the microphone/loudspeaker system at an angle of about 45° toward the rear of the vehicle being served. This provided a worst-case exposure to sound from the loudspeaker system since the vehicle was not located directly between the loudspeaker and measurement location. Cars were lined up in the access lane during the noise measurement period indicating that the drive-through lane was operating at or near a peak level of activity.

Each ordering cycle was observed to take approximately 60 seconds including vehicle movements. A typical ordering cycle included 5-10 seconds of loudspeaker use with typical maximum noise levels in the range of 60-62 dBA at the 40 foot-reference location. Vehicles moving through the drive-thru lane produced noise levels in the range of 55-60 dBA at the same distance. Vehicles parked at the ordering position (between the menu board and measurement site) were observed to provide significant acoustic shielding during the ordering sequence. The effects of such shielding are reflected by the noise measurement data.

As noted above, a typical drive-thru ordering cycle was observed to last about 60 seconds during peak levels of activity. This translates into about 60 cycles per hour. Assuming that loudspeaker use would average 5-10 seconds per cycle, also noted above, loudspeaker use would total 5-10 minutes per hour (or 8.3-16.7% of the time) during peak levels of activity. This corresponds to the L_{8.3} and L₂₅ statistical categories of the City’s noise ordinance. The nighttime L_{8.3} and L₂₅ standards for sources consisting primarily of speech or music are 50 and 45 dBA, respectively. Noise from idling vehicles is assumed to occur at least 50% of the time during peak levels of activity. This corresponds to a nighttime L₅₀ standard of 40 dBA.

The closest noise-sensitive receptors (residential land uses) to the project site are existing residential properties located to the south along Riggan Avenue. The closest residential property lines would be located approximately 150 feet from the drive-thru loudspeaker systems. As described above, there is an existing 6-foot CMU sound wall along all residential property lines along the south side of Riggan Avenue, in the vicinity of the proposed project. The existing sound wall would provide a minimum noise level reduction of 5 dB from noise sources along Riggan Avenue and within the proposed Orchard Walk West development, including drive-thru operations. Potential project-related noise exposure at that locations of the closest residential land uses was calculated based upon the above-described reference noise measurement data, the existing sound walls and the normal rate of sound attenuation over distance for a “point” noise source (6 dB/doubling of distance).

Table V summarizes project-related noise levels at the closest residential property lines to the proposed drive-thru operations, and compares the levels to applicable noise ordinance standards. The project would be expected to comply with the City’s noise ordinance in all categories during the daytime and nighttime hours.

TABLE V WORST-CASE NOISE EXPOSURE COMPARED TO NOISE ORDINANCE STANDARDS DRIVE-THRU OPERATIONS, ORCHARD WALK WEST							
Category	Cumulative Min/Hr. (L _n)	Daytime (6:00 a.m.-7:00 p.m.)			Nighttime (7:00 p.m.-6:00 a.m.)		
		Noise Standard ¹	Project Noise	Compliance	Noise Standard ¹	Project Noise	Compliance
1	30 (L ₅₀)	45	38	Yes	40	38	Yes
2	15 (L ₂₅)	50	43	Yes	45	43	Yes
3	5 (L _{8.3})	55	43	Yes	50	43	Yes
4	1 (L _{1.7})	60	45	Yes	55	45	Yes
5	0 (L _{max})	65	46	Yes	60	46	Yes

¹Standards adjusted (made 5 dB more restrictive) for noise sources consisting primarily of speech or music.

Source: WJV Acoustics, Inc.

It should be noted; existing ambient noise levels measured along the residential property lines closest to the proposed drive-thru operations (see Table IV) already exceed the applicable

ambient noise level standards in all statistical categories, without the project. Such elevated existing ambient noise levels would warrant an adjustment (increase) of the applicable noise levels standards. However, such an adjustment was not required (or applied with this analysis) to demonstrate project compliance with the City's exterior noise level standards. Noise mitigation would therefore not be required for compliance with the noise ordinance.

Compliance with the City's *noise element* is determined using the hourly L_{eq} and L_{max} noise level descriptors (non-transportation noise sources). The L_{eq} noise level descriptor represents the energy average noise level (average) over a one-hour period of time. The L_{eq} noise level descriptor is generally considered to be comparable (within 1-2 dB) of the L_{50} statistical noise level descriptor. Noise levels associated with drive-thru operations at the proposed Orchard Walk West retail development would not exceed the City's exterior noise level standards provided in the General Plan Noise Element. Noise mitigation would therefore not be required for compliance with the General Plan Noise Element.

CONCLUSIONS AND RECOMMENDATIONS

The proposed drive-thru restaurants at the proposed Orchard Walk West commercial/retail development will comply with applicable City of Visalia exterior and interior noise level requirements without additional mitigation. Additionally, noise levels associated with drive-thru restaurant operations would not exceed existing (without project) noise levels at nearby noise-sensitive residential land uses.

The conclusions and recommendations of this acoustical analysis are based upon the best information known to WJV Acoustics, Inc. (WJVA) at the time the analysis was prepared concerning the proposed site plan, noise levels produced by similar drive-thru loudspeaker systems and hours of peak project operations. Any significant changes in these factors will require a reevaluation of the findings of this report. Additionally, any significant future changes in drive-thru sound system technology, noise regulations or other factors beyond WJVA's control may result in long-term noise results different from those described by this analysis.

Respectfully submitted,



Walter J. Van Groningen
President

WJV:wjv

FIGURE 1: PROJECT SITE PLAN

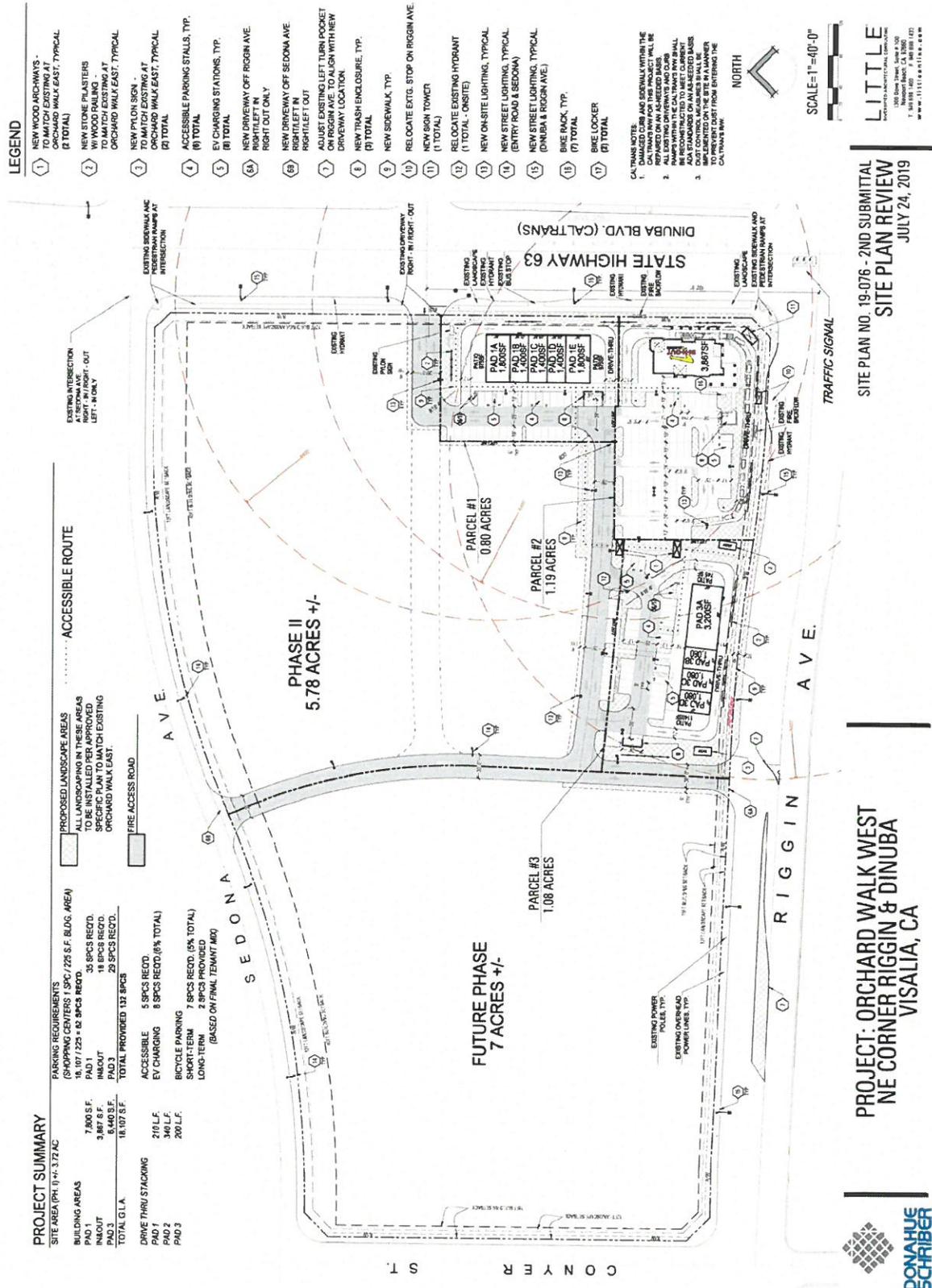


FIGURE 2: PROJECT SITE VICINITY AND NOISE MONITORING SITE LOCATIONS



APPENDIX A

ACOUSTICAL TERMINOLOGY

AMBIENT NOISE LEVEL: The composite of noise from all sources near and far. In this context, the ambient noise level constitutes the normal or existing level of environmental noise at a given location.

CNEL: Community Noise Equivalent Level. The average equivalent sound level during a 24-hour day, obtained after addition of approximately five decibels to sound levels in the evening from 7:00 p.m. to 10:00 p.m. and ten decibels to sound levels in the night before 7:00 a.m. and after 10:00 p.m.

DECIBEL, dB: A unit for describing the amplitude of sound, equal to 20 times the logarithm to the base 10 of the ratio of the pressure of the sound measured to the reference pressure, which is 20 micropascals (20 micronewtons per square meter).

DNL/L_{dn}: Day/Night Average Sound Level. The average equivalent sound level during a 24-hour day, obtained after addition of ten decibels to sound levels in the night after 10:00 p.m. and before 7:00 a.m.

L_{eq}: Equivalent Sound Level. The sound level containing the same total energy as a time varying signal over a given sample period. L_{eq} is typically computed over 1, 8 and 24-hour sample periods.

NOTE: The CNEL and DNL represent daily levels of noise exposure averaged on an annual basis, while L_{eq} represents the average noise exposure for a shorter time period, typically one hour.

L_{max}: The maximum noise level recorded during a noise event.

L_n: The sound level exceeded "n" percent of the time during a sample interval (L₉₀, L₅₀, L₁₀, etc.). For example, L₁₀ equals the level exceeded 10 percent of the time.

A-2

ACOUSTICAL TERMINOLOGY

NOISE EXPOSURE CONTOURS:

Lines drawn about a noise source indicating constant levels of noise exposure. CNEL and DNL contours are frequently utilized to describe community exposure to noise.

NOISE LEVEL REDUCTION (NLR):

The noise reduction between indoor and outdoor environments or between two rooms that is the numerical difference, in decibels, of the average sound pressure levels in those areas or rooms. A measurement of Noise level reduction combines the effect of the transmission loss performance of the structure plus the effect of acoustic absorption present in the receiving room.

SEL or SENEL:

Sound Exposure Level or Single Event Noise Exposure Level. The level of noise accumulated during a single noise event, such as an aircraft overflight, with reference to a duration of one second. More specifically, it is the time-integrated A-weighted squared sound pressure for a stated time interval or event, based on a reference pressure of 20 micropascals and a reference duration of one second.

SOUND LEVEL:

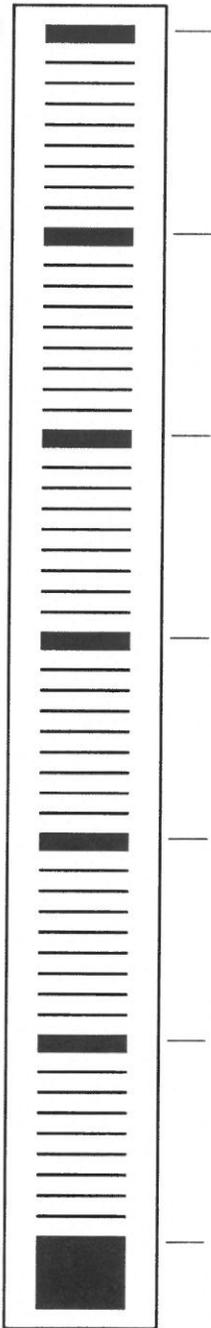
The sound pressure level in decibels as measured on a sound level meter using the A-weighting filter network. The A-weighting filter de-emphasizes the very low and very high frequency components of the sound in a manner similar to the response of the human ear and gives good correlation with subjective reactions to noise.

SOUND TRANSMISSION CLASS (STC):

The single-number rating of sound transmission loss for a construction element (window, door, etc.) over a frequency range where speech intelligibility largely occurs.

APPENDIX B
EXAMPLES OF SOUND LEVELS

NOISE SOURCE	SOUND LEVEL	SUBJECTIVE DESCRIPTION
AMPLIFIED ROCK 'N ROLL ▶	120 dB	DEAFENING
JET TAKEOFF @ 200 FT ▶		
	100 dB	VERY LOUD
BUSY URBAN STREET ▶		
	80 dB	LOUD
FREEWAY TRAFFIC @ 50 FT ▶		
	60 dB	MODERATE
CONVERSATION @ 6 FT ▶		
TYPICAL OFFICE INTERIOR ▶		FAINT
SOFT RADIO MUSIC ▶	40 dB	
RESIDENTIAL INTERIOR ▶		VERY FAINT
WHISPER @ 6 FT ▶	20 dB	
HUMAN BREATHING ▶	0 dB	





#1

MEETING DATE: July 24, 2019
SITE PLAN NO. 19-076 Resubmittal
PARCEL MAP NO.
SUBDIVISION:
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

- RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.
- During site plan design/policy concerns were identified, schedule a meeting with
- Planning Engineering prior to resubmittal plans for Site Plan Review.
- Solid Waste Parks and Recreation Fire Dept.

REVISE AND PROCEED (see below)

- A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.
- Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.
- Your plans must be reviewed by:
- CITY COUNCIL REDEVELOPMENT
- PLANNING COMMISSION PARK/RECREATION
- CUP
- HISTORIC PRESERVATION OTHER: _____

ADDITIONAL COMMENTS :

If you have any questions or comments, please call (559) 713-4444.

Site Plan Review Committee



**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

- Adrian Rubalcaba 713-4271
- Diego Corvera 713-4209
- Lupe Garcia 713-4197

ITEM NO: 1 DATE: July 24, 2019

SITE PLAN NO.: 19-076R
PROJECT TITLE: ORCHARD WALK WEST - PHASE 1
DESCRIPTION: MULTI-PAD COMMERCIAL DEVELOPMENT W/
THREE PROPOSED BUILDINGS
APPLICANT: DONAHUE SCHRIBER REALTY GROUP
PROP OWNER: DONAHUE SCHRIBER REALTY GROUP
LOCATION: NWC DINUBA & RIGGIN
APN: 078-120-030, 032

SITE PLAN REVIEW COMMENTS

- REQUIREMENTS (indicated by checked boxes)
- Install curb return with ramp, with _____ radius;
- Install curb; gutter **ONSITE AS NEEDED**
- Drive approach size: Use radius return; **REFER TO CITY COMMERCIAL STANDARDS**
- Sidewalk: **6'** width; **10'** parkway width at **RIGGIN AVE**
- Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- Right-of-way dedication required. A title report is required for verification of ownership.
- Deed required prior to issuing building permit;
- City Encroachment Permit Required. **FOR ALL WORK IN THE PUBLIC RIGHT-OF-WAY**
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.
- CalTrans Encroachment Permit required. CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088; **WORK WITHIN DINUBA BLVD RIGHT-OF-WAY**
- Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.
- Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. Prepared by registered civil engineer or project architect. All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) directed to the City's existing storm drainage system; b) directed to a permanent on-site basin; or c) directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: _____ : _____ maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
- Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
- Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)
- Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- Traffic indexes per city standards:

- Install street striping as required by the City Engineer.
 - Install landscape curbing (typical at parking lot planters).
 - Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
 - Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
 - Provide "R" value tests: each at
 - Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
 - Access required on ditch bank, 15' minimum Provide wide riparian dedication from top of bank.
 - Show Valley Oak trees with drip lines and adjacent grade elevations. Protect Valley Oak trees during construction in accordance with City requirements.
 - A permit is required to remove Valley Oak trees. Contact Public Works Admin at 713-4428 for a Valley Oak tree evaluation or permit to remove. A pre-construction conference is required.
 - Relocate existing utility poles and/or facilities.
 - Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
 - Subject to existing Reimbursement Agreement to reimburse prior developer:
 - Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
 - If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
 - If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- Comply with prior comments. Resubmit with additional information. Redesign required.

Additional Comments:

- 1. Proposed commercial development is located on a partially developed site. City records indicate the on & off-site improvement permits have not received a final or expired therefore not accepted by the City. Outstanding Encroachment permit # E070278 "Offsite Improvements for Orchard Walk" shall be rectified prior to proposed phase one development. Outstanding Building Permit # B071362 shall be rectified prior to proposed phase one development. Further coordinate with the City Engineer.***
- 2. All installed public infrastructure and hardscape improvements will need to be re-tested, reconditioned, etc. as further determined by the City Engineer. All items identified by City Engineer requiring repair, retest, reconditioning, etc. shall be completed prior to final acceptance of the public improvements and prior to permit final for new phase one improvement.***
- 3. Place drive approach location on Sedona per City standard C-32 if intended to be a permanent drive approach. If drive approach is temporary, removal and replacement with curb and gutter will be required and future Drive approach will need to be to current standards for location. Barricading of unused drive approaches along perimeter of Conyer and Sedona will be required.***
- 4. Sedona and Conyer local streets are currently fenced-off/barricaded due to the previously unfinished development. Completion of the outstanding improvements will be required on Sedona in order to utilize the local street circulation. This includes curb returns at SE and NE corner of Sedona and Conyer. Additional coordination with City Engineer is required to establish an acceptable plan for proposed phase one project.***
- 5. New improvements shall comply with current City standards - improvements not yet constructed per the original approved plans shall be updated & installed accordingly.***
- 6. Refer to requirements by Traffic Safety Dept.***

- 7. If a new parcel map is desired, a separate Site Plan submittal for a tentative parcel map would be required. If a Lot Line Adjustment is desired, Site Plan shall indicate which lot lines are proposed to be adjusted.*
- 8. All refuse enclosures shall comply with current Solid Waste standards. Enclosures shall be located and positioned for a direct stab service and accommodate vehicle maneuverability onsite. 24' refuse enclosures with concrete apron and metal gates are required. As proposed, revisions to the site plan are required to comply to City standards coordinate with Solid Waste Department. Contact Edward Zuniga 713-4338. provide written approval for alternate layout with plans submittal.*
- 9. Meet City standard parking PK-1 through PK-5.*
- 10. Site plan indicates a new bus stop turnout on Riggin however there is currently one existing. Completion of the adjacent bus pad to current City standards would be required.*
- 11. The previous site design was approved with two access points (drive approaches) on Riggin Ave. The proposed new drive approach location appears to comply with current City standards however an additional approach in the future, if desired, may not be supported given the minimum distance requirements between access points and adjacent intersections per City arterial standards. The new left turn pocket location will require demolition of the existing and additional median improvements. The median landscaping shall be relocated/replanted accordingly. Refer to City arterial left turn pocket standards. Be advised the proposed new drive approach location is directly adjacent to an existing power pole and bus turnout. This will have an adverse affect on line of sight distance for exiting traffic. Development plan shall address this issue.*
- 12. City owns and maintains new street lighting. Project shall install street lights along Dinuba, Riggin, Conyer, and Sedona. Per the Shannon Ranch development standards, decorative "sun valley" lighting is to be used on Conyer and Sedona streets. Lighting along Dinuba and Riggin shall comply with standard street lighting. Refer to City Street Lighting standards for arterials, collectors, and local streets. Refer to "Ornamental" standards for Sedona.*
- 13. All improvements within right-of-way along Dinuba will be subject to Caltrans jurisdiction and requirements.*
- 14. Proposed development will incur impact fees associated with site improvement and building construction. Refer to page 4 for applicable fees and estimate.*

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: **19-076**
Date: **7/24/2019**

Summary of applicable Development Impact Fees to be collected at the time of building permit:
(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

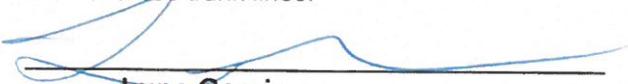
(Fee Schedule Date:**8/3/2018**)
(Project type for fee rates:**FAST FOOD/RETAIL**)

Existing uses may qualify for credits on Development Impact Fees.

FEE ITEM	FEE RATE
<input type="checkbox"/> Groundwater Overdraft Mitigation Fee	
<input checked="" type="checkbox"/> Transportation Impact Fee	\$17,174/1000sf x 16.75 = \$287,665
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	Fast Food \$4,535/each, Retail \$25/1000sf Treatment Plant Fee: Fast Food \$19,888/each, Retail \$55/1000sf Restaurant TBD
<input type="checkbox"/> Sewer Front Foot Fee	
<input type="checkbox"/> Storm Drain Acq/Dev Fee	
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input type="checkbox"/> Waterways Acquisition Fee	
<input type="checkbox"/> Public Safety Impact Fee: Police	
<input type="checkbox"/> Public Safety Impact Fee: Fire	
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$333/1000sf x 16.75 = \$5,578
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.


Lupe Garcia

SITE PLAN REVIEW COMMENTS

Paul Scheibel, Planning Division, 559-713-4369

Date: July 24, 2019

SITE PLAN NO: 2019-076 - B
PROJECT: Orchard Walk West – Phase 1
DESCRIPTION: MULTI-PAD COMMERCIAL DEVELOPMENT WITH THREE PROPOSED BUILDINGS, OFF-STREET PARKING, DINING, SHOPS, WALKWAYS AND LANDSCAPING
APPLICANT: DONAHUE SCHRIBER REALTY GROUP
PROP. OWNER: DONAHUE SCHRIBER REALTY GROUP
LOCATION TITLE: NW CORNER INTERSECTION RIGGIN AVE AND DINUBA
APN TITLE: 078-120-030
GENERAL PLAN: Commercial Mixed Use
ZONING: C-MU (Mixed Use Commercial)

Rule 9510 – This project may be subject to the Rule 9510 requirements of the San Joaquin Valley Air Pollution Control District – see District web-site for information.

Planning Division Recommendation:

- Revise and Proceed
 Resubmit

Project Requirements

- CUP for proximity to residences on south side of Riggan Ave.
- Parcel Map
- Finding of Consistency with the Orchard Walk SP, or amendment to the SP
- Additional information as needed- Noise study for drive-thru lanes within 250 feet of residences (ZO sect. 17.32.162)

PROJECT SPECIFIC INFORMATION: July 24, 2019

1. Demonstrate general conformance with Orchard Walk Specific Plan, including architectural theme
2. Trash Enclosure at Pad 1 does not meet City standards for truck access.
3. Eliminate parking spaces that back onto drive thru lanes and their access drives.
4. Recommend architecturally compatible trellis covers on patio areas.
5. Show vehicle queuing for the drive-thrus on Pads 1 and 3.

PROJECT SPECIFIC INFORMATION: April 24, 2019

1. Demonstrate general conformance with Orchard Walk Specific Plan
2. Trash Enclosure at Pad 1 does not meet City standards for truck access.
3. Eliminate parking spaces that back onto drive thru lanes and their access drives.
4. Recommend architecturally compatible trellis covers on patio areas.

17.19.060 Development standards in the C-MU zones outside the downtown area.

The following development standards shall apply to property located in the C-MU zone and located outside the Downtown Area, which is defined as the area that is south of Murray Avenue, west of Ben Maddox Way, north of Mineral King Avenue, and east of Conyer Street:

- A. Minimum site area: five (5) acres.
- B. Maximum building height: fifty (50) feet.
- C. Minimum required yards (building setbacks):
 1. Front: fifteen (15) feet;

2. Rear: zero (0) feet;
3. Rear yards abutting an R-1 or R-M zone district: fifteen (15) feet;
4. Side: zero (0) feet;
5. Side yards abutting an R-1 or R-M zone district: fifteen (15) feet;
6. Street side yard on corner lot: ten (10) feet.

D. Minimum required landscaped yard (setback) areas:

1. Front: fifteen (15) feet;
2. Rear: five (5) feet;
3. Rear yards abutting an R-1 or R-M zone district: five (5) feet;
4. Side: five (5) feet (except where a building is located on side property line);
5. Side yards abutting an R-1 or R-M zone district: five (5) feet;
6. Street side on corner lot: ten (10) feet.

The provisions of Chapter 17.58 shall also be met, if applicable.

Parking:

1. Provide parking spaces based Zoning Ordinance Section 17.34.020
2. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot.
3. Provide handicapped space(s).
4. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking.
5. A planter is required every other row. (5-9 feet in width containing trees on twenty (20) foot centers.
6. No repair work or vehicle servicing allowed in a parking area.
7. It is highly recommended that bicycle rack(s) be provided on site plan.
8. No parking shall be permitted in a required front/rear/side yard.
9. Design/locate parking lot lighting to deflect any glare away from abutting residential areas.
10. Parking lot to be screened from view by a 3-foot tall solid wall or shrubs when located adjacent to a public street or when across from residential property.
11. Front carport area to have a 3 to 6-foot tall screening wall.
12. Provide shopping cart storage areas on site plan.
13. Provide transit facilities on site plan.
14. Provide shared parking/access agreements
15. Provide off-street loading facility.
16. The project should provide preferential parking spaces for carpools and vanpools to decrease the number of single occupant vehicle work trips. The preferential treatment could include covered parking spaces or close-in parking spaces, or designated free parking, or a guaranteed space for the vehicle.
17. Provide a "No Parking" (dead-head) stall at the end of the parking row (for rows over 6 stalls deep with no outlet) to allow vehicles to turn around rather than backing out if no stalls are available.

Fencing and Screening:

1. Provide screening for roof mounted equipment (Zoning Ordinance Section 17.30.130.F).
2. Provide second-story screening for all windows that may intrude into adjacent residential properties. Details and cross-sections will be required to be reviewed and approved prior to issuance of building permits (Zoning Ordinance Section 17.30.130.F).
3. Provide screened trash enclosure with solid screening gates (Zoning Ordinance Section 17.30.130.F).
4. Provide solid screening of all outdoor storage areas. Outdoor storage to be screened from public view with solid material (Zoning Ordinance Section 17.30.130.F).

5. Outdoor retail sales prohibited.
6. Cross Sections need to be provided for site Plan Review if there is greater than an 18-inch difference between the elevation of the subject site and the adjacent properties, and the sections would be required for the public hearing process also.
7. All outdoor storage areas are to be identified on the site plan and they are to be shown with screening (fencing). No materials may be stored above the storage area fence heights (Zoning Ordinance Section 17.30.130.F).
8. Provide minimum of _____-foot high concrete block wall or masonry wall along/around the following: _____
9. Provide minimum of _____-foot high solid wooden residential fence along/around the following: _____
10. Provide minimum of _____-foot high chain-link fence _____ with _____ without slats along/around the following: _____
11. If there is an anticipated grade difference of more than 12-inches between this site and the adjacent sites, a cross section of the difference and the walls must be provided as a part of the Subdivision and/or CUP application package.
12. NOTE: The maximum height of block walls and fences is 7-feet in the appropriate areas; this height is measured on the tallest side of the fence. If the height difference is such that the fence on the inside of the project site is not of sufficient height, the fence height should be discussed with Planning Staff prior to the filing of applications to determine if an Exception to fence/wall height should also be submitted.

Landscaping:

1. The City has adopted the State Water Efficient Landscape Ordinance. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance. **NOTE: Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.**
2. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.015-2).
3. In the P(R-M) multi-family residential zone, all multiple family developments shall have landscaping including plants, and ground cover to be consistent with surrounding landscaping in the vicinity. Landscape plans to be approved by city staff prior to installation and occupancy of use and such landscaping to be permanently maintained. (Zoning Ordinance Section 17.16.180)
4. All landscape areas to be protected with 6-inch concrete curbs (Zoning Ordinance Section 17.30.130.F).
5. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months.
6. Provide a detailed landscape and irrigation plan as a part of the building permit package (Zoning Ordinance Section 17.34.040).
7. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.30.130.C).
8. Provide a detailed landscape and irrigation plan for review prior to issuance of building permits. Please review Zoning Ordinance section 17.30.130-C for current landscaping and irrigation requirements.
9. Provide a conceptual landscape plan for resubmittal or planning commission review.
10. Locate existing oak trees on site and provide protection for all oak trees greater than 2" diameter (see Oak Tree Preservation Ordinance).
11. Maintenance of landscaped areas. - A landscaped area provided in compliance with the regulations prescribed in this title or as a condition of a use permit or variance shall be planted with materials

suitable for screening or ornamenting the site, whichever is appropriate, and plant materials shall be maintained and replaced as needed, to screen or ornament the site. (Prior code § 7484)

Lighting:

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.
2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

Drive-thru Performance Standards (Section 17.32.162)

- A. Purpose and Intent: It is the purpose of this section to specify performance standards applicable to uses that seek to incorporate a drive-thru lane in associate with specified use. This section does not apply to carwashes and lube and oil changing stations.
- B. Performance Standards:
 1. Separation from residences: The drive-thru lane shall be no less than 250 feet from the nearest residence or residentially zoned property.
 2. Stacking: The drive-thru lane shall contain no less than ten vehicle stacking, measured from the pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicles spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.
 3. Circulation: No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive thru shall not take ingress or egress from a local residential road.
 4. Noise: no component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60db between the hours of 7:00 p.m. and 6:00 a.m. daily.
 5. Screening: The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.

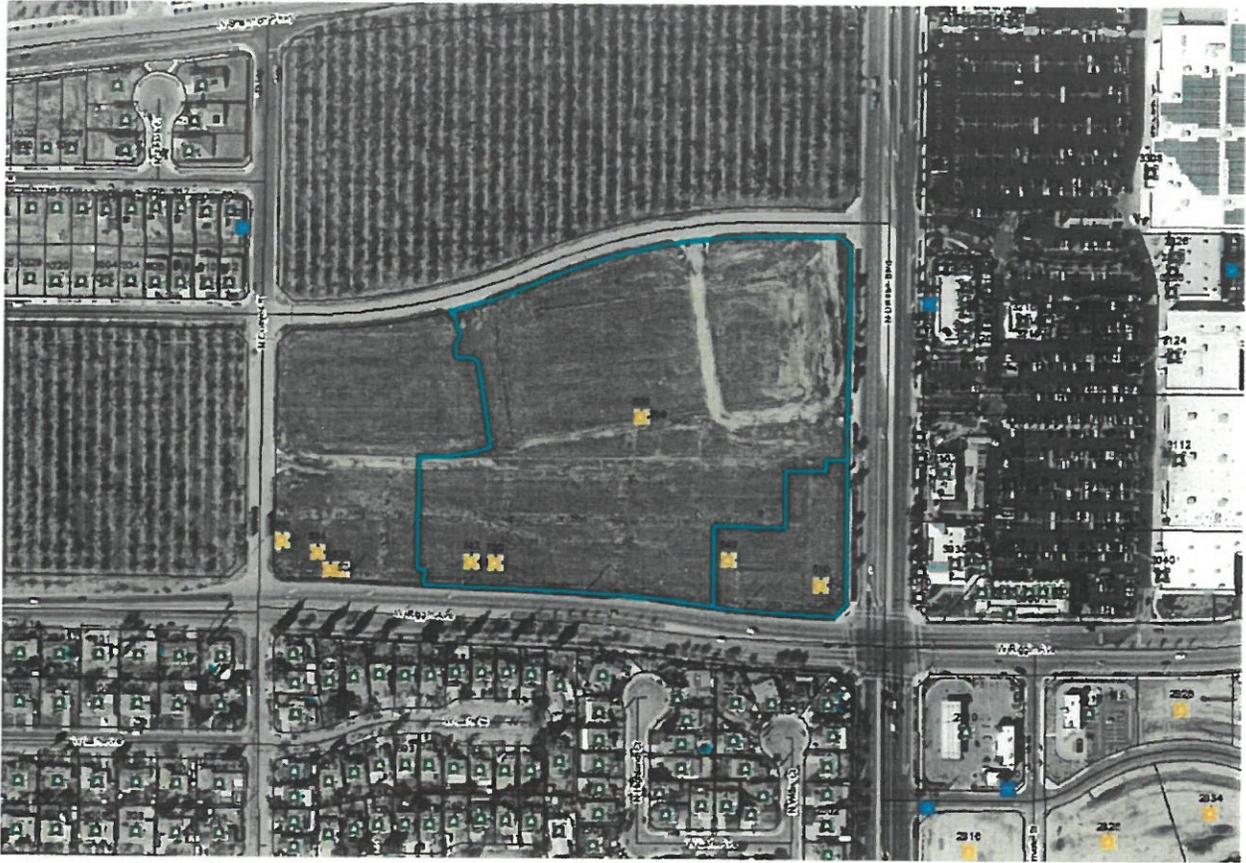
Menu boards and signage: Shall be oriented or screened to avoid direct visibility from adjacent public streets.

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments. The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.



Signature

Development standards are those contained in the Orchard Walk Specific Plan, May 2007.



City of Visalia
Building: Site Plan
Review Comments

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project
Please refer to the applicable California Code & local ordinance for additional requirements.

- A building permit will be required. *For information call (559) 713-4444*
- Submit 1 digital set of professionally prepared plans and 1 set of calculations. (Small Tenant Improvements)
- Submit 1 digital set of plans prepared by an architect or engineer. Must comply with 2016 California Building Cod Sec. 2308 for conventional light-frame construction or submit 1 digital set of engineered calculations.
- Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:**
- Meet State and Federal requirements for accessibility for persons with disabilities.
- A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- All accessible units required to be adaptable for persons with disabilities.
- Maintain sound transmission control between units minimum of 50 STC.
- Maintain fire-resistive requirements at property lines.
- A demolition permit & deposit is required. *For information call (559) 713-4444*
- Obtain required permits from San Joaquin Valley Air Pollution Board. *For information call (661) 392-5500*
- Plans must be approved by the Tulare County Health Department. *For information call (559) 624-8011*
- Project is located in flood zone _____ • Hazardous materials report.
- Arrange for an on-site inspection. (Fee for inspection \$157.00) *For information call (559) 713-4444*
- School Development fees. Commercial \$0.61 per square foot. Residential \$3.79 per square foot.
- Park Development fee \$ _____, per unit collected with building permits.
- Existing address must be changed to be consistent with city address. *For information call (559) 713-4320*
- Acceptable as submitted
- No comments at this time

Additional comments: See Previous Comments

Melinda Jones 7/24/19
Signature



Site Plan Review Comments For:

Visalia Fire Department
Corbin Reed, Fire Marshal
420 N. Burke
Visalia, CA 93292
559-713-4272 Office
559-713-4808 Fax

Date: 07/22/2019

Item # 1

Site Plan # 19-076 resub

APN: 078120030

Location:

The following comments are applicable when checked:

- The Site Plan Review comments are issued as general overview of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2016 California Fire Code (CFC), 2016 California Building Codes (CBC) and City of Visalia Municipal Codes.
- All fire detection, alarm, and extinguishing systems in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If building has been vacant for a significant amount of time, the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. *2016 CFC 901.6*
- No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire & life safety requirements including fire protection systems and fire hydrants in accordance with all applicable sections of the California Fire Code.
- Construction and demolition sites prior to and during construction shall comply with the following:
 - Water Supply for fire protection, either temporary or permanent, shall be made available as soon as combustible materials arrive on the site. *2016 CFC 3312*
 - An all-weather, 20 feet width Construction Access Road capable of holding a 75,000 pound fire apparatus. Fire apparatus access shall be provided within 100 feet of temporary or permanent fire department connections. *2016 CFC 3310*
- More information is needed before a Site Plan Review can be conducted. Please submit plans with more detail. Please include information on

General:

- Address numbers must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses served are by a common driveway, the range of numbers shall be posted at the roadway/driveway. *2016 CFC 505.1*
- All hardware on exit doors, illuminated exit signs and emergency lighting shall comply with the 2016 California Fire Code. This includes all locks, latches, bolt locks, panic hardware, fire exit hardware and gates.
- Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. *2016 CFC 304.3.3*

- A Knox Box key lock system is required. Where access to or within a structure or area is restricted because of secured openings (doors and/or gates), a key box is to be installed in an approved location. The key box shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 420 N Burke, Visalia, CA 93292. Please allow adequate time for shipping and installation. *2016 CFC 506.1*
- If your business handles hazardous material in amounts that exceed the Maximum Allowable Quantities listed on *Table 5003.1.1(1), 5003.1.1(2), 5003.1.1(3) and 5003.1.1(4) of the 2016 California Fire Code*, you are required to submit an emergency response plan to the Tulare County Health Department. Also you shall indicate the quantities on your building plans and prior to the building final inspection a copy of your emergency response plan and Safety Data Sheets shall be submitted to the Visalia Fire Department.

Water Supply for Residential, Commercial & Industrial:

Residential

- Fire hydrant spacing and location shall comply with the following requirements:
The exact location and number of fire hydrants shall be at the discretion of the fire marshal, fire chief and/or their designee. *Visalia Municipal Code 16.36.120(5)*
 - Single-family residential developments shall be provided with fire hydrants every six hundred (600) lineal feet of residential frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Multi-family, zero lot line clearance, mobile home park or condominium developments shall be provided with fire hydrants every four hundred (400) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Multi-family or condominium developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every six (600) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.

Commercial & Industrial

- Where a portion of the facility or building is more than 400 feet from a hydrant on a fire apparatus access road, on-site fire hydrant(s) shall be provided. *2016 CFC 507.5.1*
- Due to insufficient building information, the number and distance between fire hydrants cannot be determined by the Site Plan Review process. The number of fire hydrants and distance between required fire hydrants shall be determined by utilizing type of construction and square footage in accordance with *CFC 2016 Appendix C102 & C103 & CFC 507.5.1*
- To determine fire hydrant location(s) and distribution the following information was provided to the Site Plan Review committee: **Type of construction** _____ **Square footage** _____

Emergency Access

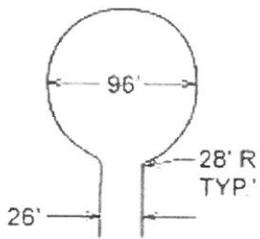
- A fire apparatus access roads shall be provided and must comply with the 2016 CFC and extend within 150 feet of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility. Fire apparatus

access roads shall have an unobstructed width of not less than 20 feet. Minimum turning radius for emergency fire apparatus shall be 20 feet inside radius and 43 feet outside radius. *2016 CFC 503.1.1*

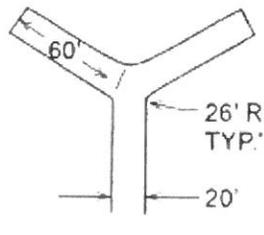
☒ Buildings or portions of buildings or facilities with a vertical distance between the grade plan and the highest roof surface that exceed 30 feet shall provide an approved fire apparatus access roads capable of accommodating fire department aerial apparatus.

- Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders.
- Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building.
- Overhead utility and power lines shall not be located over the aerial fire apparatus access road or between the aerial fire apparatus road and the building.

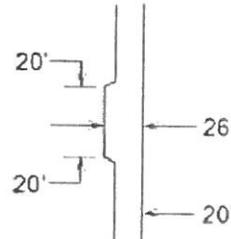
☐ Fire apparatus access roads in excess of 150 feet and dead end shall be provided with a turnaround. Fire apparatus access roads with a length of 151-500 feet shall be a minimum of 20 feet in width. Length of 501-750 feet shall be 26 feet in width. *2016 CFC Table D103.4*



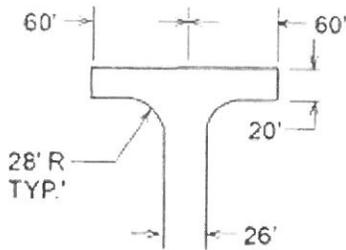
96' DIAMETER
CUL-DE-SAC



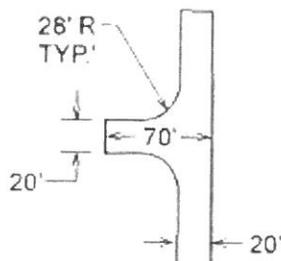
60' "Y"



MINIMUM CLEARANCE
AROUND A FIRE
HYDRANT



120' HAMMERHEAD



ACCEPTABLE ALTERNATIVE
TO 120' HAMMERHEAD

☒ Approved No PARKING – FIRE LANE signs shall be provided for fire apparatus access roads to identify such roads or prohibit the obstruction thereof. Signs shall have a minimum dimension of 12 inches wide by 18 inches high and have red letters on a white reflective background. *2013 CFC 503.3/D103.6*

SIGN TYPE "A"



12"

SIGN TYPE "C"



12"

SIGN TYPE "D"



12"

18"

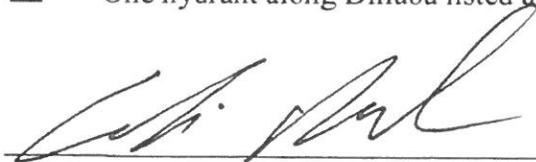
- On site Fire Apparatus Access Roads shall be provided and have an unobstructed width of not less than the following;
- 20 feet width, exclusive of shoulders (No Parking)
 - More than 26 feet width, exclusive of shoulders (No Parking one side)
 - More than 32 feet wide, exclusive of shoulders (Parking permitted on both sides)
- Marking- approved signs, other approved notices or marking that include the words "NO PARKING-FIRE LANE shall be provided for fire apparatus access roads to identify such roads or prohibit the obstruction thereof. *CFC 503.3*
- Gates on access roads shall be a minimum width of 20 feet and shall comply with the following:
2016 CFC D103.5
- Gates shall be of the swinging or sliding type.
 - Gates shall allow manual operation by one person (power outages).
 - Gates shall be maintained in an operative condition at all times.
 - Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. (Note: Knox boxes shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 420 N Burke, Visalia, CA 93292. Please allow adequate time for shipping and installation.)
- Streets shall meet the City of Visalia's Design & Improvement Standards for streets to ensure that fire apparatus can make access to all structures in the event of an emergency.

Fire Protection Systems

- An automatic fire sprinkler system will be required for this building. Also, a fire hydrant is required within 50 feet of the Fire Department Connection (FDC). Where an existing building is retrofitted with a sprinkler system (NFPA 13 or NFPA 13R) a fire hydrant shall be provided within 75 feet of the FDC. An additional 25 feet of distance between a fire hydrant and FDC may be granted when a fire sprinkler Density is designed with an additional 25%. *2016 CFC 912 and Visalia Municipal Code 8.20.010 subsection C103.4*
- Locking fire department connection (FDC) caps are required. The caps shall be ordered using an approved Knox Authorization Order Form. The forms are located at the fire department administration office located at 420 N Burke, Visalia, CA 93292. *2016 CFC 912.4.1*
- Commercial cooking appliances and domestic cooking appliances used for commercial purposes that produces grease laden vapors shall be provided with a Type 1 Hood, in accordance with the California Mechanical Code, and an automatic fire extinguishing system. *2016 CFC 904.12 & 609.2*

Special Comments:

- One hydrant along Dinuba listed as existing is not, one existing hydrant along Riggin is not shown.



Corbin Reed
Fire Marshal

SPR-19-076

City of Visalia
Police Department
303 S. Johnson St.
Visalia, Ca. 93292
(559) 713-4370

Site Plan Review Comments



No Comment at this time.



Request opportunity to comment or make recommendations as to safety issues as plans are developed.



Public Safety Impact fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001

Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.



Not enough information provided. Please provide additional information pertaining to:



Territorial Reinforcement: Define property lines (private/public space).



Access Controlled / Restricted etc:



Lighting Concerns:



Landscaping Concerns:



Traffic Concerns:



Surveillance Issues:



Line of Sight Issues:



Other Concerns:

[Signature] *U140*

Site Plan Review Comments For:

California Water Service
Stuart Skoglund, Superintendent
216 N. Valley Oaks Dr.
Visalia, CA 93292
559-624-1662 Office
559-735-3189 Fax

Date: 07/24/2019
Item # 1
Site Plan # 19-076
Project:
Description:
Applicant:
Location: NW corner Riggin & Dinuba
APN:

The following comments are applicable when checked:

- No Comments at this time

- Fire Hydrants
Comments:

- Services
Comments: All buildings are close to Dinuba and Riggin so services can be installed off of existing main.

- Mains
Comments: There is no on-site main on this project. Existing main on Dinuba and Riggin already front this project.

- Back flow requirements
Comments: Backflow devices will be required on all new and existing domestic, irrigation, and fire services.

Additional Comments:

Stuart Skoglund
Superintendent

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

July 24, 2019

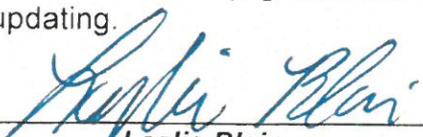
ITEM NO. 1	RESUBMITTAL
SITE PLAN NO:	SPR19-076
PROJECT TITLE:	Orchard Walk West - Phase 1
DESCRIPTION:	Multi-pad Commercial Development with 3 Proposed buildings, OffStreet Parking, Dining, Shops, Walkways and Landscaping
APPLICANT:	Donahue Schriber Realty Group
OWNER:	Donahue Schriber Realty Group
APN:	078-120-030
LOCATION:	NW Corner Intersection Rigg in Ave and Dinuba

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- No Comments
- See Previous Site Plan Comments
- Install Street Light(s) per City Standards.
- Install Street Name Blades at Locations.
- Install Stop Signs at *driveway exit* Locations.
- Construct parking per City Standards PK-1 through PK-4.
- Construct drive approach per City Standards.
- Traffic Impact Analysis required (CUP)
 - Provide more traffic information such as change change in trip generation. Depending on development size, characteristics, etc., a TIA may be required.
- Additional traffic information required (Non Discretionary)
 - Trip Generation - Provide documentation as to concurrence with General Plan.
 - Site Specific - Evaluate access points and provide documentation of conformance with COV standards. If noncomplying, provide explanation.
 - Traffic Impact Fee (TIF) Program - Identify improvements needed in concurrence with TIF.

Additional Comments:

- See COV Design and Improvement Standard C-32 Drive Approach Locations.
- As there may be a change in land use and/or building square footage from previous TIA performed, applicant to provide (increase/decrease) in trip generation for comparison purposes. Depending on outcome, TIA may require updating.



Leslie Blair

19076

COMMERCIAL BIN SERVICE

- No comments.
- See comments below
- Revisions required prior to submitting final plans. See comments below.
- Resubmittal required. See comments below.
- Customer responsible for all cardboard and other bulky recyclables to be broken down before disposing of in recycle containers
- ALL refuse enclosures must be R-3 OR R-4
- Customer must provide combination or keys for access to locked gates/bins
- Type of refuse service not indicated.
- Location of bin enclosure not acceptable. See comments below.
- Bin enclosure not to city standards double.
- Inadequate number of bins to provide sufficient service. See comments below.
- Drive approach too narrow for refuse trucks access. See comments below.
- Area not adequate for allowing refuse truck turning radius of : Commercial 50 ft. outside 36 ft. inside; Residential 35 ft. outside, 20 ft. inside.
- Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- Bin enclosure gates are required
- Hammerhead turnaround must be built per city standards.
- Cul - de - sac must be built per city standards.
- Bin enclosures are for city refuse containers only. Grease drums or any other items are not allowed to be stored inside bin enclosures.
- Area in front of refuse enclosure must be marked off indicating no parking
- Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS) with no less than 38' clear space in front of the bin, included the front concrete pad.
- Customer will be required to roll container out to curb for service.
- Must be a concrete slab in front of enclosure as per city standards, the width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.
- Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.
- City ordinance 8.28.120-130 (effective 07/19/18) requires contractor to contract with City for removal of construction debris unless transported in equipment owned by contractor or unless contracting with a franchise permittee for removal of debris utilizing roll-off boxes.
- Comment**
All bin enclosure are not acceptable. Need to avoid Landscape Islands at both location at PADS 1E and 3D. The enclosure at IN-N-OUT will have to be designed and located for a STAB service (DIRECT ACCESS). Be sure to have enclosure gates open 180 degrees. All gates must be equipped with chain bolts to secure them from closing. Solid Waste will not roll these bins out for service. 1000 gallon grease traps will be required for restaurants and grease traps are not allowed inside the enclosures.

Jim Ross, Solid Waste Manager, 559-713-4533

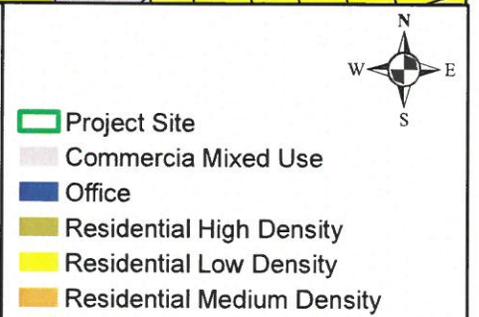
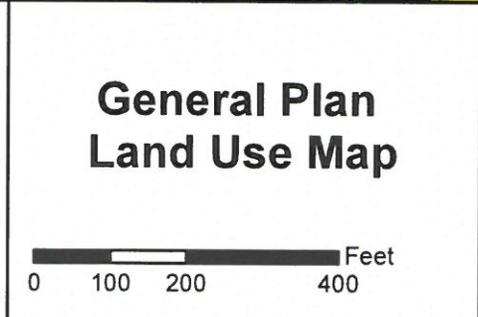
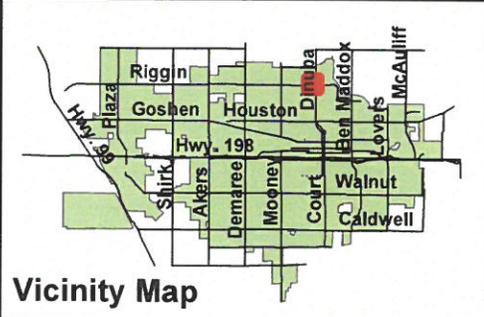
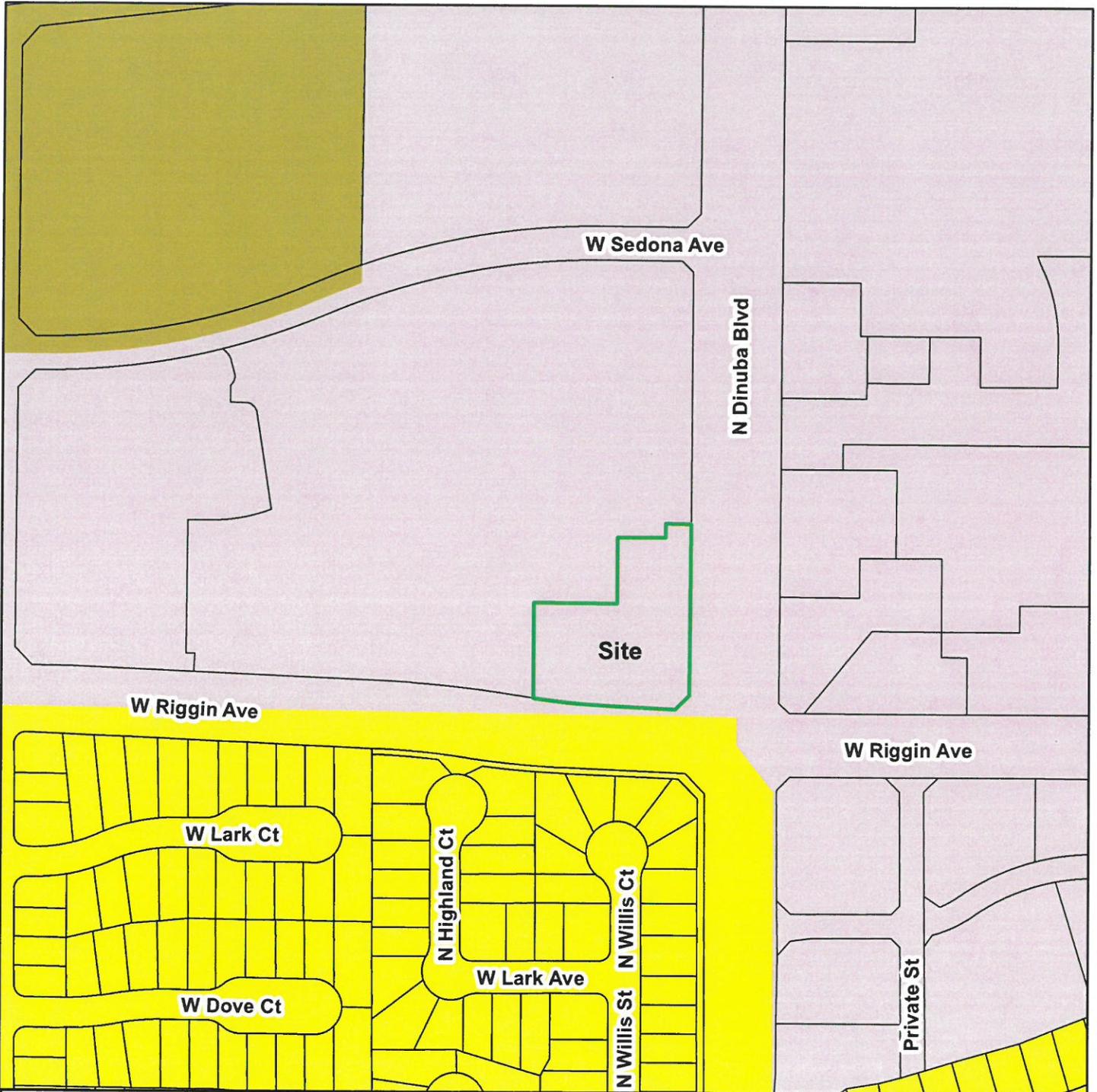
Edward Zuniga, Solid Waste Supervisor, 559-713-4338



Conditional Use Permit No. 2019-36

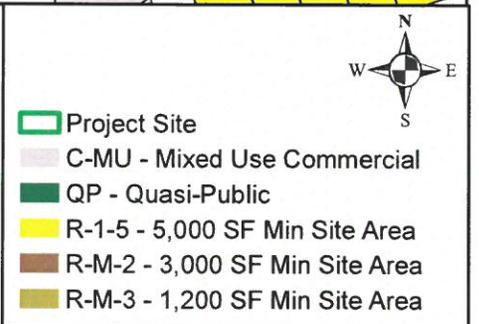
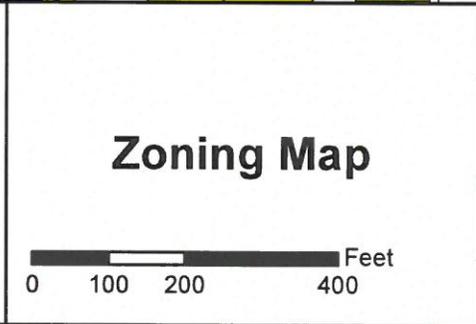
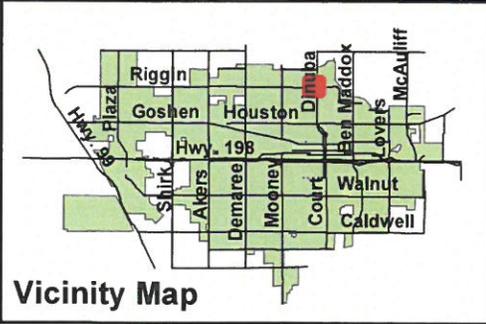
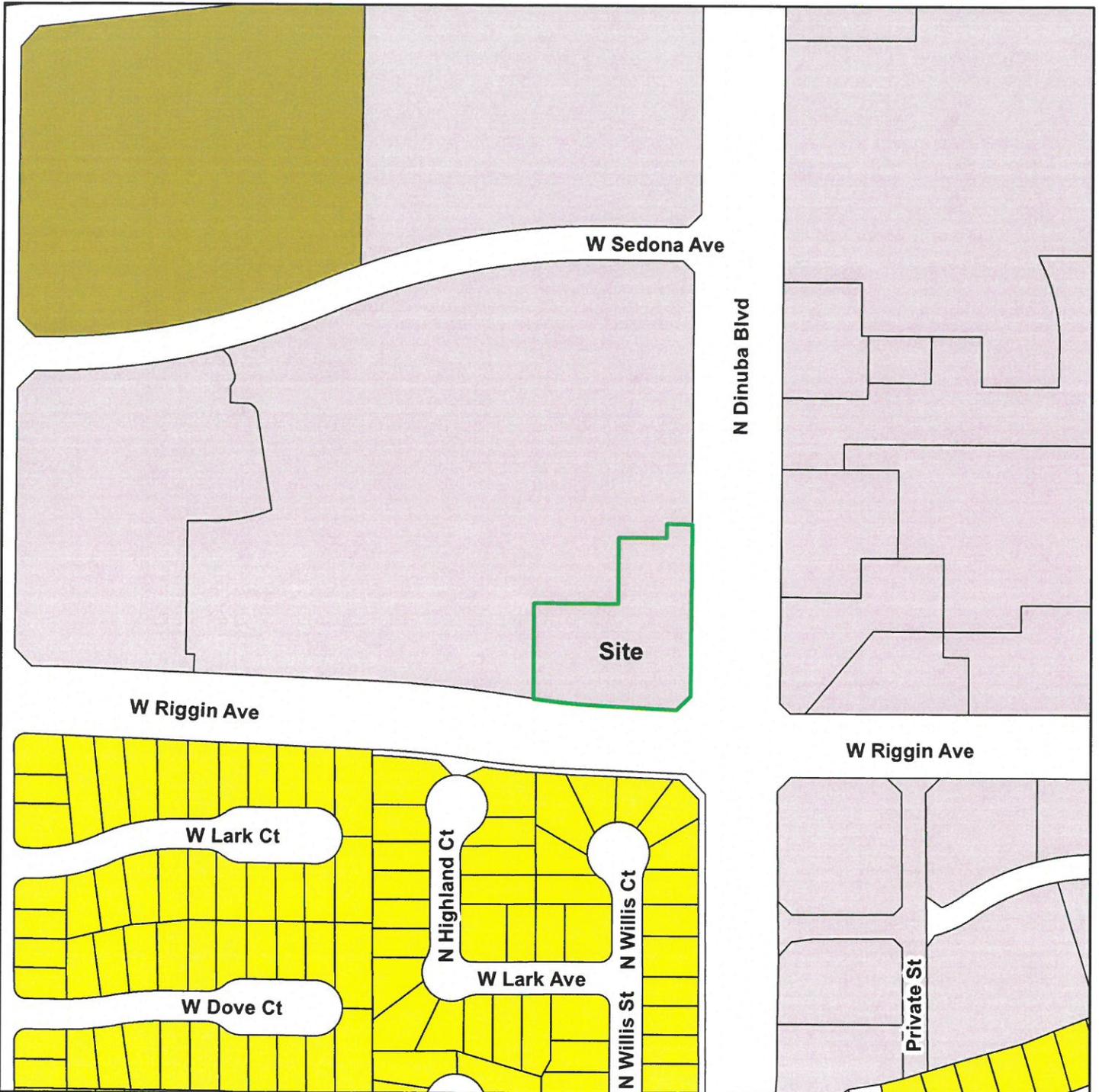
The project site is located on the northwest corner of north Dinuba Blvd. and west Rigglin Ave.

(APN: 078-120-030 & 078-120-32)

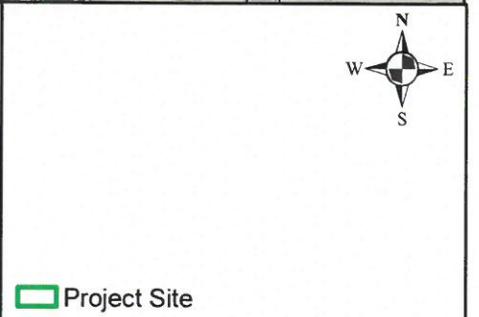
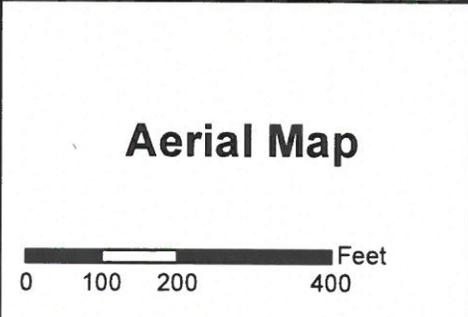
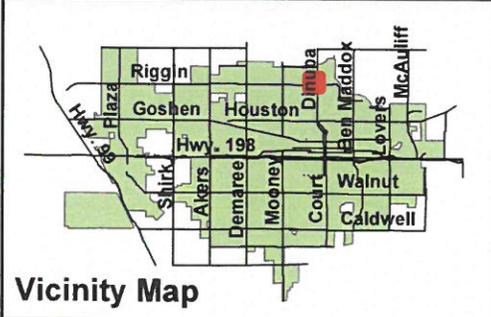


Conditional Use Permit No. 2019-36

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(APN: 078-120-030 & 078-120-32)



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