

PLANNING COMMISSION AGENDA

CHAIRPERSON:

Adam Peck



VICE CHAIRPERSON:

Brett Taylor

COMMISSIONERS: Adam Peck, Brett Taylor, Liz Wynn, Lawrence Segrue, Chris Gomez

MONDAY, JUNE 13, 2016; 5:30 PM WORKSESSION, 7:00 PM REGULAR MEETING,
COUNCIL CHAMBERS, 707 W. ACEQUIA, VISALIA CA

1. WORK SESSION –Consultant
Subdivision & Zoning Ordinance Update
2. BREAK –
3. THE PLEDGE OF ALLEGIANCE –
4. CITIZEN’S COMMENTS – This is the time for citizens to comment on subject matters that are not on the agenda but are within the jurisdiction of the Visalia Planning Commission. The Commission requests that a 5-minute time limit be observed for comments. Please begin your comments by stating and spelling your name and providing your street name and city. Please note that issues raised under Citizen’s Comments are informational only and the Commission will not take action at this time.
5. CHANGES OR COMMENTS TO THE AGENDA–
6. CONSENT CALENDAR - All items under the consent calendar are to be considered routine and will be enacted by one motion. For any discussion of an item on the consent calendar, it will be removed at the request of the Commission and made a part of the regular agenda.
 - No Items on Consent Calendar
7. PUBLIC HEARING – Brandon Smith
Conditional Use Permit No. 2016-11: A request by Suncrest Bank to allow reconfiguration of a drive-thru lane serving an existing bank building on 0.95 acres in the C-DT (Central Business District Retail) Zone. The project site is located at 501 W. Main Street, on the west side of West Street between Main Street and Acequia Avenue. (APN: 093-195-005, 006) The project is Categorical Exempt from the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15302, Categorical Exemption No. 2016-25
8. PUBLIC HEARING – Paul Bernal
 - a. Conditional Use Permit No. 2016-09: A request by Vermeltfoort Architects Inc. to construct a new 4,767 square foot drive-thru carwash with an attached pay station, vacuum canopy area and a detached 245 square foot vacuum equipment building on 1.51-acres of an overall 17.01-acre site. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area. The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039). An Initial Study (Initial Study No. 2016-22) was prepared for this project, which disclosed the proposed project has no new effects that could occur, or new mitigation

measures that would be required that have not been addressed within the scope of the City of Visalia General Plan Program Environmental Impact Report (SCH No. 2010041078). The Program Environmental Impact Report was certified by Resolution No. 2014-37 on October 14, 2014. Therefore, staff concludes that the Program Environmental Impact Report adequately analyzed and addressed the conditional use permit and tentative parcel map applications.

- b. Tentative Parcel Map No. 2016-06: A request by Lane Engineers Inc., to subdivide 17.01 acres into four parcels. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area. The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039). An Initial Study (Initial Study No. 2016-22) was prepared for this project, which disclosed the proposed project has no new effects that could occur, or new mitigation measures that would be required that have not been addressed within the scope of the City of Visalia General Plan Program Environmental Impact Report (SCH No. 2010041078). The Program Environmental Impact Report was certified by Resolution No. 2014-37 on October 14, 2014. Therefore, staff concludes that the Program Environmental Impact Report adequately analyzed and addressed the conditional use permit and tentative parcel map applications.

9. PUBLIC HEARING – Paul Scheibel

Title: General Plan Amendment 2016-06, 5th Cycle General Plan Housing Element Update (2014-2023)

Project Description: The project is the 5th Cycle General Plan Housing Element Update. State Housing Element Law (Government Code Section 65580 (et seq.)) requires local governments to plan for the existing and projected housing needs of all economic segments of the community. The City of Visalia last updated its Housing Element in 2010, which served a planning period to December 31, 2015. The Housing Element Update is a comprehensive review and select update of the 2010 Housing Element background information and goals, policies, and programs. The nine-year planning period for the new Housing Element covers the period January 1, 2016, to December 31, 2023. Adoption of the Housing Element Update is anticipated in July 2016.

Environmental Review: Negative Declaration No. 2015-56 has been prepared for the project with comment period from April 25, 2016 to June 1, 2016.

10. REGULAR ITEM – Eric Frost Ballot Measure

11. DIRECTOR'S REPORT/ PLANNING COMMISSION DISCUSSION-

The Planning Commission meeting may end no later than 11:00 P.M. Any unfinished business may be continued to a future date and time to be determined by the Commission at this meeting. The Planning Commission routinely visits the project sites listed on the agenda.

For the hearing impaired, if signing is desired, please call (559) 713-4359 twenty-four (24) hours in advance of the scheduled meeting time to request these services. For the visually impaired, if enlarged print or Braille copy is desired, please call (559) 713-4359 for this assistance in advance of the meeting and such services will be provided as soon as possible following the meeting.

Any written materials relating to an item on this agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the City Office, 315 E. Acequia Visalia, CA 93291, during normal business hours.

APPEAL PROCEDURE

THE LAST DAY TO FILE AN APPEAL IS THURSDAY, JUNE 23, 2016 BEFORE 5 PM

According to the City of Visalia Zoning Ordinance Section 17.02.145 and Subdivision Ordinance Section 16.04.040, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal form with applicable fees shall be filed with the City Clerk at 220 N. Santa Fe, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.visalia.city or from the City Clerk.

THE NEXT REGULAR MEETING WILL BE HELD ON MONDAY, JUNE 27, 2016



REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE: June 13, 2016

PROJECT PLANNER: Brandon Smith, Senior Planner
(559) 713-4636; brandon.smith@visalia.city

SUBJECT: Conditional Use Permit No. 2016-11: A request by Suncrest Bank to allow reconfiguration of a drive-thru lane serving an existing bank building on 0.95 acres in the C-DT (Central Business District Retail District) Zone. The project site is located at 501 W. Main Street, on the west side of West Street between Main Street and Acequia Avenue. (APN: 093-195-005, 006)

STAFF RECOMMENDATION

Staff recommends approval of Conditional Use Permit No. 2016-11, based upon the findings and conditions in Resolution No. 2016-21. Staff's recommendation is based on the conclusion that the request is consistent with the General Plan and Zoning Ordinance.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No. 2016-11 based on the findings and conditions in Resolution No. 2016-21.

PROJECT DESCRIPTION

Conditional Use Permit No. 2016-11 is a request to allow reconfiguration of a bank drive-thru lane as part of a building remodel and exterior improvements, as depicted in the site plan attached as Exhibit "A". The site is comprised of a series of tenant spaces fronting Main Street and an on-site parking lot located along Acequia Avenue south of the tenant spaces.

The parking lot is currently developed with 43 parking spaces. In the center of the parking lot are side-by-side drive-thru lanes for bank teller service sheltered under a canopy. The parking lot and drive-thru lanes legally exist but do not meet the City's current improvement standards for parking lots, handicap accessibility, and vehicular access. In particular, the existing drive-thru lanes are located in the middle of a drive isle and the vehicle stacking lane shares the same space as the back-out area for adjacent parking stalls. Vehicles using parking spaces along this drive-isle are forced to use the drive-thru lanes in order to exit the parking lot.

This layout will be replaced with a new parking lot containing 41 parking spaces and a single dedicated drive-thru lane for an automatic teller machine sheltered by an overhead canopy. The drive-thru will be relocated to the west side of the site and will be constructed to accommodate four vehicles. The parking lot consists entirely of two-way drive isles accessed by single driveways on West Street and Acequia Avenue.

Floor plans are provided in Exhibit "B" and building elevations and renderings are provided in Exhibits "C" and "D". Banks are a permitted use in the C-DT (Central Business District Retail) zone. Although drive-thru lanes are currently not an allowed use in this zone, the change of a legally existing nonconforming use/structure is permitted under Zoning Ordinance Section 17.40.070 subject to the granting of a conditional use permit.

BACKGROUND INFORMATION

General Plan Land Use Designation:	Downtown Mixed Use
Zoning:	C-DT / Central Business District Retail
Surrounding Land Uses and Zoning:	North: C-DT / Bank South: PA (Professional/Administrative Office) / Offices East: C-DT (Regional Retail Commercial) – Fast-food restaurant without drive-thru West: C-DT (Regional Retail Commercial) – Offices
Environmental Review:	Categorical Exemption No. 2016-25
Special Districts:	Design District “DRD”
Site Plan:	2016-29

RELATED PLANS & POLICIES

Please see attached summary of related plans and policies. The proposed project is consistent with applicable plans and policies.

RELATED PROJECTS

None.

PROJECT EVALUATION

Staff supports the requested conditional use permit, as conditioned, based on project consistency with the General Plan and the Zoning Ordinance.

Land Use Compatibility

Banks with a drive-thru lane are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site consists of a bank and additional lease space with a large on-site parking lot that is uncharacteristic for the central business district. The parking lot contains a drive-thru lane that has existed for several years. Although drive-thru lanes are no longer an allowed use in the C-DT zone, the proposed reconfiguration results in a superior parking lot design and on-site circulation.

Staff believes that the preservation of a drive-thru lane on the site and the proposed reconfiguration does not compromise the nature and character of the site, being that the drive-thru lane is adjacent to another parking lot on the west. Furthermore, the drive-thru lane does not constitute an integral feature of the site based on the physical separation of the commercial uses from the parking lot and based on the drive-thru's location in the rear of the property.

Drive-Thru Performance Standards

The City has adopted performance standards applicable to uses incorporating use of drive-thru lanes into their facility. The standards are found in Zoning Ordinance Section 17.32.162 and pertain to separation from residences, vehicle stacking, circulation, noise, screening, and menu boards and signage. Drive-thru facilities that cannot meet all performances standards are allowed subject to the granting of a conditional use permit wherein the project's ability to meet that performance standard is reviewed.

The proposed drive-thru facility is not able to meet the performance standard for vehicle stacking. This standard specifies that the drive-thru lane shall contain no less than ten vehicle stacking measured from the point of transaction to the entrance of the drive-thru lane. The proposed drive-thru lane accommodates for four vehicle stacking at the drive-thru's entrance.

Staff recommends in favor of the drive-thru lane as proposed on the basis that the affiliated bank is not anticipated to draw a significant amount of vehicles in its drive-thru lane as would a typical fast food restaurant. In addition, the reconfigured drive-thru location and layout is superior to the existing layout in terms of accommodating excess vehicle stacking on-site and separating the drive-thru and parking lot functions.

Findings for Expansion of nonconforming uses and structures

Drive-thru lanes are currently not an allowed use in the C-DT zone, although the change of a legally existing nonconforming use/structure is permitted under Zoning Ordinance Section 17.40.070 subject to the granting of a conditional use permit.

Zoning Ordinance Section 17.40.070 states that an existing legal nonconforming use or structure may be minimally expanded or changed subject to the granting of a conditional use permit if five findings can be made. Staff has determined that these finds can be made with regards to the proposed project and has included them as recommended findings, #3 through #7.

Parking

The parking lot and drive-thru lane reconfiguration will result in the loss of on-site parking spaces, from 43 spaces down to 41 spaces. However, the site is located inside the Central Business District Parking Zone A wherein parking in-lieu fees may be paid to compensate for any of the required parking spaces not located on-site.

Building Elevations

Exhibits "C" and "D" provide building elevations and renderings proposed for the Suncrest Bank and retail space remodel and the new parking lot and drive-thru. The elevations are provided as reference for the entire project, being that the bank and retail space remodel are not part of the request for Conditional Use Permit.

Environmental Review

The requested action is considered Categorically Exempt under Section 15302 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2016-25).

Projects determined to meet this classification include the replacement or reconstruction of existing structures and facilities where the new structure will be located on the same site as the structure replaced and will have substantially the same purpose and capacity as the structure replaced.

RECOMMENDED FINDINGS

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:

- The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The proposed use is similar in nature and intensity to other surrounding businesses in the area. The project site has adequate ingress and egress and parking for the proposed fast food restaurant.
 - The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity. The proposed use is similar in nature and intensity to other surrounding businesses in the area. The project site has adequate ingress and egress and parking for the proposed fast food restaurant.
3. That the change resulting from the proposed project is minimal. Specifically, the project does not result in any additional square footage, parking spaces, or otherwise intensity of use, over what was existing at the time of adoption of an ordinance making the use or structure nonconforming.
 4. That the change resulting from the proposed project will not adversely affect or be materially detrimental to adjoining properties.
 5. That there is a need for relief of overcrowded conditions or for modernization in order to properly operate the use.
 6. That the use and/or structure is existing and has not been discontinued for a one hundred eighty (180) day continuous period.
 7. That the change resulting from the proposed project will not increase the discrepancy between existing conditions and the standards of coverage, front yards, side yards, rear yard, height of structures or distances between structures prescribed in the regulations for the C-DT zone in which the structure is located.
 8. That the project is considered Categorically Exempt under Section 15302 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2016-25)

RECOMMENDED CONDITIONS OF APPROVAL

1. That the site be developed in substantial compliance with the comments and conditions of the Site Plan Review Committee as set forth under Site Plan No. 2016-029.
2. That the project consisting of the drive-thru lane and parking lot configuration be operated in substantial compliance with the site plan shown in Exhibit "A".
3. That landscape and irrigation plans be submitted with the building permit, designed by a professional landscape architect. In addition, landscape and irrigation plans shall comply with the State Model Water Efficient Landscape Ordinance by submittal of Landscape Documentation Packages and Certificates of Compliance certified by a California licensed landscape architect with sections signed by appropriately licensed or certified persons as required by ordinance.
4. That all signs shall require a separate building permit.
5. That all other federal, state and city codes, ordinances and laws be met.
6. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2016-11.

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.02.145, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal with applicable fees shall be in writing and shall be filed with the City Clerk at 220 N. Santa Fe Street, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.visalia.city or from the City Clerk.

Attachments:

- Related Plans & Policies
- Resolution 2016-21
- Exhibit "A" – Site Plan
- Exhibit "B" – Floor Plan
- Exhibit "C" – Exterior Elevations
- Exhibit "D" – Exterior Building Renderings
- Site Plan Review No. 2016-029 Comments
- General Plan Land Use Map
- Zoning Map
- Aerial Map
- Location Map

Related Plans & Policies

Zoning Ordinance Chapter 17.32

Special Provisions

17.32.162 Drive-thru lanes performance standards.

A. Purpose and Intent. It is the purpose of this section to specify performance standards applicable to uses that seek to incorporate a drive-thru lane in association with a specified use. This section does not apply to carwashes and lube and oil changing stations.

B. Performance standards:

1. Separation from residences. The drive-thru lane shall be no less than two hundred fifty (250) feet from the nearest residence or residentially zoned property.

2. Stacking. The drive-thru lane shall contain no less than ten (10) vehicle stacking, measured from pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicle spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.

3. Circulation. No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive-thru shall not take ingress or egress from a local residential road.

4. Noise. No component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60 db between the hours of 7:00 p.m. and 6:00 a.m. daily.

5. Screening. The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.

6. Menu boards and signage. Shall be oriented or screened to avoid direct visibility from adjacent public streets.

Zoning Ordinance Chapter 17.38

Conditional Use Permits

17.38.010 Purposes and powers

In certain zones conditional uses are permitted subject to the granting of a conditional use permit. Because of their unusual characteristics, conditional uses require special consideration so that they may be located properly with respect to the objectives of the zoning ordinance and with respect to their effects on surrounding properties. In order to achieve these purposes and thus give the zone use regulations the flexibility necessary to achieve the objectives of this title, the planning commission is empowered to grant or deny applications for conditional use permits and to impose reasonable conditions upon the granting of such permits. (Prior code § 7525)

17.38.030 Lapse of conditional use permit

A conditional use permit shall lapse and shall become void twenty-four (24) months after the date on which it became effective, unless the conditions of the permit allowed a shorter or greater time limit, or unless prior to the expiration of twenty-four (24) months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site which was the subject of the permit. A permit may be renewed for an additional period of one year; provided, that prior to the expiration of twenty-four (24) months from the date the permit originally became effective, an application for renewal is filed with the planning commission. The commission may grant or deny an application for renewal of a conditional use permit. In the case of a planned residential development, the recording of a final map and improvements thereto shall be deemed the same as a building permit in relation to this section. (Ord. 2001-13 § 4 (part), 2001: prior code § 7527)

17.38.040 Revocation

Upon violation of any applicable provision of this title, or, if granted subject to a condition or conditions, upon failure to comply with the condition or conditions, a conditional use permit shall be suspended automatically. The planning commission shall hold a public hearing within sixty (60) days, in accordance with the procedure prescribed in Section 17.38.080, and if not satisfied that the regulation, general provision or condition is being complied with, may revoke the permit or take such action as may be necessary to insure compliance with the regulation, general provision or condition. Appeals of the decision of the planning commission may be made to the city council as provided in Section 17.38.120. (Prior code § 7528)

17.38.050 New application

Following the denial of a conditional use permit application or the revocation of a conditional use permit, no application for a conditional use permit for the same or substantially the same conditional use on the same or substantially the same site shall be filed within one year from the date of denial or revocation of the permit unless such denial was a denial without prejudice by the planning commission or city council. (Prior code § 7530)

17.38.060 Conditional use permit to run with the land

A conditional use permit granted pursuant to the provisions of this chapter shall run with the land and shall continue to be valid upon a change of ownership of the site or structure which was the subject of the permit application subject to the provisions of Section 17.38.065. (Prior code § 7531)

17.38.065 Abandonment of conditional use permit

If the use for which a conditional use permit was approved is discontinued for a period of one hundred eighty (180) days, the use shall be considered abandoned and any future use of the site as a conditional use will require the approval of a new conditional use permit.

17.38.080 Public hearing--Notice

- A. The planning commission shall hold at least one public hearing on each application for a conditional use permit.
- B. Notice of the public hearing shall be given not less than ten days nor more than thirty (30) days prior to the date of the hearing by mailing a notice of the time and place of the hearing to property owners within three hundred (300) feet of the boundaries of the area occupied or to be occupied by the use which is the subject of the hearing, and by publication in a newspaper of general circulation within the city. (Prior code § 7533)

17.38.090 Investigation and report

The planning staff shall make an investigation of the application and shall prepare a report thereon which shall be submitted to the planning commission. (Prior code § 7534)

17.38.100 Public hearing--Procedure

At the public hearing the planning commission shall review the application and the statement and drawing submitted therewith and shall receive pertinent evidence concerning the proposed use and the proposed conditions under which it would be operated or maintained, particularly with respect to the findings prescribed in Section 17.38.110. The planning commission may continue a public hearing from time to time as it deems necessary. (Prior code § 7535)

17.38.110 Action by planning commission

- A. The planning commission may grant an application for a conditional use permit as requested or in modified form, if, on the basis of the application and the evidence submitted, the commission makes the following findings:
 - 1. That the proposed location of the conditional use is in accordance with the objectives of the zoning ordinance and the purposes of the zone in which the site is located;
 - 2. That the proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the vicinity.
- B. A conditional use permit may be revocable, may be granted for a limited time period, or may be granted subject to such conditions as the commission may prescribe. The commission may grant conditional approval for a permit subject to the effective date of a change of zone or other ordinance amendment.
- C. The commission may deny an application for a conditional use permit. (Prior code § 7536)

17.38.120 Appeal to city council

The decision of the City planning commission on a conditional use permit shall be subject to the appeal provisions of Section 17.02.145. (Prior code § 7537) (Ord. 2006-18 § 6, 2007)

17.38.130 Effective date of conditional use permit

A conditional use permit shall become effective immediately when granted or affirmed by the council, or upon the sixth working day following the granting of the conditional use permit by the planning commission if no appeal has been filed. (Prior code § 7539)

Zoning Ordinance Chapter 17.40

Nonconforming Uses and Structures

17.40.070 Expansion of nonconforming uses and structures.

An existing legal nonconforming use or legal nonconforming structure may be minimally expanded or changed subject to the granting of a conditional use permit after a noticed public hearing as specified in Chapter 17.38, and if all of the following findings are made:

- A. That such expansion or change is minimal. An expansion or change is considered to be minimal if the expansion comprises generally twenty (20) percent or less additional square footage of structure or site area or twenty (20) percent increase or less in intensity as measured by additional vehicle trips, parking need generation, etc., over what was existing at the time of adoption of an ordinance making the use or structure nonconforming;
- B. That such expansion or change will not adversely affect or be materially detrimental to adjoining properties;
- C. That there is a need for relief of overcrowded conditions or for modernization in order to properly operate the use;
- D. That the use and/or structure is existing and has not been discontinued for a one hundred eighty (180) day continuous period;
- E. That the expansion shall not increase the discrepancy between existing conditions and the standards of coverage, front yards, side yards, rear yard, height of structures or distances between structures prescribed in the regulations for the zone in which the structure is located. (Prior code § 7546)

RESOLUTION NO. 2016-21

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT NO. 2016-11: A REQUEST BY SUNCREST BANK TO ALLOW RECONFIGURATION OF A DRIVE-THRU LANE SERVING AN EXISTING BANK BUILDING ON 0.95 ACRES IN THE C-DT (CENTRAL BUSINESS DISTRICT RETAIL DISTRICT) ZONE. THE PROJECT SITE IS LOCATED AT 501 W. MAIN STREET, ON THE WEST SIDE OF WEST STREET BETWEEN MAIN STREET AND ACEQUIA AVENUE. (APN: 093-195-005, 006)

WHEREAS, Conditional Use Permit No. 2016-11 is a request by Suncrest Bank to allow reconfiguration of a drive-thru lane serving an existing bank building on 0.95 acres in the C-DT (Central Business District Retail District) Zone. The project site is located at 501 W. Main Street, on the west side of West Street between Main Street and Acequia Avenue. (APN: 093-195-005, 006); and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on June 13, 2016; and

WHEREAS, the Planning Commission of the City of Visalia finds Conditional Use Permit No. 2016-11, as conditioned by staff, to be in accordance with Chapter 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, the Planning Commission finds the project to be Categorical Exempt consistent with the California Environmental Quality Act (CEQA) and City of Visalia Environmental Guidelines.

NOW, THEREFORE, BE IT RESOLVED that the project is exempt from further environmental review pursuant to CEQA Section 15302.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan and Zoning Ordinance. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The proposed use is similar in nature and intensity to other surrounding businesses in the area. The project site has adequate ingress and egress and parking for the proposed fast food restaurant.
 - The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity. The proposed use is similar in nature and intensity to other surrounding

businesses in the area. The project site has adequate ingress and egress and parking for the proposed fast food restaurant.

3. That the change resulting from the proposed project is minimal. Specifically, the project does not result in any additional square footage, parking spaces, or otherwise intensity of use, over what was existing at the time of adoption of an ordinance making the use or structure nonconforming.
4. That the change resulting from the proposed project will not adversely affect or be materially detrimental to adjoining properties.
5. That there is a need for relief of overcrowded conditions or for modernization in order to properly operate the use.
6. That the use and/or structure is existing and has not been discontinued for a one hundred eighty (180) day continuous period.
7. That the change resulting from the proposed project will not increase the discrepancy between existing conditions and the standards of coverage, front yards, side yards, rear yard, height of structures or distances between structures prescribed in the regulations for the C-DT zone in which the structure is located.
8. That the project is considered Categorically Exempt under Section 15302 of the Guidelines for the Implementation of the California Environmental Quality Act (CEQA). (Categorical Exemption No. 2016-25)

BE IT FURTHER RESOLVED that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the site be developed in substantial compliance with the comments and conditions of the Site Plan Review Committee as set forth under Site Plan No. 2016-029.
2. That the project consisting of the drive-thru lane and parking lot configuration be operated in substantial compliance with the site plan shown in Exhibit "A".
3. That landscape and irrigation plans be submitted with the building permit, designed by a professional landscape architect. In addition, landscape and irrigation plans shall comply with the State Model Water Efficient Landscape Ordinance by submittal of Landscape Documentation Packages and Certificates of Compliance certified by a California licensed landscape architect with sections signed by appropriately licensed or certified persons as required by ordinance.
4. That all signs shall require a separate building permit.
5. That all other federal, state and city codes, ordinances and laws be met.
6. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2016-11.

DATE	DESCRIPTION	BY	CHECKED
10/11/2017	ISSUED FOR PERMIT	J. TETER	J. TETER
10/11/2017	ISSUED FOR PERMIT	J. TETER	J. TETER
10/11/2017	ISSUED FOR PERMIT	J. TETER	J. TETER
10/11/2017	ISSUED FOR PERMIT	J. TETER	J. TETER
10/11/2017	ISSUED FOR PERMIT	J. TETER	J. TETER
10/11/2017	ISSUED FOR PERMIT	J. TETER	J. TETER

NOT FOR CONSTRUCTION

TETER, LLP
 ARCHITECTS ENGINEERS CONSTRUCTORS
 1315 S. GARDEN ST. SUITE 100, VISALIA, CA 93278
 TEL: 559.233.1111 FAX: 559.233.1112
 WWW.TETERLLP.COM



BUILDING RENOVATION
SUNCREST BANK
 501 W. MAIN STREET
 VISALIA, CA
 DRAWING TITLE
 SITE PLAN REVIEW
 PROJECT NO. 16-0942

SPR-1
 DRAWING
 16-0942

PROJECT INFORMATION

OWNERS:
 SUNCREST BANK
 501 W. MAIN STREET
 VISALIA, CA 93278

PROPERTY DESCRIPTION:
 501 W. MAIN STREET
 2000-SQ-FOOT
 SOUTH-EAST CORNER OF MAIN ST. & 8TH ST.
 VISALIA, CA 93278

PARCEL SIZE:
 10,000 SQ. FT.

USE:
 COMMERCIAL

BUILDINGS:
 2000-SQ-FOOT
 2000-SQ-FOOT
 2000-SQ-FOOT

TOTAL:
 2000-SQ-FOOT

PARKING:
 41 STALLS

PROJECT UTILITIES

ELECTRICAL:
 ELECTRICAL CONTRACTOR TO BE DETERMINED

GAS:
 THE GAS COMPANY
 500/427-2222

WATER:
 CALIFORNIA WATER SERVICE
 507/345-7534

SEWER:
 CITY OF VISALIA PUBLIC WORKS
 507/345-7534

TELEPHONE:
 SBC
 507/345-7534

SANITATION:
 CITY OF VISALIA SOLID WASTE
 507/345-7534

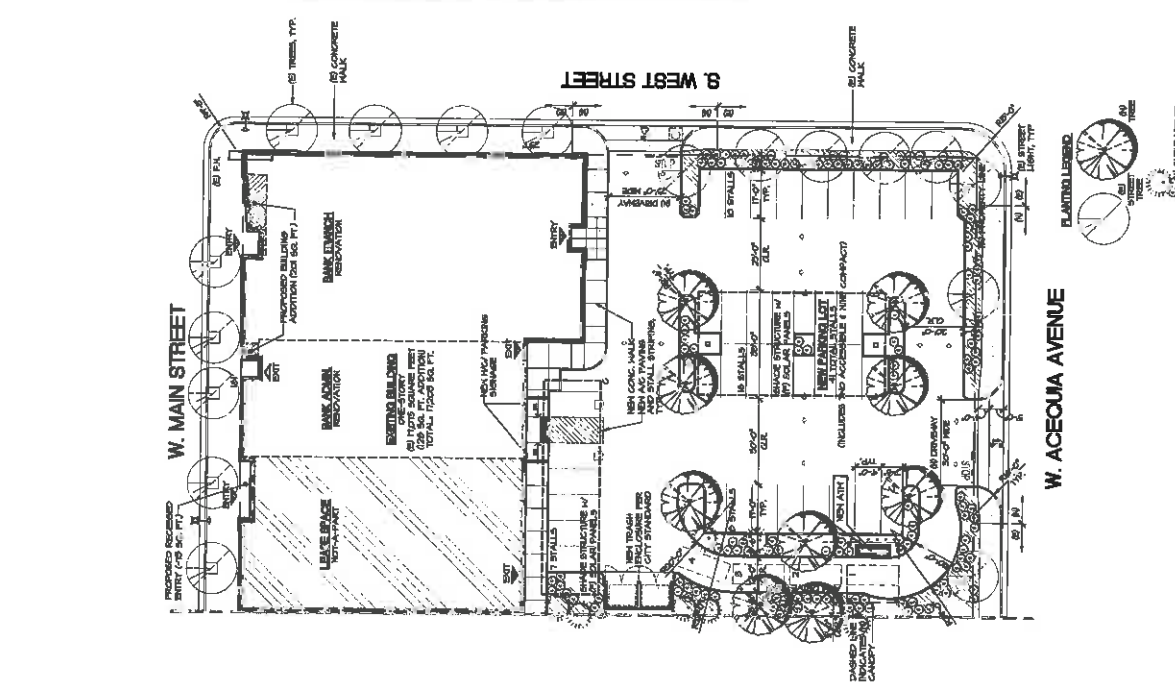
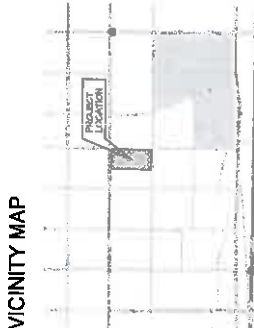
PROJECT AGENCIES

CITY OF VISALIA:
 501 W. ACEQUIA AVENUE
 VISALIA, CA 93278

PLUMBING DEPARTMENT:
 501 W. ACEQUIA AVENUE
 VISALIA, CA 93278

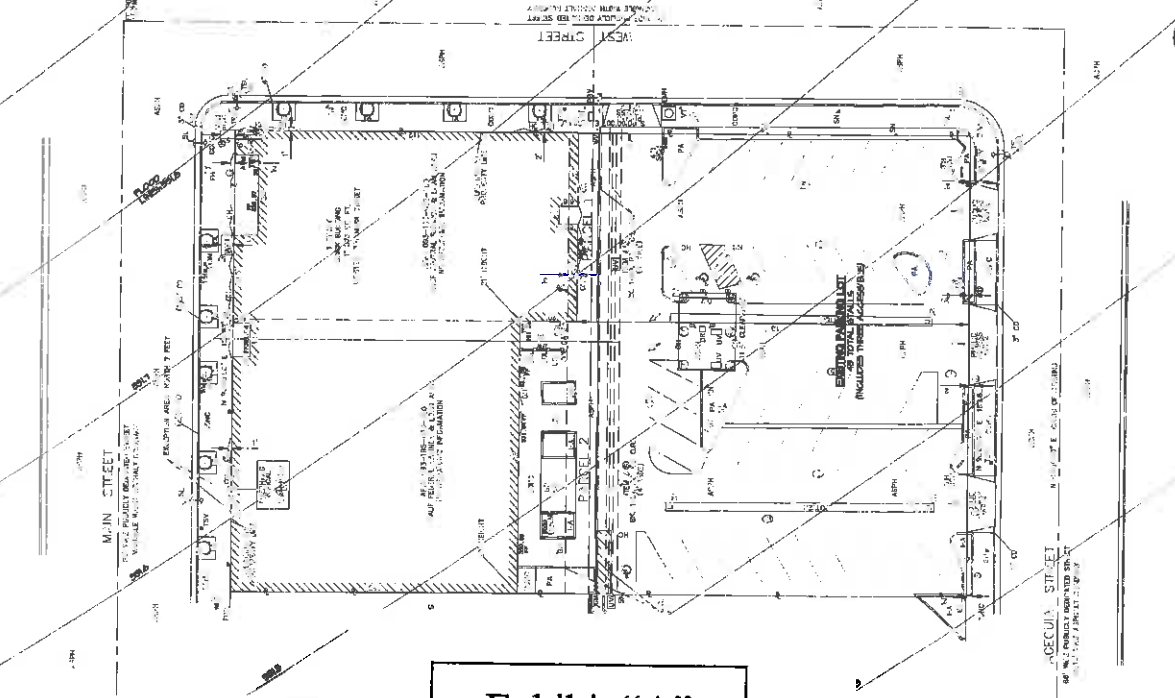
PERMITS DEPARTMENT:
 501 W. ACEQUIA AVENUE
 VISALIA, CA 93278

BUILDING SAFETY:
 501 W. ACEQUIA AVENUE
 VISALIA, CA 93278



PLANTING LEGEND:

- 1" TREE
- 2" TREE
- 3" TREE
- 4" TREE
- 5" TREE
- 6" TREE
- 7" TREE
- 8" TREE
- 9" TREE
- 10" TREE
- 11" TREE
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- 13" TREE
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- 92" TREE
- 93" TREE
- 94" TREE
- 95" TREE
- 96" TREE
- 97" TREE
- 98" TREE
- 99" TREE
- 100" TREE



OVERALL SITE PLAN - EXISTING

SCALE: 1" = 30'-0"

16

Exhibit "A"

DATE	DESCRIPTION
11/18/19	CLIP, SIGNATURE

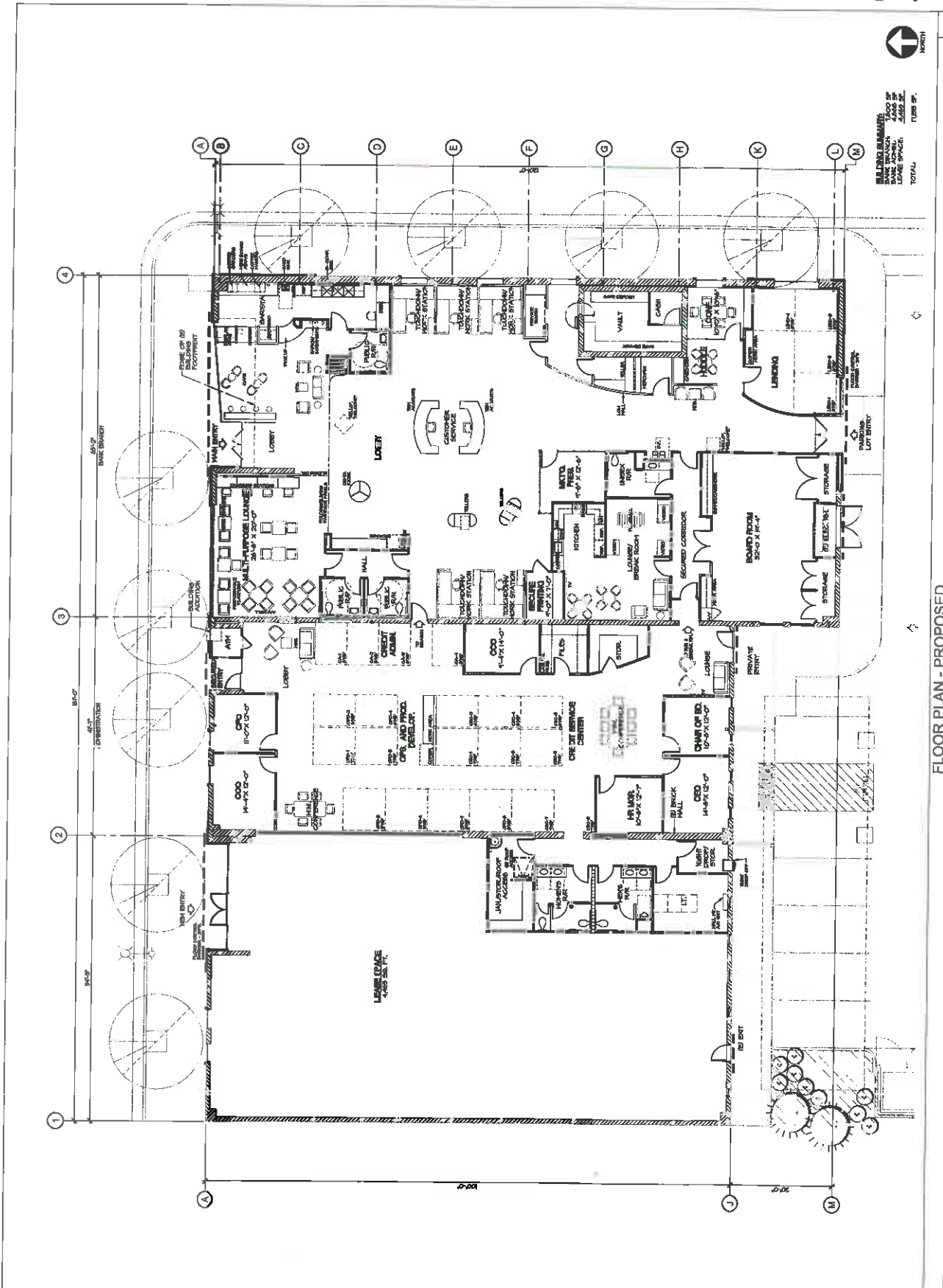


JETER, LLP
 278 N. HALE AVE., 8TH FLOOR, CA 94011 | 415.435.5000
 REGISTERED PROFESSIONAL ENGINEERS IN ARCHITECTURE AND CIVIL ENGINEERING
 ARCHITECTS ENGINEERS CONTRACTORS



BUILDING RENOVATION
 SUNCREST BANK
 501 W. MAIN STREET
 VISALIA, CA
 DRAWING TITLE
 FLOOR PLAN - PROPOSED

PROJECT NO. E-19-012
 DRAWING NO. A201
 DATE: 11/18/19



BUILDING BARRIERS
 10000 SF
 4000 SF
 4000 SF
 4000 SF
 TOTAL
 16000 SF

SCALE: 1/8" = 1'-0"
 NORTH

FLOOR PLAN - PROPOSED

Exhibit "B"

KEYNOTES

DATE	4/18/16	DATE	4/18/16
BY	CLM, S. BARRON	BY	CLM, S. BARRON
DESCRIPTION		DESCRIPTION	
REVISIONS		REVISIONS	

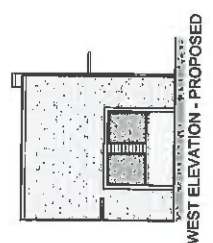
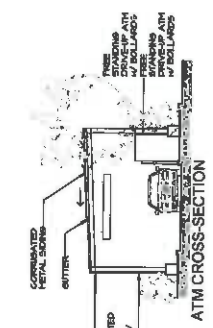
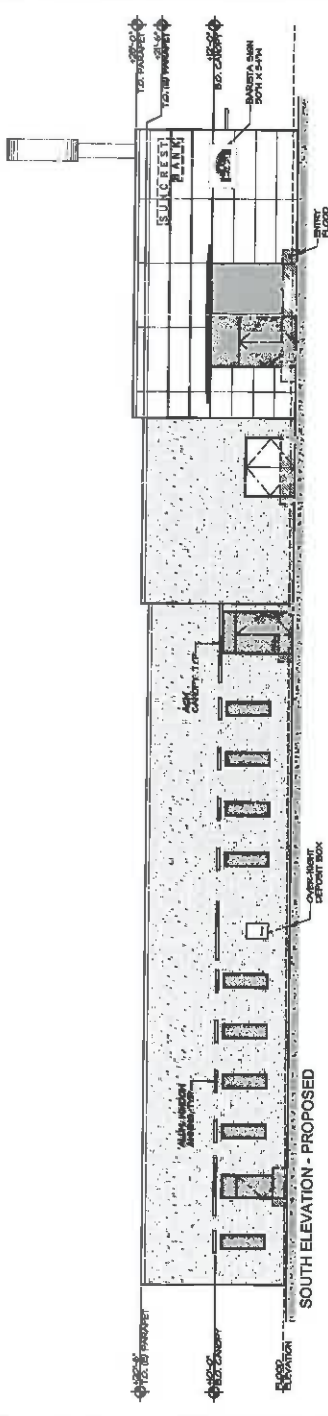
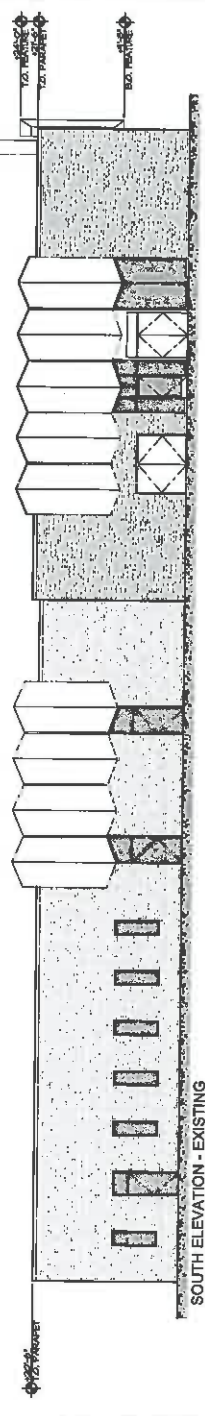
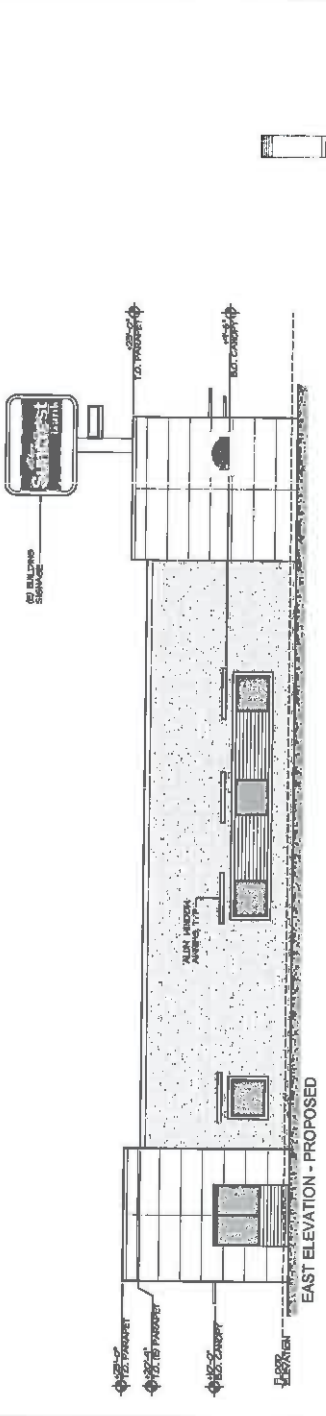
NOT FOR CONSTRUCTION

TRTTR, LLP
 ARCHITECTS ENGINEERS CONTRACTORS
 1300 N. BROAD ST. SUITE 100 | PALMDALE, CA 93550 | TEL: 805.791.1000
 WWW.TRTR.COM | INFO@TRTR.COM | WWW.TRTR.COM



BUILDING RENOVATION
 SUNCREST BANK
 501 W. MAIN STREET
 VISALIA, CA
 DRAWING TITLE
 EXTERIOR ELEVATIONS

PROJ. NO. 15-0442
 DRAWING
 A301



LEGEND

GENERAL NOTES

SCALE: 1/8" = 1'-0"

EXTERIOR ELEVATIONS



Exhibit "D"













MEETING DATE March 2, 2016
SITE PLAN NO. 16-029
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

- RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.
- During site plan design/policy concerns were identified, schedule a meeting with
- Planning Engineering prior to resubmittal plans for Site Plan Review.
- Solid Waste Parks and Recreation Fire Dept.

REVISE AND PROCEED (see below)

- A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.
- Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.
- Your plans must be reviewed by:
- CITY COUNCIL REDEVELOPMENT
- PLANNING COMMISSION PARK/RECREATION
- CUP
- HISTORIC PRESERVATION Other

ADDITIONAL COMMENTS :

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

Site Plan Review Committee



ITEM NO: 7

DATE: March 02, 2016



Site Plan Review Comments For:

City of Visalia
Fire Department
707 W Acequia
Visalia, CA 93291
559-713-4261 office
559-713-4808 fax

SITE PLAN NO: SPR16029
PROJECT TITLE: SUNCREST BANK
DESCRIPTION: NEW PARKING LOT AND BUILDING RENOVATION (CDT) (AE) (DRD)
APPLICANT: TETER A+E
PROP OWNER: SUNCREST BANK
LOCATION: 501 W MAIN ST
APN(S): 093-195-005

The following comments are applicable when checked:

- The Site Plan Review comments are issued as general overview of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2013 California Fire Code (CFC), 2013 California Building Codes (CBC) and City of Visalia Municipal Codes.
- All fire detection, alarm, and extinguishing systems in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If building has been vacant for a significant amount of time, the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. *2013 CFC 901.6*
- No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire & life safety requirements including fire protection.
- More information is needed before a Site Plan Review can be conducted. Please submit plans with more detail. Please include information on

General:

- Address numbers must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses are served by a common driveway, the range of numbers shall be posted at the roadway/driveway. *2013 CFC 505.1*
- A Knox Box key lock system is required. Where access to or within a structure or an area is restricted because of secured openings (doors and/or gates) or for fire-fighting purposes, a key box is to be installed in an approved location. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Acequia Ave. Please allow adequate time for shipping and installation.) *2013 CFC 506.1*
- All hardware on exit doors shall comply with Chapter 10 of the 2013 California Fire Code. This includes all locks, latches, bolt locks, and panic and fire exit hardware.
- Provide illuminated exit signs and emergency lighting through-out building. *2013 CFC 1011*
- When portion of the building are built upon a property line or in close proximity to another structure the exterior wall shall be constructed as to comply *2013 California Building Code Table 508.4 and Table 602.*

- Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. *2013 CFC 304.3.3*
- If your business handles hazardous material in amounts that exceed the Maximum Allowable Quantities listed on *Table 5003.1.1(1), 5003.1.1(2), 5003.1.1(3) and 5003.1.1(4) of the 2013 California Fire Code*, you are required to submit an emergency response plan to the Tulare County Health Department. Also you shall indicate the quantities on your building plans and prior to the building final inspection a copy of your emergency response plan and Safety Data Sheets shall be submitted to the Visalia Fire Department.

Water Supply:

- Construction and demolition sites shall have an approved water supply for fire protection, either temporary or permanent, and shall be made available as soon as combustible material arrives on the site. *2013 CFC 3312*
- No additional fire hydrants are required for this project; however, additional fire hydrants may be required for any future development.
- There is/are fire hydrants required for this project. (See marked plans for fire hydrant locations.)
- Fire hydrant spacing shall comply with the following requirements:

The exact location of fire hydrants and final decision as to the number of fire hydrants shall be at the discretion of the fire marshal, fire chief and/or their designee. *Visalia Municipal Code 16.36.120 & 16.36.120(8)*

 - Single-family residential developments shall be provided with fire hydrants every six hundred (600) lineal feet of residential frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Multi-family, zero lot line clearance, mobile home park or condominium developments shall be provided with fire hydrants every four hundred (400) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Multi-family or condominium developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every six (600) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Commercial or industrial developments shall be provided with fire hydrants every three hundred (300) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Commercial or industrial developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every five hundred (500) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
- When any portion of a building is in excess of one hundred fifty (150) feet from a water supply on a public street there shall be provided on site fire hydrants and water mains capable of supplying the required fire flow. *Visalia Municipal Code 16.36.120(6)*

Emergency Access:

- A construction access road is required and shall be a minimum of 20 feet wide. The road shall be an all-weather driving surface accessible prior to and during construction. The access road shall be capable of holding 75,000 pound piece of fire apparatus, and shall provide access to within 100 feet of temporary or permanent fire department connections. *2013 CFC 3310*

- Buildings or portions of buildings or facilities with a vertical distance between the grade plans and the highest roof surface exceed 30 feet shall provide an approved fire apparatus access roads capable of accommodating fire department aerial apparatus. Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders. Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building. *2013 CFC D105*

- A fire apparatus access roads shall be provide and must comply with the CFC and extend to within 150 of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility. Minimum turning radius for emergency fire apparatus shall be 20 feet inside radius and 43 feet outside radius. *2013 CFC 503.1.1*

- Fire apparatus access roads in excess of 150 feet and dead end shall be provided with a turnaround. Length 151-500 feet shall be a minimum of 20 feet in width and have a 120 foot Hammerhead, 60-foot "Y" or 96-Foot diameter Cul-de-sac in accordance with Figure D103.1 of the 2013 CFC. Length 501-750 feet shall be 26 feet in width and have a 120 foot Hammerhead, 60-foot "Y" or 96-Foot diameter Cul-de-sac in accordance with Figure D103.1 of the 2013 CFC.

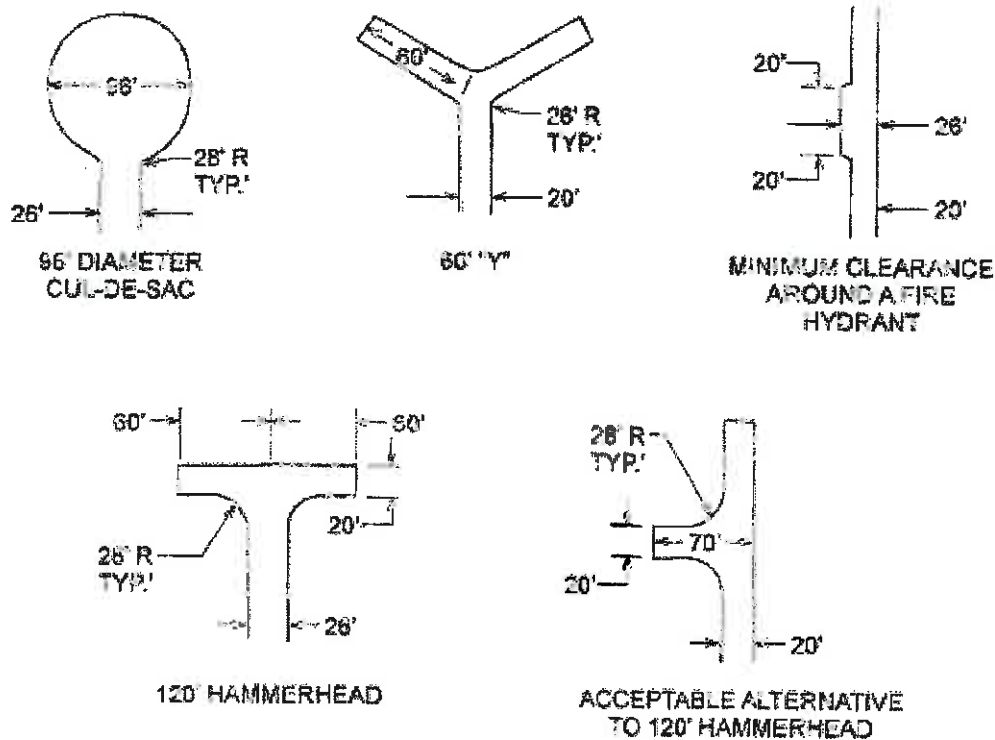



FIGURE D103.1
DEAD-END FIRE APPARATUS ACCESS ROAD TURNAROUND

- Gates on access roads shall be a minimum width of 20 feet and shall comply with the following:
2013 CFC D103.5
- Typical chain and lock shall be the type that can be cut with a common bolt cutter, or the developer may opt to provide a Knox Box key lock system.
 - Gates shall be of the swinging or sliding type.
 - Gates shall allow manual operation by one person. (power outages)
 - Gates shall be maintained in an operative condition at all times.
 - Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Acequia Ave. Please allow adequate time for shipping and installation.)
- In any and all new One- or two-family dwellings residential developments regardless of the number of units, street width shall be a minimum of 36 feet form curb to curb to allow fire department access and to permit parking on both sides of the street. A minimum of 20 feet shall be provided for developments that don't allow parking on the streets. *2013 CFC D107.2*

Fire Protection Systems:

- An automatic fire sprinkler system will be required for this building. Also a fire hydrant is required within 50 feet of the Fire Department Connection (FDC). *2013 CFC 903 and Visalia Municipal Code 16.36.120(7)*
- Commercial cooking appliances and domestic cooking appliances used for commercial purposes that produces grease laden vapors shall be provided with a Type I Hood, in accordance with the California Mechanical Code, and an automatic fire extinguishing system. *2013 CFC 904.11& 609.2*

Special Comments:



Maribel Vasquez
Fire Inspector

SITE PLAN REVIEW COMMENTS

Paul Bernal, Planning Division (559) 713-4025

Date: March 2, 2016

SITE PLAN NO: 2016-029
PROJECT TITLE: SUNCREST BANK
DESCRIPTION: NEW PARKING LOT AND BUILDING RENOVATION (CDT) (AE) (DRD)
APPLICANT: TETER A+E
PROP. OWNER: SUNCREST BANK
LOCATION TITLE: 501 W MAIN ST
APN TITLE: 093-195-005
GENERAL PLAN: Downtown Mixed Use
EXISTING ZONING: CDT – Central Business District

Planning Division Recommendation:

- Revise and Proceed
 Resubmit

Project Requirements

- Conditional Use Permit for drive-thru facility
- Parking District "A"
- PBID
- Building Permits
- Additional Information as Needed

PROJECT SPECIFIC INFORMATION: 03/02/2016

1. A CUP is required for the new ATM drive-thru. The new drive-thru lane does not meet the City's Drive-thru performance standards (see below).
2. The sites reconfigured parking lot complies with City standards. The parking lot redesign increases the number of on-site parking spaces from 39 stalls to 41 stalls.

- Staff initial finding is that the proposed site plan IS CONSISTENT with the City General Plan. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

Drive-thru Performance Standards (Section 17.32.162)

- A. Purpose and Intent: It is the purpose of this section to specify performance standards applicable to uses that seek to incorporate a drive-thru lane in associate with specified use. This section does not apply to carwashes and lube and oil changing stations.
- B. Performance Standards:
1. Separation from residences: The drive-thru lane shall be no less than 250 feet from the nearest residence or residentially zoned property.
 2. Stacking: The drive-thru lane shall contain no less than ten vehicle stacking, measured from the pickup window to the designated entrance to the drive-thru lane. There shall be no less than three vehicles spaces distance from the order menu/speaker (or like device) to the designated entrance to the order window.
 3. Circulation: No portion of the drive-thru lane shall obstruct any drive aisles or required on-site parking. The drive thru shall not take ingress or egress from a local residential road.
 4. Noise: no component or aspect of the drive-thru lane or its operation shall generate noise levels in excess of 60db between the hours of 7:00 p.m. and 6:00 a.m. daily.

5. **Screening:** The entire drive-thru lane shall be screened from adjacent street and residential view to a height of three feet. Screening devices shall be a combination of berming, hedge and landscape materials, and solid walls as approved by the City Planner.
6. **Menu boards and signage:** Shall be oriented or screened to avoid direct visibility from adjacent public streets.

Downtown Parking District: [Zoning Ordinance Section 17.30.019A Article 2]

1. The site maintains a parking lot with 39 parking stalls. The redesigned parking lot will increase the number of parking stalls to 41.

Parking:

1. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot (Zoning Ordinance Section 17.34.030.I).
2. Provide handicapped space(s) (see Zoning Ordinance Section 17.34.030.H).
3. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.34.040.D & 17.30.130.C).
4. It is highly recommended that bicycle rack(s) be provided on site plan.
5. Parking lot to be screened from view by a 3-foot tall solid wall or shrubs when located adjacent to a public street.

Fencing and Screening:

1. Provide screened trash enclosure with solid screening gates (Zoning Ordinance Section 17.30.130.F).

Landscaping:

1. The City has adopted the State Water Efficient Landscape Ordinance. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance. **NOTE: Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.**
2. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.130.C).
3. All landscape areas to be protected with 6-inch concrete curbs (Zoning Ordinance Section 17.30.130.F).
4. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months.
5. Provide a detailed landscape and irrigation plan as a part of the building permit package (Zoning Ordinance Section 17.34.040).
6. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.30.130.C).
7. Maintenance of landscaped areas. - A landscaped area provided in compliance with the regulations prescribed in this title or as a condition of a use permit or variance shall be planted with materials suitable for screening or ornamenting the site, whichever is appropriate, and plant materials shall be maintained and replaced as needed, to screen or ornament the site. (Prior code § 7484)

Lighting:

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.
2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

DOWNTOWN RETAIL DESIGN DISTRICT BUILDING DESIGN CRITERIA

Amended April 1996

A. General

The following standards shall apply to all building alterations or new construction within the downtown retail design district

1. Definitions

- a. Owner: Shall mean the person or persons in actual charge of the structure or a portion of the structure. It includes:
 1. The agent or agents of the owner specifically charged with responsibility for maintenance of the structure by a written contractual agreement.
 2. The Occupants specifically charged with responsibility for maintenance of the structure by a written contractual agreement.
 3. May, should and preferred are permissive in nature and indicate that compliance is discretionary on the part of the applicant. Where these are not used, compliance is mandatory.

2. Color – section deleted

3. Signs – signs should contribute to the commercial quality and character of the district. Within the district, the following standards for signage are prescribed:

a. Primary Signage

- 1) Material: Plastic is prohibited as a primary sign material.
- 2) Size:

- a. Two (2) square feet of sign area is permitted for each one (1) foot of linear occupancy frontage top a maximum of fifty (50) square feet.

- b. The sign area as calculated for the occupancy frontage may be used on the occupancy frontage, or may be divided between the occupancy frontage And any other exterior wall. (see DRD for more)

- 3) Location: Users may choose which exterior side of the building will be used for the purpose of calculating the permitted sign area. The exterior side chosen is the primary occupancy frontage. The building sign so calculated shall be mounted on the primary occupancy frontage.

- b. On Awnings: A building sign may be affixed to or incorporated as a part of the design of the front valance only of an awning; however, such sign area shall be deducted

from that calculated for the exterior building wall to which the awning is attached. Numerals used for the purpose of identifying street addresses need not be deducted from the calculated sign area.

- c. Street addresses
- d. Additional signs of a maximum twenty-five percent of the sign area calculated for the primary occupancy frontage will be allowed for each remaining exterior wall, provided that the sign for any given wall does not exceed two square feet per linear foot of the wall length. This subsection does not apply to alley frontages visible from center or Acequia streets.
- e. Pedestrian oriented sign: one double faced sign not exceeding an area of three (3) square feet per face, to be constructed of wood, with a carved and/or painted logo. Pedestrian oriented signs shall be a minimum of 8 feet above the sidewalk and shall not exceed a maximum height of 12 feet.
- f. A pedestrian oriented sign may also be affixed to or incorporated into the design of the side valance of awnings which are perpendicular to the store frontage. Both side valances of an awning may be so utilized, however, only one sign face per awning side is allowed. Each sign face shall not exceed three (3) square feet. If this option is chosen, no other pedestrian oriented sign is allowed.
- g. The building sign area as calculated on the primary occupancy frontage may also be used on the alley frontage for those businesses with direct pedestrian entrances from alleyways, where such entrances and frontages are visible from center or Acequia streets. In no case shall the sign area used on the alley frontage exceed that permitted for the primary occupancy frontage.
- h. Each building may display a reader board of a maximum area of twenty (20) square feet indicating the name, address and type of business of the businesses within the building. If the reader board is located on a part of the building qualifying as an occupancy frontage for which sign area is calculated, the sign area used for the reader board shall be deducted from the total permitted for that building.

Reader boards shall be designed as one with each copy panel consisting of similar materials and designs. Permits for reader boards shall not be issued without consent of the property owner.
- i. Within the district, internally illuminated signs are prohibited, with the exception of neon.
- j. Plastic is prohibited as a primary sign material.
- k. Each commercial use which has direct pedestrian access through an exterior building wall which is visible from a public right-of-way, shall be allowed at least ten (10) square feet of building sign area, regardless of building occupancy frontage. Commercial uses having a sole access from the interior of any building or from an enclosed lobby or court shall not be allowed the minimum building sign area referred to in this section.
- l. Permanent signage painted or affixed to the surface of retail display windows shall cover no more than twenty percent (20%) of the display window area and shall be allowed in addition to the calculated building sign area.
- m. Temporary signs pertaining to special store events or sales lasting no more than thirty (30) days may be painted or affixed to display windows provided that their total area shall not exceed thirty percent (30%) of the window. Such temporary sign or signs shall be removed within seven (7) days of the completion of said event or sale. No permit is required for such temporary sign(s).

4. Awnings – awnings attached above street level storefronts and/or upper-story windows are encouraged. Size and scale shall be appropriate to the building, however, and significant elements of the building's architecture should be obscured by upper-story awnings. Ground floor awnings shall project a minimum of five (5) feet over the sidewalk. Awning materials shall be cloth or canvas. Awning colors shall be limited to a blue, burgundy, tan or tones and/or prints of these. White may be incorporated into the design of awnings for contrast or valance signage, but shall not compromise a majority of the color theme of any awning.
5. Prohibited material and elements.
 - a. Use of wood siding, cladding or wood shingles, in excess of ten percent (10%) of the total area of any building façade.
 - b. Mansard form roof tiles.
 - c. Unbroken masses of split face, slump stone or concrete unit masonry.
 - d. Use of reflective or mirrored surface cladding, in excess of then percent (10%) of the total area of any building façade.
 - e. Exposed utility conduit, junction boxes, meters, or fuse boxes on the front façade of buildings.

B. Renovation

The following criteria should be followed when undertaking the renovation of existing buildings in the district.

1. Facades -
 - a. Where originally constructed buildings facades remain, their appearance shall not be altered. Such facades shall be repaired and preserved. Where facades have been altered, as much original material and detail shall be retained in the rehabilitation as possible.
 - b. Where most of the existing architectural design dated from an interim remodeling and where such remodeling adds to the traditional character of the district, rehabilitation shall conform to the period of such remodeling and not to the original design.
 - c. Where the original design cannot be determined or where financial considerations preclude full-scale rehabilitation of a façade which has previously been altered, a design which is not a pure rehabilitation but which is in keeping of the structure are prohibited.
 - d. Where originally constructed facades and/or architectural details have been covered by an interior remodeling, the removal of coverings is encouraged. When original materials, facades and/or architectural details are uncovered, these shall not be recovered.
2. Windows and doors – where they still exist, the original sills, lintels, frames, sash, muntins and glass of windows and transoms shall be preserved. The original doorway elements, including sill, lintels, frames, and the doors shall also be retained. Where possible, replacements should duplicate the originals in design and materials. The blocking or covering of any portion of an existing window or door opening with permanent materials is prohibited. When new window or door openings are created the scale of these should approximate that of the traditional architecture of the district. The base of new window opening shall be a maximum of 30 inches above ground level. Whenever the size and/or scale of any existing window openings are altered, the base of all the window openings in the storefront shall be brought into conformance with the maximum 30-inch standard. This section shall not apply to ordinary repair or replacement of window glass or frames where the size and/or scale of window openings are not altered.
3. Building materials

- a. Brick is most dominant traditional building material within the district and is preferred as a replacement material. Brick shall not be covered by any synthetic material, asbestos. Or wood shingles, wood or aluminum siding, or any other synthetic materials. Where brick has been painted, repainted in a color which matches the natural color of the brick as closely as possible is preferred. Where brick remains unpainted, the use of paint in the exterior is prohibited, since unpainted brick is a crucial element of the traditional character of the district.
- b. Stuccoed brick surfaces should be retained, unless held in place with wire mesh, in which case the stucco should be removed. Where retained, stucco should be repainted in a brick color.

C. New building standards

The objective of this section is to ensure that contemporary design is compatible with the traditional theme and character of the downtown retail district. The effect of the proposed design of new construction on the overall character of the district should be the first frame of reference for such compatibility. The next consideration should be the effect of the proposed design on the adjacent buildings and streetscape.

1. New construction shall maintain the continuity of existing rows of buildings; facades shall be constructed at the property line facing the street, or at a setback even with that of adjacent buildings.
2. New buildings shall be constructed to within ten percent (10%) of the maximum height of adjacent buildings; however, in no case shall a building exceed fifty (50) feet in height. The scale of new architectural elements should be consistent with that of adjacent structures.
3. Brick is the preferred exterior building material for new construction. The color texture should be similar to that of brick traditionally used in the district.
4. The scale of window and door openings in new buildings should approximate that of the traditional architecture of the district. The base of window openings shall be a maximum of 30 inches above ground level.

D. Maintenance and repair required

Neither the owner of nor the person(s) in actual charge of a structure within the district shall permit such structure to fall into a state of disrepair which may result in the deterioration of any exterior appurtenance or architectural features so as to produce, or tend to produce, in the judgment of the planning commission, a detrimental effect on the character of the district as a whole or the life and character of the structure in question, including, but not limited to;

- a. The deterioration or decay of exterior walls or other vertical supports;
- b. The deterioration of roofs or other horizontal members;
- c. The deterioration of exterior chimneys;
- d. The extensive deterioration or crumbling of exterior plaster or mortar;
- e. The extensive peeling or shipping of exterior paint;
- f. The deterioration of any feature so as to create or permit the creation of any hazardous or unsafe condition or conditions

The person(s) in actual charge of any structure is(are) deemed to be the owner(s) of the structure or any portion thereof, unless the occupant(s) or agent(s) of the owner(s) are specifically charged with the responsibility for maintenance of the structure by a lease or other written contractual agreement between the owner(s) and the occupant(s) or agent(s).

The city's zoning compliance officer, or other designated enforcement official shall serve written notice upon the owner and/or other person in actual charge of any structure deemed to be in violation of these provision. Said written notice shall specify the nature of the condition or conditions which are in violation of the maintenance and repair requirement and direct that said conditions be repaired or corrected within an appropriately reasonable period of time. Said notice shall further advise the owner and/or the person in actual charge of said

structure of his/her right to request a hearing before the planning commission to review the determination of the city's enforcement officer. The planning commission shall adopt reasonable rules and regulations comporting with appropriate due process for the notice and conduct of such hearings pursuant to this provision. The planning commission may approve, modify, or reject the determination of the city's enforcement officer at the conclusion of such hearing. Should the owner and/or person in actual charge of subject structure fail and/or refuse to request a hearing, should the planning commission determine at the conclusion of such a hearing that there is a violation of this provision, the planning commission shall require that the structure in question be brought into compliance within an appropriately reasonable period of time. Thereafter, any person who fails to comply with the requirements of this provision shall be deemed to be guilty of an infraction and shall be subject to the provisions and penalties provided in the city's ordinance code relating to zoning.

E. Minimum alterations required

At a minimum, all structures and architectural features of structures, including awnings, marquees, and other appurtenances within the district shall conform to the color and repair requirements stated in paragraphs a.1., A.3., And d. Above. Any improvement or architectural feature found not meeting these standards shall be repainted or otherwise changed to reflect appropriate colors and appearance within one (1) year, provided that the estimated cost of bringing a structure into compliance is in excess of \$2000. The colors of cloth or canvas awnings shall be changed by replacement of the awning fabric, rather than painting. The time limits for complying with paragraphs a.1. And d. shall begin to run from the date of adoption of all these building design criteria by the city council. The responsibility for meeting the standards of this paragraph shall rest with the person(s) in actual charge of any structure. The person(s) in actual charge is(are) deemed to be the owner(s) of the structure or any portion thereof, unless the occupant(s) or agent(s) or the owner(s) are specifically charged with responsibility for maintenance of the structure by a lease or other written contractual agreement between the owner(s) and the occupant(s) or agent(s).

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments. The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

Signature



SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

March 2, 2016

ITEM NO: 7
SITE PLAN NO: SPR16029
PROJECT TITLE: SUNCREST BANK
DESCRIPTION: NEW PARKING LOT AND BUILDING RENOVATION (CST) (AE) (DRD)
APPLICANT: TETER A+E
PROP. OWNER: SUNCREST BANK
LOCATION: 501 W MAIN ST
APN(S): 093-195-005

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- No Comments
- See Previous Site Plan Comments
- Install Street Light(s) per City Standards.
- Install Street Name Blades at Locations.
- Install Stop Signs at Locations.
- Construct parking per City Standards PK-1 through PK-4.
- Construct drive approach per City Standards.
- Traffic Impact Analysis required.

Additional Comments:

- Parking stalls in northwest corner of lot – How are vehicles to enter and back out of? Location interferes with drive thru ATM.



Leslie Blair

**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

- Jason Huckleberry 713-4259
- Adrian Rubalcaba 713-4271

ITEM NO: 7 DATE: MARCH 2, 2016

SITE PLAN NO.: 16-029
PROJECT TITLE: SUNCREST BANK
DESCRIPTION: NEW PARKING LOT AND BUILDING RENOVATION (CDT) (AE) (DRD)
APPLICANT: TETER A+E
PROP OWNER: SUNCREST BANK
LOCATION: 501 W MAIN ST
APN: 093-195-005

SITE PLAN REVIEW COMMENTS

- REQUIREMENTS (indicated by checked boxes)
- Install curb return with ramp, with _____ radius; **SEE ADDL COMMENTS**
- Install curb; gutter **AT DRIVE APPROACH DEMO'S**
- Drive approach size: **25' MIN** Use radius return; **REFER TO OFFICE/COMMERCIAL STANDARDS**
- Sidewalk: _____ width; _____ parkway width at **REPLACE IN-KIND TO CITY STANDARDS**
- Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- Right-of-way dedication required. A title report is required for verification of ownership.
- Deed required prior to issuing building permit; **SEE ADDL COMMENTS**
- City Encroachment Permit Required. **ALL WORK WITHIN PUBLIC RIGHT-OF-WAY**
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.
- CalTrans Encroachment Permit required. CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;
- Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.
- Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. Prepared by registered civil engineer or project architect. All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) directed to the City's existing storm drainage system; b) directed to a permanent on-site basin; or c) directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: _____ : _____ maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
- Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
- Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)
- Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- Traffic indexes per city standards:

- Install street striping as required by the City Engineer.
- Install landscape curbing (typical at parking lot planters).
- Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- Provide "R" value tests: each at
- Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- Access required on ditch bank, 15' minimum Provide wide riparian dedication from top of bank.
- Show Oak trees with drip lines and adjacent grade elevations. Protect Oak trees during construction in accordance with City requirements.
- A permit is required to remove oak trees. Contact Joel Hooyer at 713-4295 for an Oak tree evaluation or permit to remove. A pre-construction conference is required.
- Relocate existing utility poles and/or facilities.
- Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- Subject to existing Reimbursement Agreement to reimburse prior developer:
- Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- Comply with prior comments. Resubmit with additional information. Redesign required.

Additional Comments:

- 1. Proposed renovation of building and parking lot will require additional improvements to comply with current accessibility standards. The curb ramp returns at Main/West & Acequia/West will need to be inspected for accessibility conformance; modify as necessary. The existing sidewalks surrounding the property may also require replacement in order to comply with current cross slope standards; redesign & modify accordingly. Refer to City standards.***
- 2. Proposed location of trash enclosure is adequate however a standard sized concrete apron will be required; revise accordingly. Refer to City standards for 24' refuse enclosure, gates, and apron.***
- 3. It is recommended the parking stall nearest the refuse enclosure be designated for low use due to the high potential for reverse interruption.***
- 4. Additional right-of-way may be necessary for the installation of new curb ramp returns. Those sections encroaching onto private property shall be dedicated to the City per the Grant Deed format and procedures located on the City's website. Further coordinate with the City Engineer.***
- 5. New curb section, from new drive approach on Acequia to West St. will need to be painted red for no parking. Refresh red curb paint along existing.***
- 6. Proposed building additions will incur impact fees. Refer to page 3 for fee summary.***
- 7. Building permits for site improvement and building renovations are required. Standard permit plan check and inspection fees apply.***

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: 16-029
Date: 3/2/2016

Summary of applicable Development Impact Fees to be collected at the time of building permit:
(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

(Fee Schedule Date: 9/4/2015)
(Project type for fee rates: OFFICE)

Existing uses may qualify for credits on Development Impact Fees. OFFICE

FEE ITEM	FEE RATE
<input type="checkbox"/> Groundwater Overdraft Mitigation Fee	
<input checked="" type="checkbox"/> Transportation Impact Fee	\$6,024/1000SF X .222 = \$1,337.33
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	\$90/1000SF X .222 = \$19.98
	TREATMENT PLANT FEE:
	\$214/1000SF X .222 = \$47.51
<input type="checkbox"/> Sewer Front Foot Fee	
<input type="checkbox"/> Storm Drain Acq/Dev Fee	
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input type="checkbox"/> Waterways Acquisition Fee	
<input type="checkbox"/> Public Safety Impact Fee: Police	
<input type="checkbox"/> Public Safety Impact Fee: Fire	
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$640/1000SF X .222 = \$142.08
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Adrian Rubalcaba

QUALITY ASSURANCE DIVISION

SITE PLAN REVIEW COMMENTS

ITEM NO: Z DATE: March 02, 2016
SITE PLAN NO: SPR16029
PROJECT TITLE: SUNCREST BANK
DESCRIPTION: NEW PARKING LOT AND BUILDING RENOVATION
 (CDT) (AE) (DRD)
APPLICANT: TETER A+E
PROP OWNER: SUNCREST BANK
LOCATION: 501 W MAIN ST
APN(S): 093-195-005

YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS:

- WASTEWATER DISCHARGE PERMIT APPLICATION
- SAND AND GREASE INTERCEPTOR - 3 COMPARTMENT _____
- GREASE INTERCEPTOR min. 1000 GAL
- GARBAGE GRINDER - ¾ HP. MAXIMUM _____
- SUBMISSION OF A DRY PROCESS DECLARATION _____
- NO SINGLE PASS COOLING WATER IS PERMITTED _____
- OTHER _____
- SITE PLAN REVIEWED - NO COMMENTS

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA
PUBLIC WORKS DEPARTMENT
QUALITY ASSURANCE DIVISION
7579 AVENUE 288
VISALIA, CA 93277



AUTHORIZED SIGNATURE

3-22-16

DATE

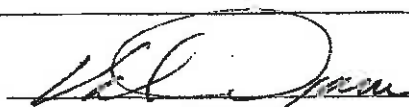
City of Visalia
Building: Site Plan
Review Comments

ITEM NO: 7 DATE: March 02, 2016
 SITE PLAN NO: SPR16029
 PROJECT TITLE: SUNCREST BANK
 DESCRIPTION: NEW PARKING LOT AND BUILDING RENOVATION (CDT) (AE) (DRD)
 APPLICANT: TETER A+E
 PROP OWNER: SUNCREST BANK
 LOCATION: 501 W MAIN ST
 APN(S): 093-195-005

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project. Please refer to the applicable California Codes & local ordinance for additional requirements.

- Business Tax Certification is required. *For information call (559) 713-4326.*
- A building permit will be required. *For information call (559) 713-4444*
- Submit 4 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)
- Submit 4 sets of plans prepared by an architect or engineer. Must comply with 2013 California Building Cod Sec. 2308 for conventional light-frame construction or submit 2 sets of engineered calculations.
- Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:**
- Meet State and Federal requirements for accessibility for persons with disabilities.
- A path of travel, parking, common area and public right of way must comply with requirements for access for persons with disabilities.
- Multi family units shall be accessible or adaptable for persons with disabilities.
- Maintain sound transmission control between units minimum of 50 STC.
- Maintain fire-resistive requirements at property lines.
- A demolition permit & deposit is required. *For information call (559) 713-4444*
- Obtain required clearance from San Joaquin Valley Air Pollution Board. Prior to any demolition work
For information call (661) 392-5500
- Location of cashier must provide clear view of gas pump island
- Plans must be approved by the Tulare County Health Department. *For information call (559) 624-7400*
- Project is located in flood zone AE * Hazardous materials report.
- Arrange for an on-site inspection. (Fee for inspection \$151.90) *For information call (559) 713-4444*
- School Development fees. Commercial \$0.54 per square foot. Residential \$3.48 per square foot.
- Existing address must be changed to be consistent with city address. *For information call (559) 713-4320*
- Acceptable as submitted
- No comments
- See previous comments dated: _____

Special comments: _____

 Date: 3/1/16
 Signature

see back of sheet

1) WHEN ALTERATIONS OR ADDITIONS ARE MADE TO EXISTING BUILDINGS OR FACILITIES, AN ACCESSIBLE PATH OF TRAVEL TO THE SPECIFIC AREA OF ALTERATION OR ADDITION SHALL BE PROVIDED UNLESS OTHERWISE EXEMPT.

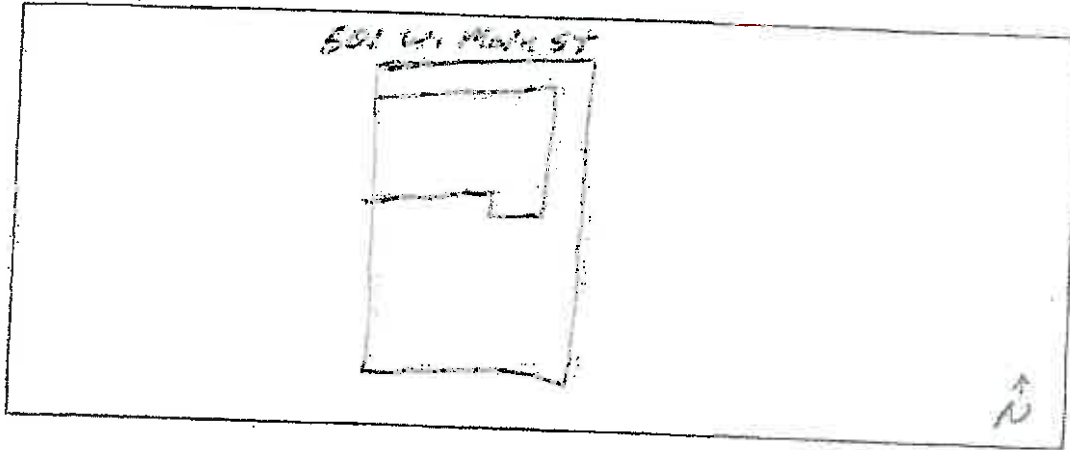
2) PRIMARY ACCESSIBLE PATH OF TRAVEL SHALL INCLUDE
① A PRIMARY ENTRANCE TO THE BUILDING OR FACILITY,
② TOILET FACILITIES SERVING THE AREA, ③ DRINKING FOUNTAINS SERVING THE AREA, PUBLIC TELEPHONES SERVING THE AREA, AND SIGNS.

City of Visalia
Parks and Urban Forestry
336 N. Ben Maddox Way
Visalia, CA 93292

Date: 3-1-16

Site Plan Review # 16029

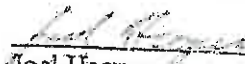
SITE PLAN REVIEW COMMENTS



COMMENTS: Sec Below None

- Please plot and protect all Valley-Oak Trees.
- Landscape along parkway to be planted by developer and maintained by a maintenance district.
- All drainage from curb and gutter along streets to be connected to storm drain system.
- All trees planted in street right-of-way to be approved by the Public Works Superintendent of Parks.
- Tie-ins to existing infrastructure may require a bore. Check with the Public Works Department prior to any street cut.

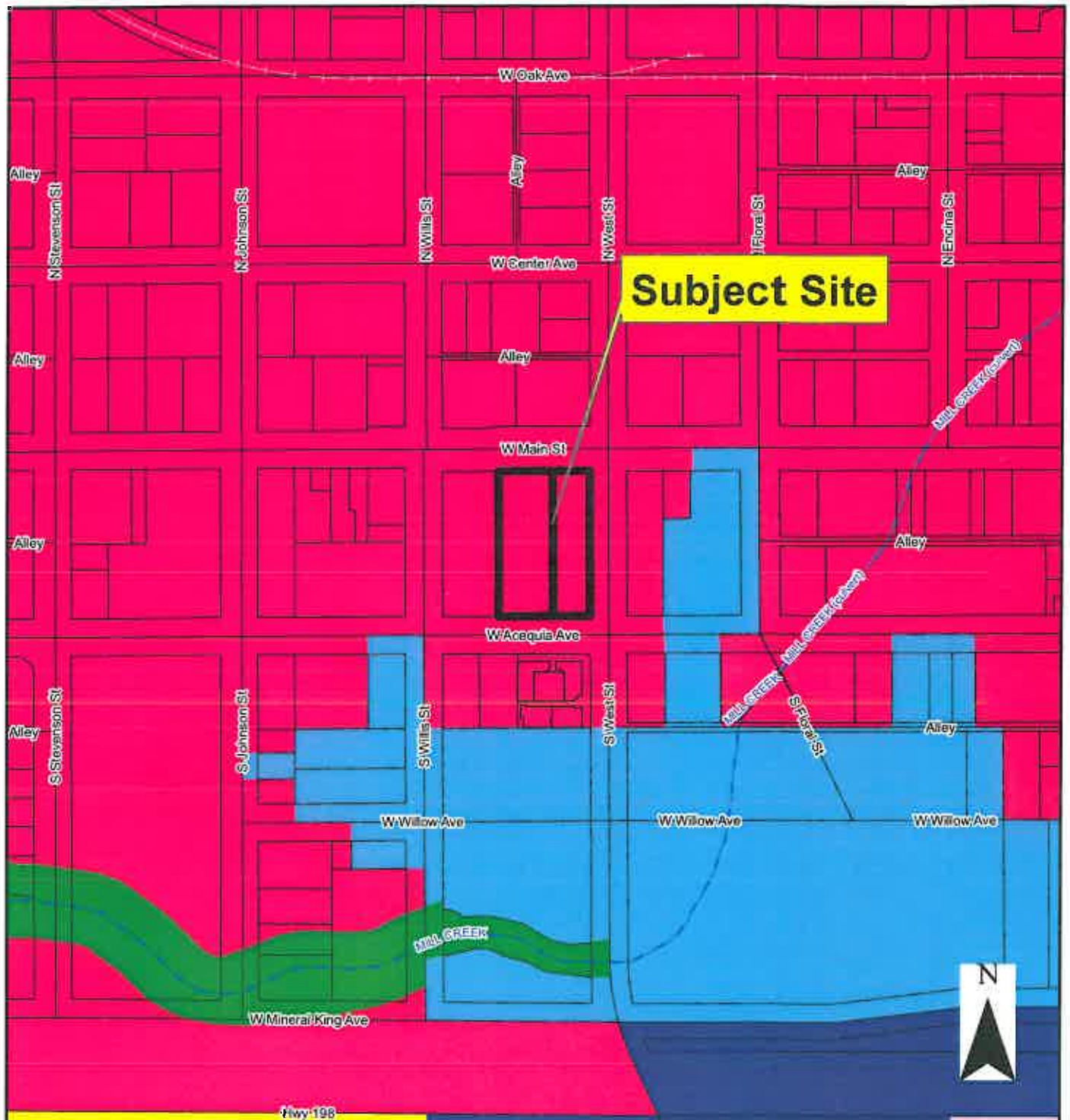
Other Comments: _____


Joel Hooyer
Parks and Urban Forestry Supervisor
559 713-4295 Fax 559 713-4818

Email: jhooyer@ci.visalia.ca.us

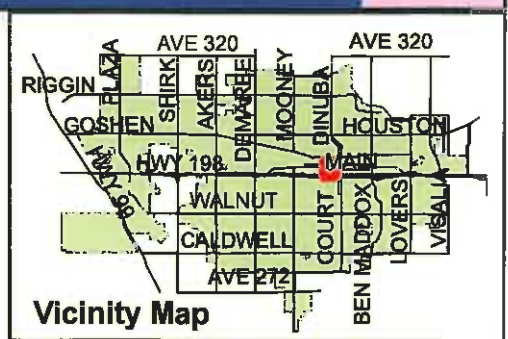
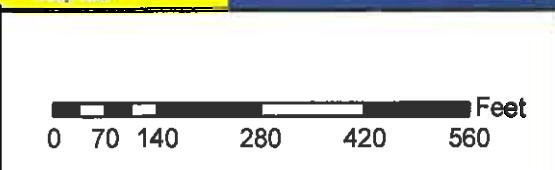
Conditional Use Permit No. 2016-11

General Plan Land Use Map

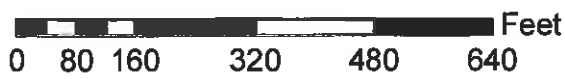
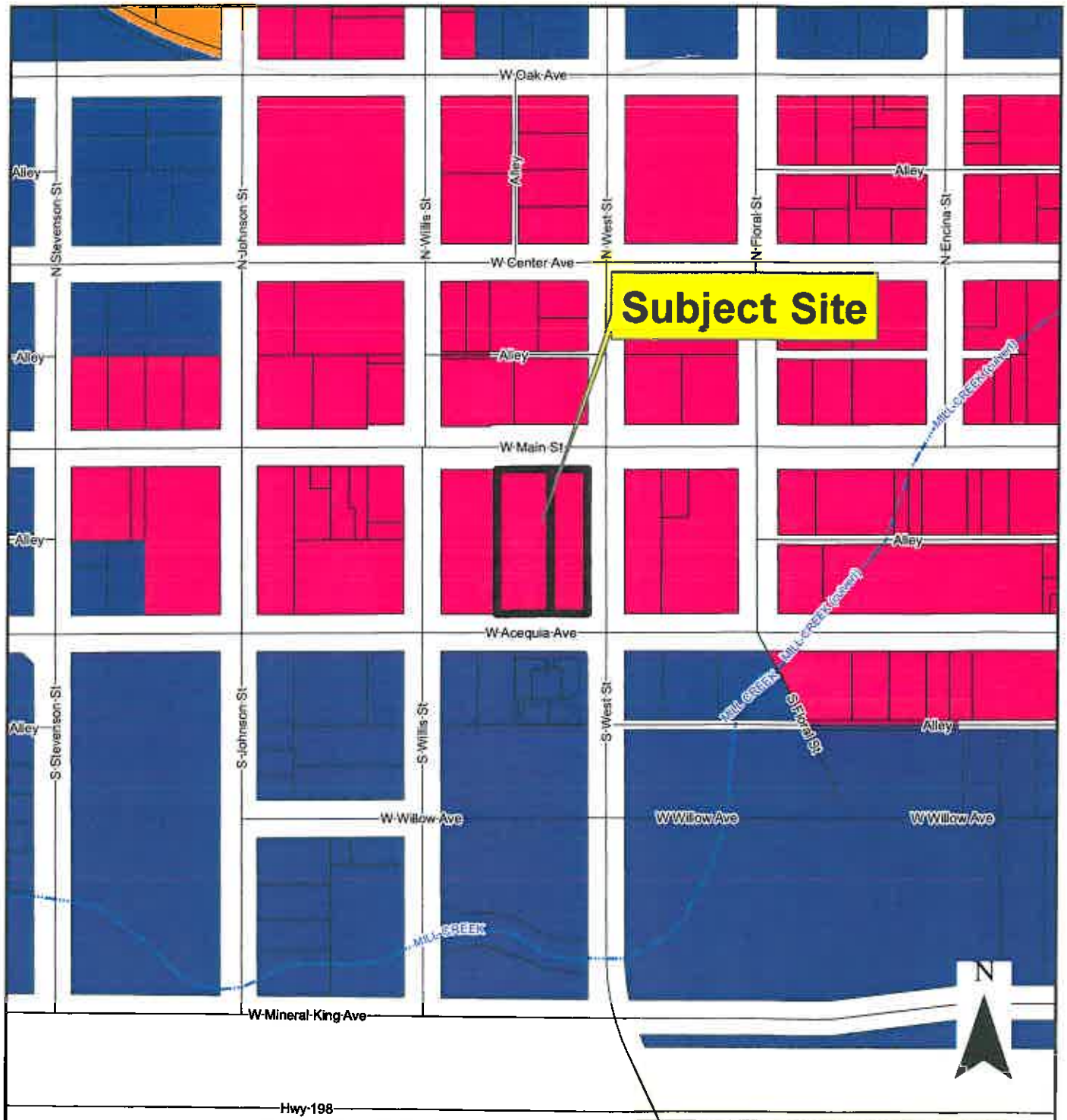


Legend

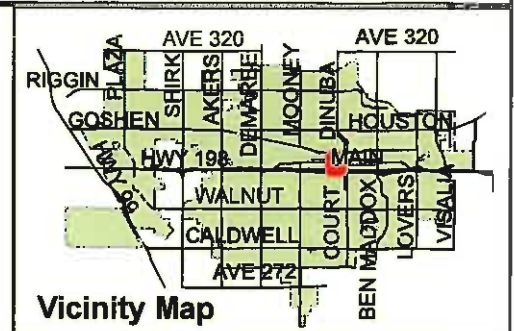
- Conservation
- Commercial Mixed Use
- Downtown Mixed Use
- Office
- Public Institutional
- Residential Low Density



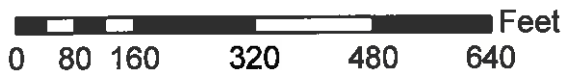
Conditional Use Permit No. 2016-11 Zoning Map



Legend	
	Central Business District
	Shopping / Office Commercial
	Professional / Admin. Office

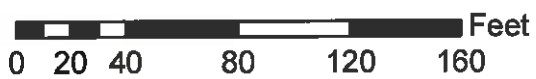


Conditional Use Permit No. 2016-11 Aerial Photo

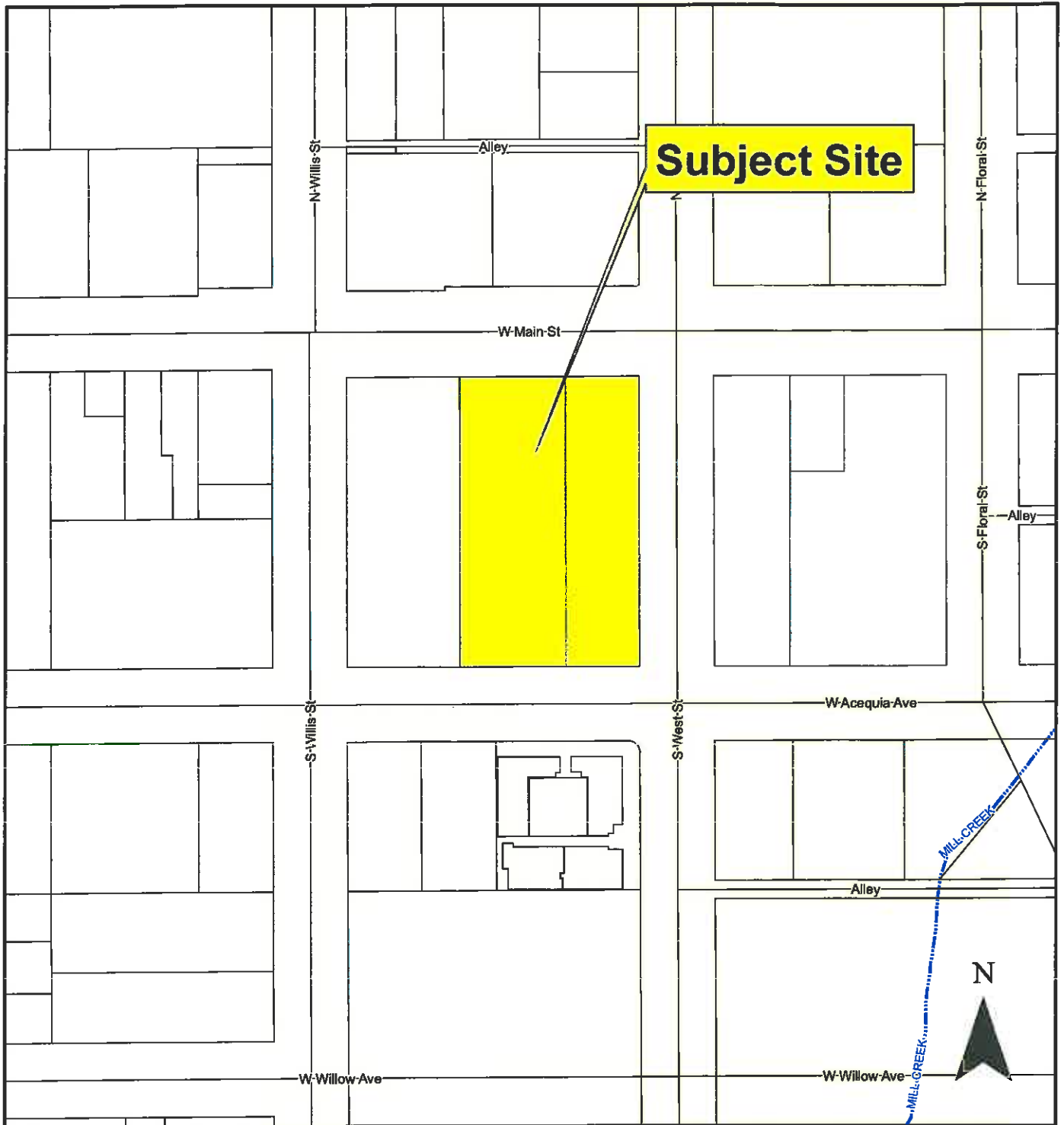


Conditional Use Permit No. 2016-11

Aerial Photo



Conditional Use Permit No. 2016-11 Location Map





REPORT TO CITY OF VISALIA PLANNING COMMISSION

HEARING DATE: June 13, 2016

PROJECT PLANNER: Paul Bernal, Principal Planner
Phone No.: (559) 713-4025
E-Mail: paul.bernal@visalia.city

SUBJECT: Conditional Use Permit No. 2016-09: A request by Vermeltfoort Architects Inc. to construct a new 4,767 square foot drive-thru carwash with an attached pay station, vacuum canopy area and a detached 245 square foot vacuum equipment building on 1.51-acres of an overall 17.01-acre site. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area. The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039).

Tentative Parcel Map No. 2016-06: A request by Lane Engineers Inc., to subdivide 17.01 acres into four parcels. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area. The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039).

STAFF RECOMMENDATION

Conditional Use Permit No. 2016-09

Staff recommends approval of Conditional Use Permit No. 2016-09, as conditioned, based upon the findings and conditions in Resolution No. 2016-18. Staff's recommendation is based on the conclusion that the request is consistent with the Visalia General Plan and Zoning Ordinance.

Tentative Parcel Map No. 2016-06

Staff recommends approval of Tentative Parcel Map No. 2016-06, based on the findings and conditions in Resolution No. 2016-29. Staff's recommendation is based on the conclusion that the request is consistent with the Visalia General Plan, Zoning and Subdivision Ordinances.

RECOMMENDED MOTION

I move to approve Conditional Use Permit No. 2016-09, based on the findings and conditions in Resolution No. 2016-18.

I move to approve Tentative Parcel Map No. 2016-06, based on the findings and conditions in Resolution No. 2016-29.

PROJECT DESCRIPTION

Conditional Use Permit No. 2016-09, is a request to construct a new 4,767 square foot drive-thru car wash with an attached pay station, vacuum canopy area and detached equipment room on 1.51 acres of an overall 17.01-acre site as depicted on Exhibit "A". Tentative Parcel Map No. 2016-06 is a request to subdivide the 17.01-acre site into four parcels ranging in size from 1.51-acres to 8.71-acres as depicted on Exhibit "B". The four parcels abut West Cameron Avenue, a defined collector street per the City's Circulation Element. If approved, Parcel 1 (1.51 acre) will be sold and developed with the Surf Thru Carwash facility as depicted on Exhibit "A".

The carwash tunnel is oriented in a north-south direction with vehicles entering from the south end of the building and exiting the tunnel at the north end (see Exhibit “C” Floor Plan). Vehicles entering the site from Cameron Avenue circulate the site in a counter-clockwise manner to enter the two-lane vehicle staging area located along the west side of the carwash tunnel as depicted on Exhibit “A”. The two-lane vehicle queue lanes provide for enough area to approximately accommodate 10 vehicles per lane. The carwash site provides for 17 covered vacuum stations. The vacuum equipment will be located within a detached 245 square foot building as depicted on Exhibit “A”. Parcel 1, of the tentative parcel map, is required to install sidewalks and the park strip landscaping along the Parcel 1 street frontage as depicted on Exhibits “A” and “K”.

The architectural elevations depict the building materials to be incorporated onto the exterior façade of the carwash building (see Exhibit “D”). The primary building material will be cement plaster but will also include split-faced block, brick veneer finish, tile on the tower and concrete roof tiles. The building will not have an indoor waiting lobby area but will provide an office and break room for the on-site manager and staff employees. Per the attached operational statement, the applicant indicates that there will be four employees per shift. The car wash facility will operate Monday through Sunday from 7:00 a.m. to 8:00 p.m. (see Exhibit “E”).

The project site is identified as Phase 2E of the South Packwood Creek Specific Plan (see Exhibit “F”). Phases 1A, 1B 1C (Packwood Shopping Center along Mooney) and Phase 2D (Costco and future retail) have been fully entitled per the approvals of CUP No. 2005-29 and CUP No. 2007-12. Phase 2E, which encompasses the entire 17.01-acre site north of Cameron Avenue, has not been fully entitled and shall be subject to the master planning process. The applicant, per the tentative parcel map Exhibit “B”, depicts a conceptual master site plan identifying how the remaining balance of the site could develop related to the carwash site. However, approval of this CUP does not entitle the remaining 15.5 acres as depicted per the conceptual site plan. Future development of this area will require additional discretionary actions (i.e., CUP comprehensive master plan) and consistency with the South Packwood Creek Specific Plan.

BACKGROUND INFORMATION

General Plan Land Use Designation	Regional Commercial
Zoning	P-C-R (Planned Regional Retail Commercial)
Surrounding Zoning and Land Use	North: Packwood Creek Trail and Packwood Creek South: Cameron Avenue & C-R – Future Packwood Creek Phase 2D development East: C-R – Future Phase 2E development area of the South Packwood Creek Specific Plan West: Packwood Creek Trail and Packwood Creek
Environmental Review	Initial Study No. 2016-22
Site Plan	2015-127

RELATED PLANS & POLICIES

On April 24, 2002, the City Council adopted the South Packwood Creek Specific Plan, which facilitated development of the Packwood Creek Commercial Center (CUP No. 2002-05). The Center is located on both sides of Mooney Boulevard, north of Visalia Parkway (see inset). The Specific Plan identified a total of two phases (I and II) and five planning areas (IA, IB, IC, IIA, and IIB).

On February 21, 2005, the City Council initiated the proceedings for Annexation No. 2005-01, a request to annex the subject site into the City limits. The Tulare County LAFCO (Local Agency Formation Commission) subsequently approved the annexation in May 2005.

On March 5, 2007, the City Council approved a final cancellation and approved disestablishment of the Agricultural Preserve applicable to the southern half of the project site, removing restrictions for development.

On September 12, 2005, the Planning Commission approved CUP No. 2005-29 to allow a planned commercial development, featuring a Costco building, with shared access and a gasoline service station in the Regional Retail Commercial Zone and Tentative Parcel Map No. 2005-19 to divide the subject 50.66 acres of vacant land into 16 parcels. Under the existing entitlement, CUP 2005-29, the proposed Costco building was to be located on the western portion of the site with the smaller tenants located along Stonebrook Street. The parcel configuration approved under Parcel Map No. 2005-19 corresponded with the proposed retail pads.

On May 14, 2007, the Planning Commission approved CUP No. 2007-12 amending CUP No. 2005-29 by relocating the Costco building to the southwest corner of Cameron Avenue and Stonebrook Street and approving the reconfiguration of several building pads. Tentative Parcel Map No. 2007-03 was approved which created parcels to facilitate the subdivision of this project site.

PROJECT EVALUATION

Staff recommends approval of the requested conditional use permit and tentative parcel map, as conditioned, based on projects consistency with the General Plan, Zoning and Subdivision Ordinances and the South Packwood Creek Specific Plan. The proposed use is consistent in nature and character with the planned uses in the P-C-R zone and the overall development pattern established through the adopted South Packwood Creek Specific Plan.

Land Use Compatibility

Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east.

The City's General Plan Land Use Map designates the site as Reginal Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.

Consistency with South Packwood Creek Specific Plan and General Plan

The South Packwood Creek Specific Plan provides a number of plans and policies regarding the development of this area. The policies cover issues that include streets and roadways, utility lines, storm drainage, oak tree protection, aesthetics, and parking.

The Specific Plan includes detailed plans and building layouts for Phase I located on the east and west sides of Mooney Boulevard but because of the long-term vision of Phase II (i.e., project site), detailed site plans were not included in the Specific Plan.

The South Packwood Creek Specific Plan included a conceptual site plan exhibit for the Phase 2E area (see Exhibit "G") but the conceptual site plan was lacking in detail (i.e., setbacks, building dimensions, landscaping, pedestrian walkways, uses, etc.) and was therefore not approved similar to the Phase 1 development plan. Rather, the Specific Plan as related to Phase 2E summarizes standards and guidelines for allowed building area, street and roadway improvements (not including driveways and median breaks), infrastructure, setbacks and development standards, signage, and the development of Packwood Creek. It should be noted, Phase 2D is fully entitled based on the approval of CUP No. 2007-12 (Costco development and future retail pads, see Exhibit "H").

The South Packwood Creek Specific Plan and related Environmental Impact Report were prepared to allow for a total of 1,076,946 square feet of building area between Phases 1 and 2. Based on an actual developed area of Phase I (east and west side of Mooney Blvd.), the Phase 2 area has a remaining allotment of 422,756 square feet of building area. As illustrated in Exhibit "H", the development of Phase 2D, including the Costco building, provides for a total of 280,300 square feet of building area in Phase 2D south of Cameron Avenue, which leaves 142,456 square feet of remaining developable commercial space in the Phase 2E north of Cameron Avenue.

The South Packwood Creek Specific Plan projected that the development of the Phase 2 area would be completed over a five to twenty-year period (between 2006 and 2020) dating from when the Plan was authored. The Specific Plan contains analysis of technical studies, which consider long-term impacts resulting from the buildout of Phases 1 and 2. An economic study prepared in 2001 concluded that projected demand for leasable commercial space could accommodate development of 1,016,000 square feet by the year 2010. Thus, staff finds that the development of the Surf Thru carwash at this time is consistent with the policies of the Specific Plan.



As previously stated, the remaining Phase 2E acreage has not been fully entitled and shall be subject to the master planning process. Future development of the remaining Phase 2E area will require additional discretionary actions (i.e., CUP comprehensive master plan) and consistency with the South Packwood Creek Specific Plan.

Traffic Circulation and Future Cameron Avenue Median Island Improvements

Cameron Avenue is adjacent to the south side of the site. Cameron Avenue is a designated collector status roadway per the City of Visalia's Circulation Element. Pursuant to the circulation element, collector streets are intended to provide connectivity between local streets and the arterial street system, and provide access to adjacent land uses.

The applicant prepared a traffic analysis for this project detailing the traffic generation and traffic patterns for the proposed carwash facility (see Exhibit "I"). The analysis reviewed impacts of the proposed use on the existing intersections and conducted a vehicle queuing analysis for left turn movements and vehicle stacking between the proposed carwash driveway and the Costco driveway. The analysis determined the project would not have significant impacts to the intersections studied as listed on Table 4 (see below), of the Traffic Analysis.

**Table 4
Existing + Project Intersection Levels of Service**

Intersection	Control	A.M. Peak Hour		P.M. Peak Hour	
		Delay	LOS	Delay	LOS
Mooney Blvd./Cameron Ave.	Signal	18.5 sec.	LOS B	24.7 sec.	LOS C
Cameron Ave./Project Dwy.	STOP-Sign	0.5 sec.	LOS A	0.8 sec.	LOS A
Cameron Ave./Costco Western Dwy.	STOP-Sign	1.4 sec.	LOS A	5.3 sec.	LOS A
Cameron Ave./Costco Main Dwy.	STOP-Sign	1.8 sec.	LOS A	3.6 sec.	LOS A
Cameron Ave./Stonebrook St.	STOP-Sign	3.7 sec.	LOS A	6.1 sec.	LOS A

Additionally, the left turn lane in Cameron Avenue provides ample storage length for vehicles queuing in the left turn lane waiting to make the left turn onto the carwash site. The City's Traffic Engineer concurs with the findings provided in the traffic analysis.

There are approximately 12,419 AADT (Average Annual Daily Trips) along this segment of Cameron Avenue as identified in the City's Circulation Element of the General Plan. Collector streets are designed to accommodate 15,000 AADT's. It should be noted the Cameron / Stonebrook intersection is planned to be signalized at a future date per the City's Capital Improvement Project program. The signalization of this intersection could be expedited if the remaining balance of Phase 2E is developed, any of the abutting commercial or multi-family zoned parcels are developed or if Stonebrook Street is constructed between Cameron and Caldwell Avenues.

Future Cameron Avenue Median Island Construction

The median island located on Cameron Avenue was constructed to prohibit left-turn (westbound) traffic movements from the Lowes site and future driveway south of the median island (see Figure 1).



During the Site Plan Review process for the carwash facility, staff identified potential turning conflicts from eastbound traffic entering the site from the proposed carwash drive approach and the future development of Phase 2E. Based on the traffic counts along Cameron Avenue and the traffic analysis prepared for this project, staff has included a condition that requires the median island to be extended east with any future development of the Phase 2E area. The future extension of the Cameron Avenue median island will restrict turning movements to “Right In and Right Out”. Construction of this median island will be at the cost of the developer and is non-reimbursable. Condition No. 5, identifying the median island requirement is included as a condition of project approval and is noted in staff’s Site Plan Review No. 2015-127 comments, which are attached to this report. However, the City’s Traffic Engineer reserves the right at any time to restrict left-turn movements if the City determines for safety reasons these vehicular movement becomes unsafe to make.

Tentative Parcel Map

The requested parcel map shown in Exhibit “B” would divide the 17.01-acre site into four parcels. Parcel 1, which is 1.51 acres, will be sold and developed with the carwash facility while the remaining three parcels will not be developed. As previously stated, Exhibit “B” provides a conceptual depiction of how the remaining Phase 2E area could develop but this CUP entitlement does not entitle the remaining parcels as depicted per Exhibit “B”. However, staff has determined that the illustrated cross access points between the carwash site and future Phase 2E area provide for future internal circulation throughout the site. To ensure future cross access requirements are adhered to, the parcel map is conditioned requiring vehicular cross-access agreements be recorded between Parcels 1 through 4 for Parcel Map No. 2016-06.

Building Elevations

Exhibit "D" provide typical building elevations proposed for the car wash facility. The elevation depicts a pitched roof, tile roofing, brick veneer, tile siding and plastered walls and columns. Staff has included Condition No. 3 of the Conditions of Approval for CUP No. 2016-09, requiring that the building elevations be developed as those provided in Exhibit "D".

Site Lighting

All site lighting is required to comply with the City's light standards. This includes lighting to be designed and installed to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property; parking lot and drive aisle lighting adjacent to residential units shall use 15-foot high light poles, with the light element to be completely recessed into the can. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated. The lighting requirements are included in the Site Plan Review comments for Planning.

Landscaping

Street tree landscaping within the park strips along Cameron Avenue (Parcel 1) is required with this project (see Exhibit "B"). The street tree requirement within the park strip for the Surf Thru Carwash is included as Condition No. 7. In addition, Surf Thru Carwash will develop their onsite-landscaping planters subject to complying the City's Water Efficient Landscape Ordinance. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State and City's water conservation requirements.

Correspondence from California Water Service Company

Staff received correspondence (Exhibit "J", attached herein) from the California Water Service Company (Cal Water). Cal Water, in its correspondence dated May 26, 2016, has indicated that Cal Water agrees to operate the water system and provide service in accordance with the rules and regulations of the California Public Utilities Commission. The determination of water availability shall remain voided for two years from the date of their letter. The letter also states that if the project does not commence within the two-year time frame, Cal Water will be under no obligation to serve the project site unless the developer receives an updated letter from Cal Water reconfirming water availability. In addition, the letter can be rescinded at any time in the event that water supply is severely reduced by legislative, regulatory or environmental factors.

South Packwood Creek Sign Program

The South Packwood Creek Specific Plan contains general standards for signage allowance within the planning areas. The signage allowance for the Surf Thru carwash falls under Section 3.2.4 Signs D -- "Building Signs (PAD, ROW and In-Line Stores)" of the South Packwood Creek Specific Plan. This section permits building signage per Design District "A" Standards, which allows a maximum of 150 sq. ft. of building signage. Additionally, the Specific Plan permits only one large monument sign for each plan area of the Packwood Creek Shopping Center. The large monument sign is to be shared by all businesses within the "plan area". The proposed monument sign, as depicted on Exhibit "A", is not permitted. If the applicant desires to deviate from the sign standards permitted through the Specific Plan, a Specific Plan Amendment must be filed and must be approved by the City Council.

Environmental Review

California Environmental Quality Act Section 15183(a) mandates that projects which are consistent with the development density established by general plan policies for which an Environmental Impact Report (EIR) was certified shall not require additional environmental review, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site.

An Initial Study (Initial Study No. 2016-22) was prepared for this project, which disclosed the proposed project has no new effects that could occur, or new mitigation measures that would be required that have not been addressed within the scope of the City of Visalia General Plan Program Environmental Impact Report (SCH No. 2010041078). The Program Environmental Impact Report was certified by Resolution No. 2014-37 on October 14, 2014. Therefore, staff concludes that the Program Environmental Impact Report adequately analyzed and addressed the conditional use permit and tentative parcel map applications.

RECOMMENDED FINDINGS

Conditional Use Permit No. 2016-09

1. That the proposed conditional use permit is consistent with the policies and intent of the General Plan, Zoning and Noise Ordinances.
2. That the proposed conditional use permit would be compatible with adjacent land uses.
3. That the proposed location of the Conditional Use Permit and the conditions under which it would be built or maintained will not be detrimental to the public health, safety, or welfare nor materially injurious to properties or improvements in the vicinity.
4. That an Initial Study was prepared for the requested Conditional Use Permit consistent with CEQA, Initial Study No. 2016-22 disclosed the proposed project has no new effects that could occur, or new mitigation measures that would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37, adopted on October 14, 2014. Therefore, the Program Environmental Impact Report adequately analyzed and addressed the Conditional Use Permit application.

Tentative Parcel Map No. 2016-06

1. That the proposed tentative parcel map is consistent with the policies and intent of the General Plan, Zoning, and Subdivision Ordinances.
2. That the proposed tentative parcel map would be consistent with similar divisions of property for commercial uses in the P-C-R zone.
3. That the proposed tentative parcel map will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
4. That an Initial Study was prepared for the requested Conditional Use Permit consistent with CEQA, Initial Study No. 2016-22 disclosed the proposed project has no new effects that could occur, or new mitigation measures that would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37, adopted on October 14, 2014. Therefore, the Program Environmental Impact Report adequately analyzed and addressed the Conditional Use Permit application.

RECOMMENDED CONDITIONS OF APPROVAL

Conditional Use Permit No. 2016-09

1. That the Conditional Use Permit shall be developed consistent with the comments and conditions of Site Plan Review No. 2015-127, incorporated herein by reference.
2. That the site be developed in substantial compliance with the site plan shown in Exhibit "A".
3. That the Surf Thru Carwash be developed in substantial compliance with the elevations shown in Exhibit "D".
4. That the site substantially complies with the Operational Statement as stated in Exhibit "E". Hours of operation for the carwash shall be limited to the hours from 7:00 am to 8:00 pm Monday through Sunday.
5. The existing median located on Cameron Avenue shall be extended east limiting access to the drive approach for the Surf Thru Carwash site thereby restricting turning movements to "Right In & Right Out" from the carwash site onto Cameron Avenue and eliminating left turn movements from Cameron Avenue onto the site. The construction of the median island will be enforced at the time of development for the remaining portion of the Phase 2E area as depicted on Exhibit "F".
6. The remaining 15.5 acres (Parcels 2, 3 & 4) are not entitled with CUP No. 2016-09. The development of the 15.5 acres (Parcels 2, 3, & 4) is subject to a master plan consistent with the South Packwood Creek Specific Plan and discretionary review.
7. That the park strip landscaping, within the boundaries of the Surf Thru Carwash be installed with this development.
8. That landscape and irrigation plans, prepared in accordance with the City of Visalia Model Water Efficient Landscape Ordinance, shall be included in the construction document plans submitted for either grading or building construction permits.
9. All new building signage shall require a separate building permit and shall be designed consistent with the adopted South Packwood Creek Specific Plan requirements.
10. That all other federal and state laws and city codes and ordinances be complied with.
11. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2016-09, prior to the issuance of any building permit for this project.

Tentative Parcel Map No. 2016-06

1. That the tentative parcel map be prepared in substantial compliance with Exhibit "B".
2. That the tentative parcel map shall be developed consistent with the comments and conditions of Site Plan Review No. 2015-127.
3. That the vehicular cross access points as depicted on Exhibit "A" be shown on the final map, and have the corresponding vehicular cross-access agreement approved by the City.
4. That the parcel map site is subject to the development standards of the South Packwood Creek Specific Plan.
5. That Tentative Parcel Map No. 2016-02 shall be null and void unless Conditional Use Permit No. 2016-09 is approved.

6. That an agreement addressing vehicular cross access, ingress and easement, and any other pertinent infrastructure or services shall be recorded with the final parcel map. The agreement shall address property owners' responsibility for repair and maintenance of the easement, repair and maintenance of shared public or private utilities, and shall be kept free and clear of any structures excepting solid waste enclosures.
7. The remaining 15.5 acres (Parcels 2, 3 & 4) are not entitled with CUP No. 2016-09. The development of the 15.5 acres (Parcels 2, 3, & 4) is subject to a master plan consistent with the South Packwood Creek Specific Plan and discretionary review.
8. That all other federal and state laws and city codes and ordinances be complied with.
9. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Parcel Map No. 2016-06, prior to the recordation of the parcel map.

APPEAL INFORMATION

According to the City of Visalia Zoning Ordinance Section 17.02.145 and Subdivision Ordinance Section 16.28.080, an appeal to the City Council may be submitted within ten days following the date of a decision by the Planning Commission. An appeal with applicable fees shall be in writing and shall be filed with the City Clerk at 220 N. Santa Fe Street, Visalia, CA 93292. The appeal shall specify errors or abuses of discretion by the Planning Commission, or decisions not supported by the evidence in the record. The appeal form can be found on the city's website www.ci.visalia.ca.us or from the City Clerk.

Attachments:

- Related Plans and Policies
- Resolution No. 2016-13 (Conditional Use Permit No. 2016-09)
- Resolution No. 2016-29 (Parcel Map No. 2016-06)
- Exhibit "A" – Site Plan
- Exhibit "B" – Tentative Parcel Map No. 2016-06
- Exhibit "C" – Floor Plan
- Exhibit "D" – Elevations
- Exhibits "F" – South Packwood Creek Specific Phasing Plan
- Exhibit "E" – Applicants Operational Statement
- Exhibit "G" – Conceptual Phase 2E Development Plan per the South Packwood Creek Specific Plan
- Exhibit "H" – Site Plan for CUP No. 2007-12 (Costco)
- Exhibit "I" – Traffic Analysis dated April 8, 2016
- Exhibit "J" – California Water Service Will Serve Letter dated May 26, 2016
- Exhibit "K" – Landscape Plan
- Initial Study No. 2016-22
- Site Plan Review Comments

- General Plan Land Use Map
- Zoning Map
- Aerial Map
- Vicinity Map

Conditional Use Permits (Section 17.38)

17.38.010 Purposes and powers.

In certain zones conditional uses are permitted subject to the granting of a conditional use permit. Because of their unusual characteristics, conditional uses require special consideration so that they may be located properly with respect to the objectives of the zoning ordinance and with respect to their effects on surrounding properties. In order to achieve these purposes and thus give the zone use regulations the flexibility necessary to achieve the objectives of this title, the planning commission is empowered to grant or deny applications for conditional use permits and to impose reasonable conditions upon the granting of such permits. (Prior code § 7525)

17.38.020 Application procedures.

- A. Application for a conditional use permit shall be made to the planning commission on a form prescribed by the commission which shall include the following data:
1. Name and address of the applicant;
 2. Statement that the applicant is the owner of the property or is the authorized agent of the owner;
 3. Address and legal description of the property;
 4. The application shall be accompanied by such sketches or drawings as may be necessary by the planning division to clearly show the applicant's proposal;
 5. The purposes of the conditional use permit and the general description of the use proposed;
 6. Additional information as required by the historic preservation advisory committee.
- B. The application shall be accompanied by a fee set by resolution of the city council sufficient to cover the cost of handling the application. (Prior code § 7526)

17.38.030 Lapse of conditional use permit.

A conditional use permit shall lapse and shall become void twenty-four (24) months after the date on which it became effective, unless the conditions of the permit allowed a shorter or greater time limit, or unless prior to the expiration of twenty-four (24) months a building permit is issued by the city and construction is commenced and diligently pursued toward completion on the site which was the subject of the permit. A permit may be renewed for an additional period of one year; provided, that prior to the expiration of twenty-four (24) months from the date the permit originally became effective, an application for renewal is filed with the planning commission. The commission may grant or deny an application for renewal of a conditional use permit. In the case of a planned residential development, the recording of a final map and improvements thereto shall be deemed the same as a building permit in relation to this section. (Ord. 2001-13 § 4 (part), 2001: prior code § 7527)

17.38.040 Revocation.

Upon violation of any applicable provision of this title, or, if granted subject to a condition or conditions, upon failure to comply with the condition or conditions, a conditional use permit shall be suspended automatically. The planning commission shall hold a public hearing within sixty (60) days, in accordance with the procedure prescribed in Section 17.38.080, and if not satisfied that the regulation, general provision or condition is being complied with, may revoke the permit or take such action as may be necessary to insure compliance with the regulation, general provision or condition. Appeals of the decision of the planning commission may be made to the city council as provided in Section 17.38.120. (Prior code § 7528)

17.38.050 New application.

Following the denial of a conditional use permit application or the revocation of a conditional use permit, no application for a conditional use permit for the same or substantially the same conditional use on the same or substantially the same site shall be filed within one year from the date of denial or revocation of the permit unless such denial was a denial without prejudice by the planning commission or city council. (Prior code § 7530)

17.38.060 Conditional use permit to run with the land.

A conditional use permit granted pursuant to the provisions of this chapter shall run with the land and shall continue to be valid upon a change of ownership of the site or structure which was the subject of the permit application subject to the provisions of Section 17.38.065. (Prior code § 7531)

17.38.065 Abandonment of conditional use permit.

If the use for which a conditional use permit was approved is discontinued for a period of one hundred eighty (180) days, the use shall be considered abandoned and any future use of the site as a conditional use will require the approval of a new conditional use permit.

17.38.070 Temporary uses or structures.

- A. Conditional use permits for temporary uses or structures may be processed as administrative matters by the city planner and/or planning division staff. However, the city planner may, at his/her discretion, refer such application to the planning commission for consideration.
- B. The city planner and/or planning division staff is authorized to review applications and to issue such temporary permits, subject to the following conditions:
 1. Conditional use permits granted pursuant to this section shall be for a fixed period not to exceed thirty (30) days for each temporary use not occupying a structure, including promotional enterprises, or six months for all other uses or structures.
 2. Ingress and egress shall be limited to that designated by the planning division. Appropriate directional signing, barricades, fences or landscaping shall be provided where required. A security officer may be required for promotional events.
 3. Off-street parking facilities shall be provided on the site of each temporary use as prescribed in Section 17.34.020.
 4. Upon termination of the temporary permit, or abandonment of the site, the applicant shall remove all materials and equipment and restore the premises to their original condition.
 5. Opening and closing times for promotional enterprises shall coincide with the hours of operation of the sponsoring commercial establishment. Reasonable time limits for other uses may be set by the city planner and planning division staff.
 6. Applicants for a temporary conditional use permit shall have all applicable licenses and permits prior to issuance of a conditional use permit.
 7. Signing for temporary uses shall be subject to the approval of the city planner.
 8. Notwithstanding underlying zoning, temporary conditional use permits may be granted for fruit and vegetable stands on properties primarily within undeveloped agricultural areas. In reviewing applications for such stands, issues of traffic safety and land use compatibility shall be evaluated and mitigation measures and conditions may be imposed to ensure that the stands are built and are operated consistent with appropriate construction standards, vehicular access and off-street parking. All fruits and vegetables sold at such stands shall be grown by the owner/operator or purchased by said party directly from a grower/farmer.
- C. The applicant may appeal an administrative decision to the planning commission. (Ord. 9605 § 30 (part), 1996: prior code § 7532)

17.38.080 Public hearing--Notice.

- A. The planning commission shall hold at least one public hearing on each application for a conditional use permit.
- B. Notice of the public hearing shall be given not less than ten days nor more than thirty (30) days prior to the date of the hearing by mailing a notice of the time and place of the hearing to property owners within three hundred (300) feet of the boundaries of the area occupied or to be occupied by the use which is the subject of the hearing, and by publication in a newspaper of general circulation within the city. (Prior code § 7533)

17.38.090 Investigation and report.

The planning staff shall make an investigation of the application and shall prepare a report thereon which shall be submitted to the planning commission. (Prior code § 7534)

17.38.100 Public hearing--Procedure.

At the public hearing the planning commission shall review the application and the statement and drawing submitted therewith and shall receive pertinent evidence concerning the proposed use and the proposed conditions under which it would be operated or maintained, particularly with respect to the findings prescribed in Section 17.38.110. The planning commission may continue a public hearing from time to time as it deems necessary. (Prior code § 7535)

17.38.110 Action by planning commission.

- A. The planning commission may grant an application for a conditional use permit as requested or in modified form, if, on the basis of the application and the evidence submitted, the commission makes the following findings:
 - 1. That the proposed location of the conditional use is in accordance with the objectives of the zoning ordinance and the purposes of the zone in which the site is located;
 - 2. That the proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the vicinity.
- B. A conditional use permit may be revocable, may be granted for a limited time period, or may be granted subject to such conditions as the commission may prescribe. The commission may grant conditional approval for a permit subject to the effective date of a change of zone or other ordinance amendment.
- C. The commission may deny an application for a conditional use permit. (Prior code § 7536)

17.38.120 Appeal to city council.

The decision of the City planning commission on a conditional use permit shall be subject to the appeal provisions of Section 17.02.145. (Prior code § 7537) (Ord. 2006-18 § 6, 2007)

17.38.130 Effective date of conditional use permit.

A conditional use permit shall become effective immediately when granted or affirmed by the council, or upon the sixth working day following the granting of the conditional use permit by the planning commission if no appeal has been filed. (Prior code § 7539)

Section 16.28 Parcel Maps

16.28.070 Consideration of tentative parcel maps.

The commission shall review the tentative parcel map and approve, conditionally approve, or disapprove the map within thirty (30) days after the receipt of such map, or at such later date as may be required to concurrently process the appurtenant environmental impact require documents required by state law and local regulations adopted in implementation thereof. (Prior code § 9240)

16.28.080 Appeals.

If the applicant is dissatisfied with the decision of the planning commission, he may, within ten days after the decision of the planning commission, appeal in writing to the council for a hearing thereon. Such hearing need not be concluded on the day thus set but may be continued. (Prior code § 9245)

16.28.090 Time limit on tentative parcel map.

Failure to file a final parcel map with the county recorder within twenty four (24) months after the date of approval or conditional approval of the tentative parcel map shall automatically revoke said approval, and a final parcel map shall not be recorded until a new tentative parcel map has been filed and approved in accordance with the provisions of this chapter. However, upon application by the owner or his authorized agent, an extension of not more than an additional thirty-six (36) months may be granted by the planning commission. If the planning commission denies an application for an extension of time, the owner or his authorized agent may appeal the action to the city council in the manner set forth in Section 16.28.080. (Prior code § 9250)

16.04.040 Appeals.

A. Planning Commission Actions. The subdivider or any interested person adversely affected may appeal any decision, determination or requirement of the planning commission by filing a notice thereof in writing with the city clerk, setting forth in detail the action and the grounds upon which the appeal is based within ten days after the action which is the subject of the appeal. An appeal shall state specifically where it is claimed there was an error or abuse of discretion by the planning commission.

B. Hearing of Appeals. Upon the filing of such an appeal, the city council shall set the matter for hearing. Such hearings shall be held within thirty (30) days after the date of filing the appeal. Within ten days following the conclusion of the hearing, the city council shall render written decision on the appeal. (Prior code § 9015)

RELATED PLANS AND POLICIES

General Plan Land Use Element

Regional Retail: Visalia has traditionally been the regional retail hub for Tulare and neighboring Kings counties, with Mooney Boulevard serving as the core for these uses. The General Plan both responds to a general community desire to maintain and strengthen Downtown (with its unique shopping character) and Mooney Boulevard. This approach also allows the city to respond to the market, capitalizing on opportunities in a proactive way. Significant portions of Mooney Boulevard between the Sequoia Mall and Highway 198 are designated as Commercial Mixed Use, which allows community-serving retail in conjunction with other uses such as residential and office (the Regional Commercial designation is maintained for Visalia Mall and adjacent retail at Mooney and Walnut). Further south, where lot sizes are larger and current regional tenants are located, preexisting regional commercial designations are preserved and expanded. This area is the city's near-term priority location for retail development.

Objectives:

LU-O-26: Facilitate development of new, well-planned commercial areas to meet the needs of residents of Visalia and its market area.

Policies:

LU-P-61: Support the continued development and revitalization of the following corridors as integral parts of the community, with offices, commercial uses, multi-family residential, and mixed use developments.

- Mooney Boulevard between Noble and Caldwell;
- Dinuba Boulevard between Houston and Ferguson;
- Ben Maddox Way between Tulare and Houston;
- Santa Fe Avenue between Tulare and Houston; and
- Houston Avenue between Hall and Cain.

The depth of commercial development along the Mooney Boulevard corridor, Ben Maddox Way, and North Dinuba Boulevard may be extended on a case by case basis if the extension increases the feasibility of commercial development and the proposed action will not create land use conflicts or reduce viability of adjacent residential properties for each zone classification.

LU-P-62: Plan for Regional Commercial areas at a limited number of highly visible freeway accessible locations as shown on the Land Use Diagram.

LU-P-63: In higher-intensity and mixed use areas, require pedestrian-oriented amenities such as small plazas, outdoor seating, public art, and active street frontages, with ground floor retail, where appropriate and justified.

New development can help create pedestrian environments with buildings oriented to the street, continuous walkways and sidewalks, limited blank walls, pedestrian-scaled buildings, and signage, parking screened from street view, landscaping and shading, and places for people to rest and meet.

LU-P-64: Provide incentives for new pedestrian friendly retail and mixed use development along major transit corridors and pedestrian-oriented commercial streets.

Incentives may include increased floor area ratios, reduced or deferred impact fees, reduced parking, and priority processing. This incentive program will be integrated with, and not duplicate, the Infill Development Incentive Program.

LU-P-65: Continue to require a master-planning process for community and regional compatibility with surrounding residential areas, an attractive appearance from major roadways, and pedestrian accessibility and safety.

Planning strategies include the use of buffering land uses, such as office uses between residential and high intensity commercial uses, and sidewalk-facing retail and high-quality public realm elements that encourage pedestrian access.

LU-P-69: Update the Zoning Ordinance to reflect the Regional Commercial designation on the Land Use Diagram, for retail commercial uses that serve residents and businesses in the region at large, such as shopping malls and large-format or “big box” stores, as well as supporting businesses such as gas stations and hotels.

RESOLUTION NO. 2016-18

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING CONDITIONAL USE PERMIT NO. 2016-09, A REQUEST BY VERMELTFOORT ARCHITECTS INC. TO CONSTRUCT A NEW 4,767 SQUARE FOOT DRIVE-THRU CARWASH WITH AN ATTACHED PAY STATION, VACUUM CANOPY AREA AND A DETACHED 245 SQUARE FOOT VACUUM EQUIPMENT BUILDING ON 1.51-ACRES OF AN OVERALL 17.01-ACRE SITE. THE OVERALL 17.01-ACRE SITE IS DEFINED AS PHASE 2E OF THE PACKWOOD SHOPPING CENTER SPECIFIC PLAN AREA. THE PROJECT SITE IS LOCATED ON THE NORTH SIDE OF WEST CAMERON AVENUE BETWEEN SOUTH MOONEY BOULEVARD AND SOUTH STONEBROOK STREET (APN: 126-062-095 & 126-730-039).

WHEREAS, Conditional Use Permit No. 2016-09, is a request by Vermeltfoort Architects Inc. to construct a new 4,767 square foot drive-thru carwash with an attached pay station, vacuum canopy area and a detached 245 square foot vacuum equipment building on 1.51-acres of an overall 17.01-acre site. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area. The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039).; and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on June 13, 2016; and

WHEREAS, the Planning Commission of the City of Visalia finds the Conditional Use Permit No. 2016-09, as conditioned by staff, to be in accordance with Chapter 17.38.110 of the Zoning Ordinance of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, an Initial Study was prepared which disclosed that no significant environmental impacts would result from this project, and no mitigation measures would be required.

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission finds that as a result of the proposed project no new effects could occur, or new mitigation measures would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37, adopted on October 14, 2014. The Program Environmental Impact Report adequately analyzed and addressed this conditional use permit application.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia makes the following specific findings based on the evidence presented:

1. That the proposed project will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
2. That the proposed conditional use permit is consistent with the policies and intent of the General Plan, Zoning Ordinance and South Packwood Creek Specific Plan. Specifically, the project is consistent with the required findings of Zoning Ordinance Section 17.38.110:
 - The proposed location of the conditional use permit is in accordance with the objectives of the Zoning Ordinance and the purposes of the zone in which the site is located. The South Packwood Creek Specific Plan projected that the development of the Phase 2 area would be completed over a five to twenty-year period (between 2006 and 2020) dating from when the Plan was authored. The Specific Plan contains analysis of technical studies, which consider long-term impacts resulting from the buildout of Phases 1 and 2. An economic study prepared in 2001 concluded that projected demand for leasable commercial space could accommodate development of 1,016,000 square feet by the year 2010. Thus, Staff finds that the development of the carwash at this time is consistent with the policies of the Specific Plan.

Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east.

The City's General Plan Land Use Map designates the site as Regional Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.

- The proposed location of the conditional use and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety, or welfare, nor materially injurious to properties or improvements in the vicinity. Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east.

The City's General Plan Land Use Map designates the site as Regional Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.

3. That an Initial Study was prepared for the requested Conditional Use Permit consistent with CEQA, Initial Study No. 2016-22 disclosed the proposed project has no new effects that could occur, or new mitigation measures that would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37 adopted on October 14, 2014. No further environmental review is required.

BE IT FURTHER RESOLVED that the Planning Commission hereby approves the Conditional Use Permit on the real property here described in accordance with the terms of this resolution under the provisions of Section 17.38.110 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the Conditional Use Permit shall be developed consistent with the comments and conditions of Site Plan Review No. 2015-127, incorporated herein by reference.
2. That the site be developed in substantial compliance with the site plan shown in Exhibit "A".
3. That the Surf Thru Carwash be developed in substantial compliance with the elevations shown in Exhibit "D".
4. That the site substantially complies with the Operational Statement as stated in Exhibit "E". Hours of operation for the carwash shall be limited to the hours from 7:00 am to 8:00 pm Monday through Sunday.
5. The existing median located on Cameron Avenue shall be extended east limiting access to the drive approach for the Surf Thru Carwash site thereby restricting turning movements to "Right In & Right Out" from the carwash site onto Cameron Avenue and eliminating left turn movements from Cameron Avenue onto the site. The construction of the median island will be enforced at the time of development for the remaining portion of the Phase 2E area as depicted on Exhibit "F".
6. The remaining 15.5 acres (Parcels 2, 3 & 4) are not entitled with CUP No. 2016-09. The development of the 15.5 acres (Parcels 2, 3, & 4) is subject to a master plan consistent with the South Packwood Creek Specific Plan and discretionary review.
7. That the park strip landscaping, within the boundaries of the Surf Thru Carwash be installed with this development.
8. That landscape and irrigation plans, prepared in accordance with the City of Visalia Model Water Efficient Landscape Ordinance, shall be included in the construction document plans submitted for either grading or building construction permits.
9. All new building signage shall require a separate building permit and shall be designed consistent with the adopted South Packwood Creek Specific Plan requirements.
10. That all other federal and state laws and city codes and ordinances be complied with.
11. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Conditional Use Permit No. 2016-09, prior to the issuance of any building permit for this project.

RESOLUTION NO 2016-29

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF VISALIA APPROVING TENTATIVE PARCEL MAP NO. 2016-06, A REQUEST BY LANE ENGINEERS INC., TO SUBDIVIDE 17.01 ACRES INTO FOUR PARCELS. THE OVERALL 17.01-ACRE SITE IS DEFINED AS PHASE 2E OF THE PACKWOOD SHOPPING CENTER SPECIFIC PLAN AREA. THE PROJECT SITE IS LOCATED ON THE NORTH SIDE OF WEST CAMERON AVENUE BETWEEN SOUTH MOONEY BOULEVARD AND SOUTH STONEBROOK STREET
(APN: 126-062-095 & 126-730-039)

WHEREAS, Tentative Parcel Map No. 2016-06 is a request by Lane Engineers Inc., to subdivide 17.01 acres into four parcels. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area. The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039); and

WHEREAS, the Planning Commission of the City of Visalia, after duly published notice did hold a public hearing before said Commission on June 13, 2016; and

WHEREAS, the Planning Commission of the City of Visalia finds the tentative parcel map in accordance with Section 16.28.070 of the Ordinance Code of the City of Visalia based on the evidence contained in the staff report and testimony presented at the public hearing; and

WHEREAS, an Initial Study was prepared which disclosed that no significant environmental impacts would result from this project, and no mitigation measures would be required.

NOW, THEREFORE, BE IT RESOLVED that the Planning Commission finds that as a result of the proposed project no new effects could occur, or new mitigation measures would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37, adopted on October 14, 2014. The Program Environmental Impact Report adequately analyzed and addressed this tentative parcel map application.

NOW, THEREFORE, BE IT FURTHER RESOLVED that the Planning Commission of the City of Visalia approves the proposed tentative parcel map based on the following specific findings and based on the evidence presented:

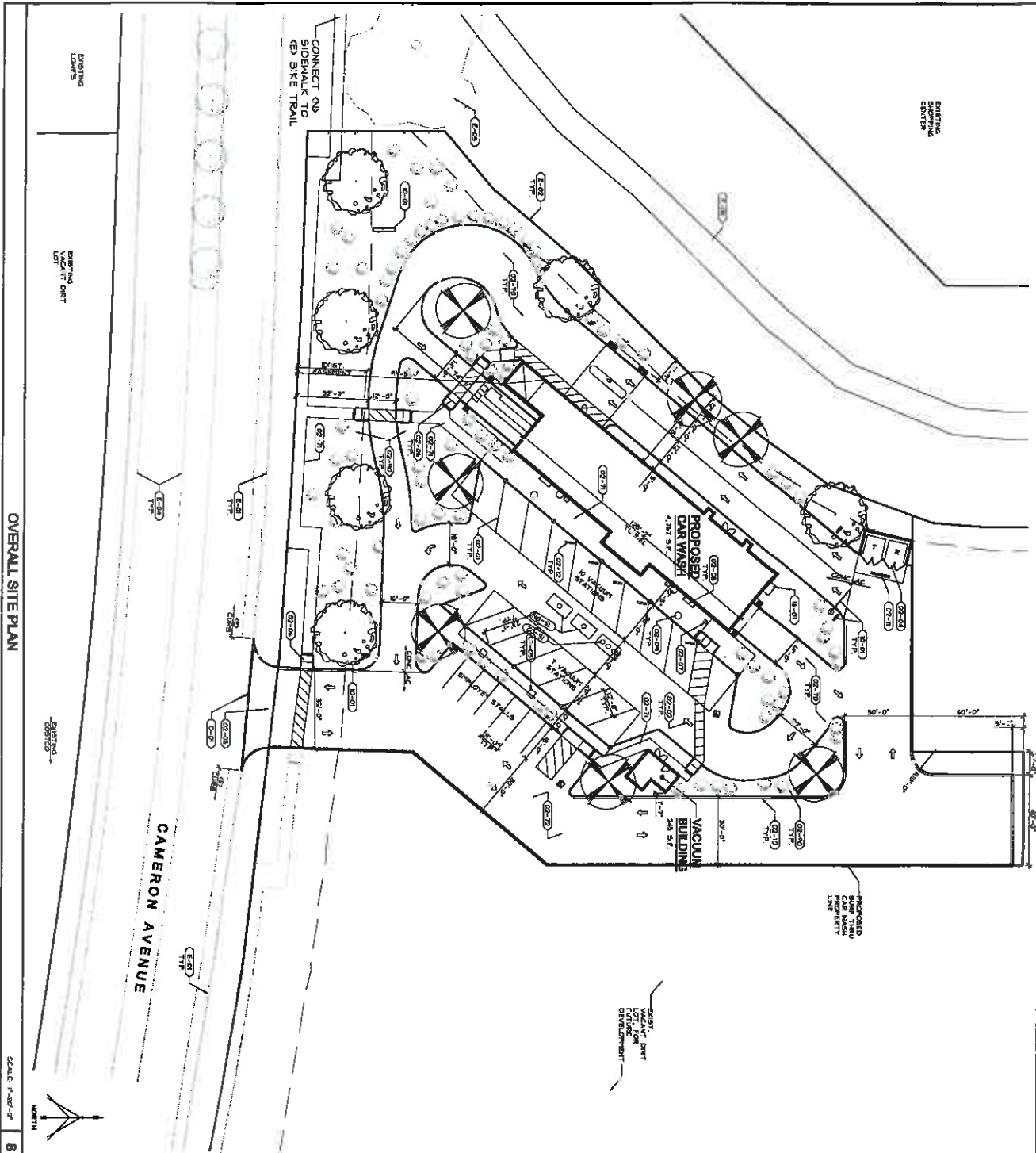
1. That the proposed tentative parcel map is consistent with the policies and intent of the General Plan, Zoning, and Subdivision Ordinances.
2. That the proposed tentative parcel map would be consistent with similar divisions of property for commercial uses in the P-C-R zone.
3. That the proposed tentative parcel map will not be detrimental to the public health, safety, or welfare, or materially injurious to properties or improvements in the vicinity.
4. That an Initial Study was prepared for the requested Conditional Use Permit consistent with CEQA, Initial Study No. 2016-22 disclosed the proposed project has no new effects that could occur, or new mitigation measures that would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37, adopted on October 14, 2014. Therefore, the Program Environmental Impact Report adequately analyzed and addressed the Tentative Parcel Map application.

BE IT FURTHER RESOLVED that the Planning Commission hereby approves the tentative parcel map on the real property hereinabove described in accordance with the terms of this resolution under the provisions of Section 16.28.070 of the Ordinance Code of the City of Visalia, subject to the following conditions:

1. That the tentative parcel map be prepared in substantial compliance with Exhibit "B".
2. That the tentative parcel map shall be developed consistent with the comments and conditions of Site Plan Review No. 2015-127.
3. That the vehicular cross access points as depicted on Exhibit "A" be shown on the final map, and have the corresponding vehicular cross-access agreement approved by the City.
4. That the parcel map site is subject to the development standards of the South Packwood Creek Specific Plan.
5. That Tentative Parcel Map No. 2016-02 shall be null and void unless Conditional Use Permit No. 2016-09 is approved.
6. That an agreement addressing vehicular cross access, ingress and easement, and any other pertinent infrastructure or services shall be recorded with the final parcel map. The agreement shall address property owners' responsibility for repair and maintenance of the easement, repair and maintenance of shared public or private utilities, and shall be kept free and clear of any structures excepting solid waste enclosures.

7. The remaining 15.5 acres (Parcels 2, 3 & 4) are not entitled with CUP No. 2016-09. The development of the 15.5 acres (Parcels 2, 3, & 4) is subject to a master plan consistent with the South Packwood Creek Specific Plan and discretionary review.
8. That all other federal and state laws and city codes and ordinances be complied with.
9. That the applicant submit to the City of Visalia a signed receipt and acceptance of conditions from the applicant and property owner, stating that they understand and agree to all the conditions of Parcel Map No. 2016-06, prior to the recordation of the parcel map.

Exhibit "A"



<p>KEYNOTES</p> <ul style="list-style-type: none"> (E-1) EAST CURB & GUTTER TO REMAIN, PAVEMENT REPAIR. (E-2) EAST PROPERTY LINE TO REMAIN. (E-3) EAST CURB TO REMAIN. (E-4) EAST STREET STRIPING TO REPAIR. (E-5) EAST SIDE YIELD TO REMAIN. (E-6) REMOVE E STREET CURB & GUTTER. (E-7) GRADING LOT STRIPING PER CITY OF VISALIA STANDARDS. (E-8) NO DIRECTIONAL ARROW STRIPING PER CITY OF VISALIA STANDARDS. (E-9) 30'-0" STREET APPROACH, PER CITY STANDARD. (E-10) NO TRASH ENCLOSURE, PER CITY STANDARD R-3, R-4, R-5, SEE LAND. (E-11) NEW 3'-0"x6'-0" CONC. PAD FOR TRASH CONTAINER. (E-12) NO CONC. RAMP W/ TRUCKWHEEL DEPRES. (E-13) NO 3' LOOP BUILT BACK. (E-14) NO SIGNAGE - NEGATIVE ASIDE DESIGN, REMOVE SIGNAGE, NAVIGATOR SIGN. (E-15) STORMWATER TRENCH/TURF GRASS PLANTER. (E-16) PAINT CURB RED w/ WHITE 'NO PARKING FIRE LANE' LETTERS @ 24" ON CENTER (SHOWN DASHED). (E-17) PAINT IN WHITE @ 24" HIGH LETTERS 'NO PARKING'. (E-18) NEW CONC. PAVING. (E-19) NEW CONC. SIDEWALK. (E-20) NO CONC. PAVING. (E-21) NO CONC. SIDEWALK. (E-22) PROVIDE 4C CURB FOR FUTURE DEVELOPMENT. (E-23) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-24) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-25) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-26) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-27) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-28) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-29) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-30) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-31) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-32) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-33) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-34) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-35) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-36) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-37) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-38) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-39) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-40) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-41) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-42) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-43) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-44) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-45) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-46) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-47) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-48) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-49) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. (E-50) 60" CURB W/ 8" SIDEWALK, UNDER SEPARATE REVIEW & ON LANDSCAPING. 	<p>3 SITE INFORMATION</p> <p>ADDRESS: CAMERON AVE & STONEBROOK ST VISALIA, CALIFORNIA</p> <p>SITE INFORMATION: SITE AREA: 27,247 SQ. FT. VEHICLE SPACES: 40 (SEE PLAN)</p> <p>PERMITS: STANDARD ENGINE PROVISIONS & PERMITS, STALLS, CURB, SIDEWALK, SIGNAGE, TRASH, 44 VEH. ACCESSIBLE HANDICAP PARKING STALL PROVIDED.</p>	<p>4 VICINITY MAP</p>
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<p>1 SITE PLAN REVIEW</p> <p>2 CONDITIONAL USE PERMIT</p> <p>DATE REVISION</p>	<p>Vermeltoft Architects, Inc. Architecture and Planning 8525 North Cedar Avenue Fresno, California 93720 Office: 559.432.6714 Fax: 559.432.6745</p>	<p>SURF THRU CAR WASH N.W.C. OF CAMERON AVENUE & STONEBROOK STREET VISALIA, CALIFORNIA</p> <p>OVERALL SITE PLAN</p> <p>A100</p>
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Exhibit "B"

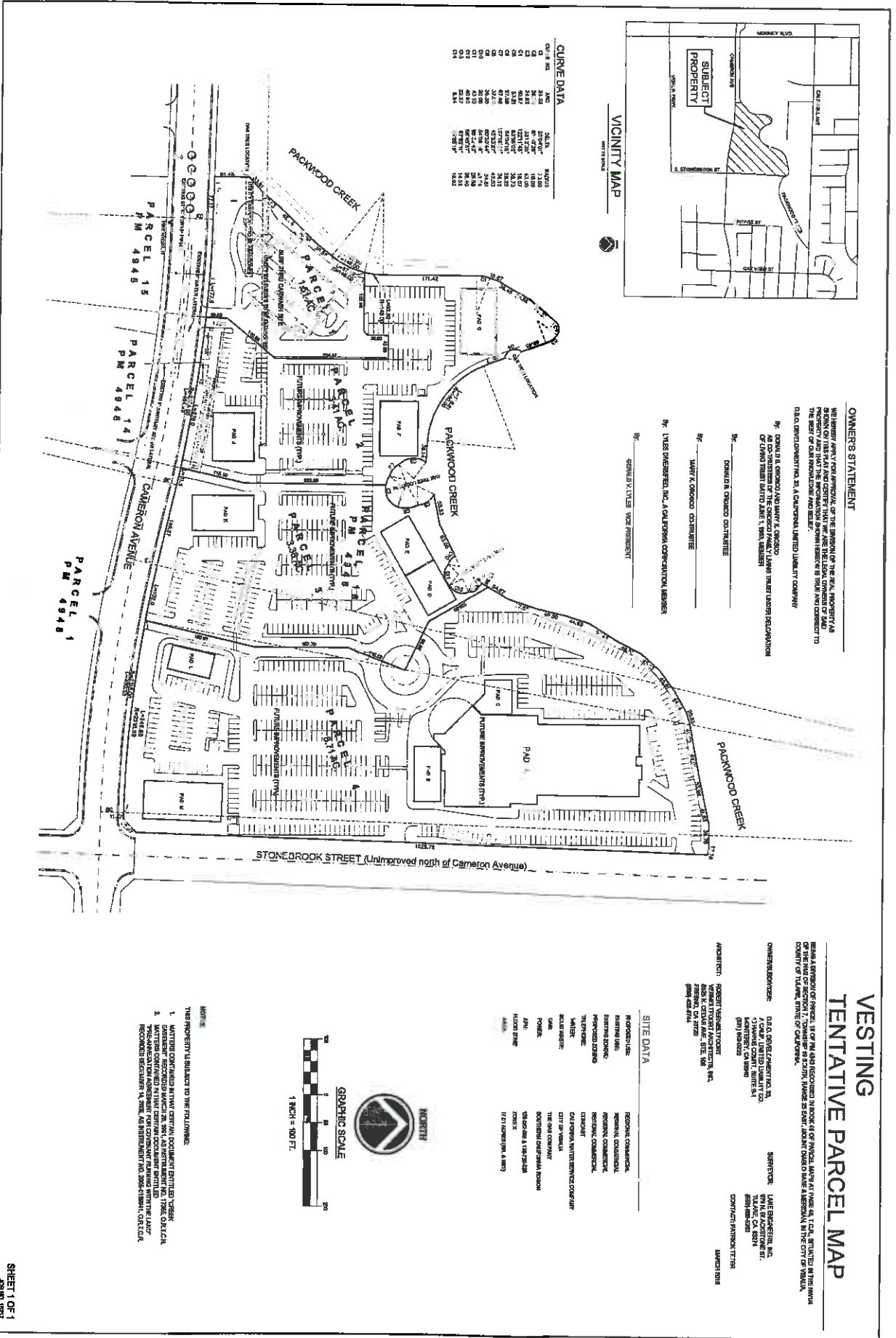
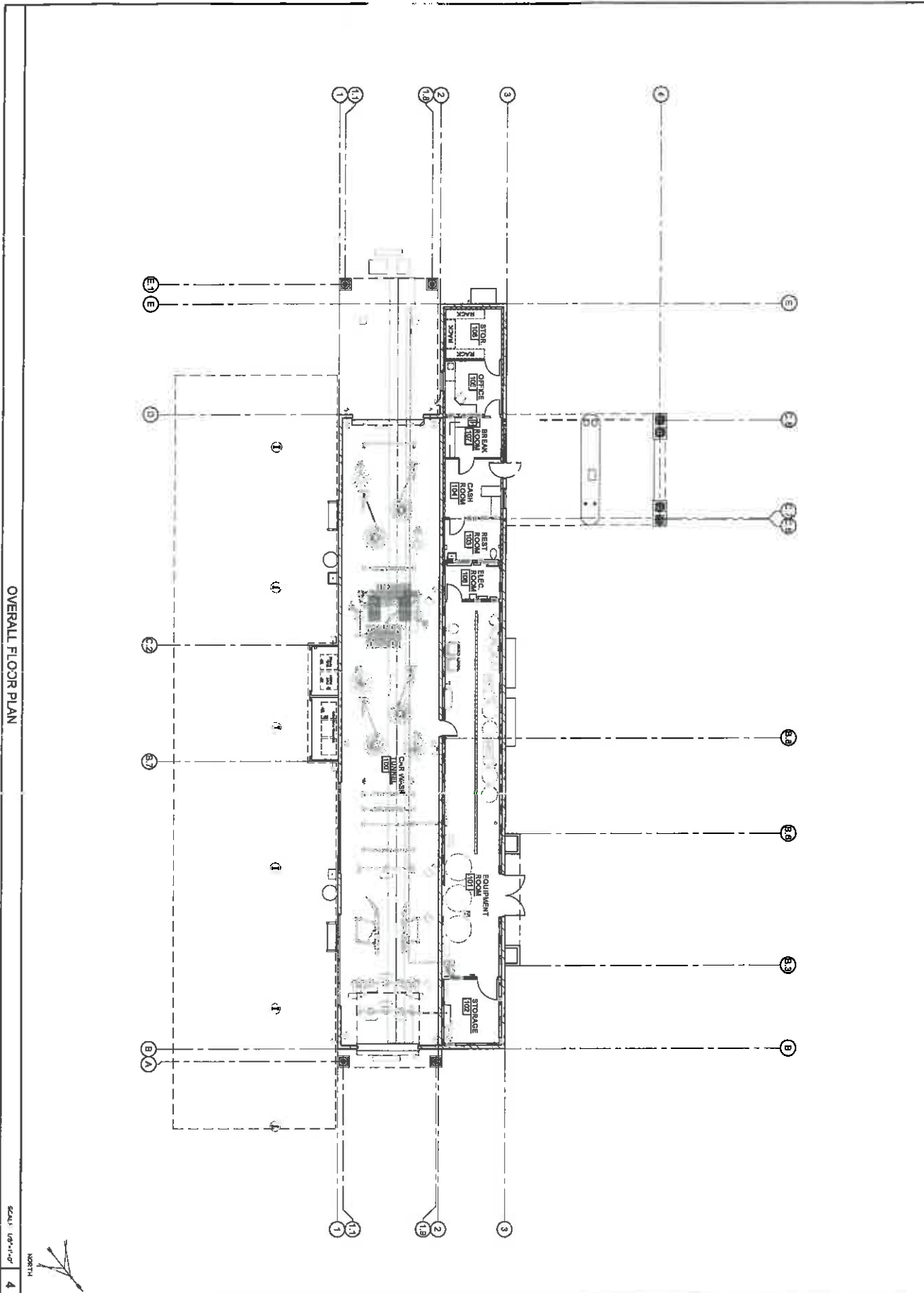


Exhibit "C"



OVERALL FLOOR PLAN

SCALE: 1/8" = 1'-0"

A200

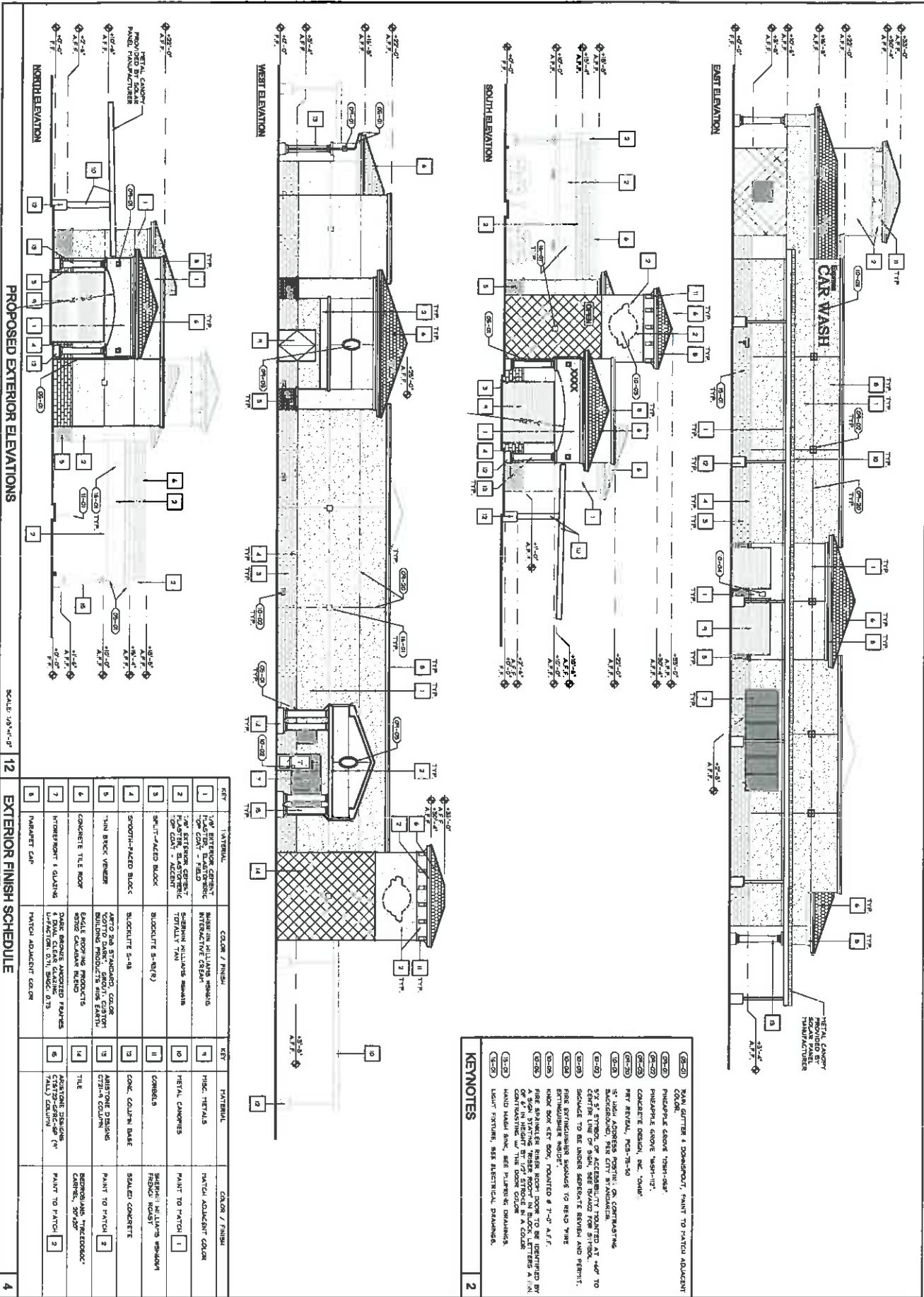
PROJECT: SURF THRU CAR WASH
 DATE: 8-4-05
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 DESIGNED BY: [Name]

SURF THRU CAR WASH
 N.W.C. OF CAMERON AVENUE &
 STONEBROOK STREET
 VISALIA, CALIFORNIA
FLOOR PLAN



Vermelfort Architects, Inc.
 Architecture and Planning
 8525 North Cedar Avenue
 Suite 106
 Fresno, California 93720
 Office: 559-432.6744
 Fax: 559-432.6745

#1918 CONDITIONAL USE PERMIT	
DATE	REVISION



PROPOSED EXTERIOR ELEVATIONS

SCALE: 1/8" = 1'-0"

12

EXTERIOR FINISH SCHEDULE

4

KEY	MATERIAL	COLOR / FINISH	KEY	MATERIAL	COLOR / FINISH
1	7" WIDE EXTENSION CERAMIC TILE - FIELD	MARSHALL ISLANDS MARBLE	9	HIGH TIEVALS	PATCH ADJACENT COLOR
2	7" WIDE EXTENSION CERAMIC TILE - CORNER	SERRINI MARBLE	10	METAL CORNICES	PAINT TO PATCH 1
3	SPALL-FACED BLOCK	BLOCKLITE S-B	11	CORNELIS	SERRINI ISLANDS MARBLE FINISH BOARD
4	TAIN BRICK VENEER	BLOCKLITE S-B	12	CONC. COLUMN BASE	SEALED CONCRETE
5	CONCRETE TILE ROOF	PERO DA STAMBOLIO COLOR KOTTO DARK, BRONZ, CANTO BUILDING PRODUCTS USA EARTH 5000 CALKUM REDD	13	ALABASTRE TESSALS	PAINT TO PATCH 2
6	WIPERSTONE T GLAZING	DARK BRONZE ANODIZED ALUMINUM FINISH (SERRINI ISLANDS MARBLE 513)	14	TILE	BEIGE/WHITE TRICKLEWOOD-CORNER, 30x30"
8	PARABET CAP	PATCH ADJACENT COLOR			PAINT TO PATCH 3

- KEYNOTES**
- (K-20) RAIN GUTTER & DOWNSPOUT, PAINT TO PATCH ADJACENT COLOR.
 - (K-21) PHYLIPPE ABOVE "STAIR-DEK"
 - (K-22) PHYLIPPE ABOVE "WASH-10"
 - (K-23) CONCRETE DESIGN, INC. "SWAB"
 - (K-24) PER REVISED, PCS-78-30
 - (K-25) IF MAIN ADDRESS PORTING ON CONTRASTING BACKGROUND, PER CITY BY ORDINANCE.
 - (K-26) CERTIFY THAT ALL ADDRESS PORTING FROM STREET TO CURBLINE IS ACCURATE PER CITY BY ORDINANCE. ADDRESS PORTING SHALL BE IDENTIFIED BY "A" IN REDDENT BY "A" STRUCK IN A CIRCLE TO INDICATE TO BE UNDER SEPARATE REVIEW AND REVISION.
 - (K-27) FIRE BRICKWORKER SHOULD TO READ THIS EXTRACT/WORKER INSIDE.
 - (K-28) HOOK BOX KEY BOX, MOUNTED @ 7'-0" A.F.F.
 - (K-29) FIRE BRICKWORKER INSIDE ROOM TO BE IDENTIFIED BY "A" IN REDDENT BY "A" STRUCK IN A CIRCLE TO INDICATE TO BE UNDER SEPARATE REVIEW AND REVISION.
 - (K-30) HAND PAINT WORK, SEE PLUMBER DRAWINGS.
 - (K-31) LIGHT FIXTURE, SEE ELECTRICAL DRAWINGS.

SURF THRU CAR WASH
 N.W.C. OF CAMERON AVENUE &
 STONEBROOK STREET
 VISALIA, CALIFORNIA
 EXTERIOR ELEVATIONS - CAR WASH



Vermeilfoort Architects, Inc.
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 Fax: 559.432.6745

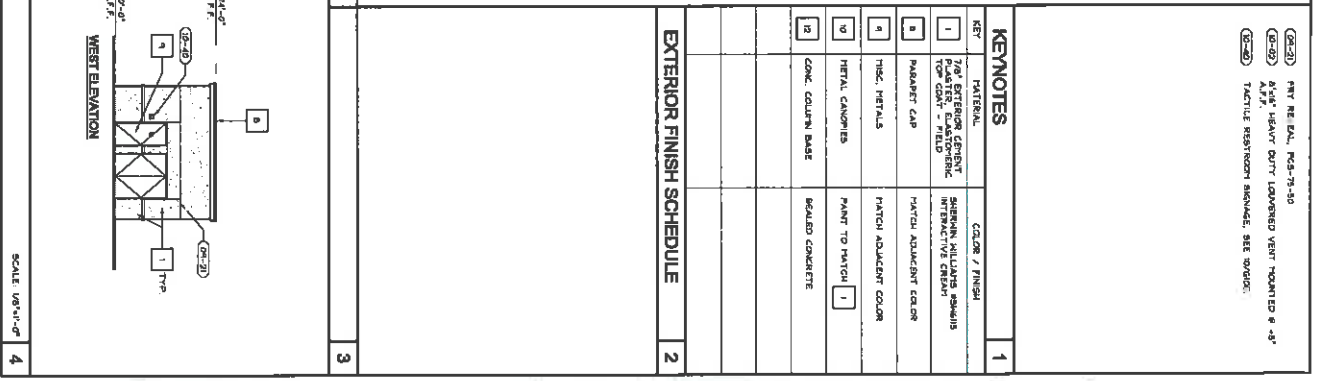
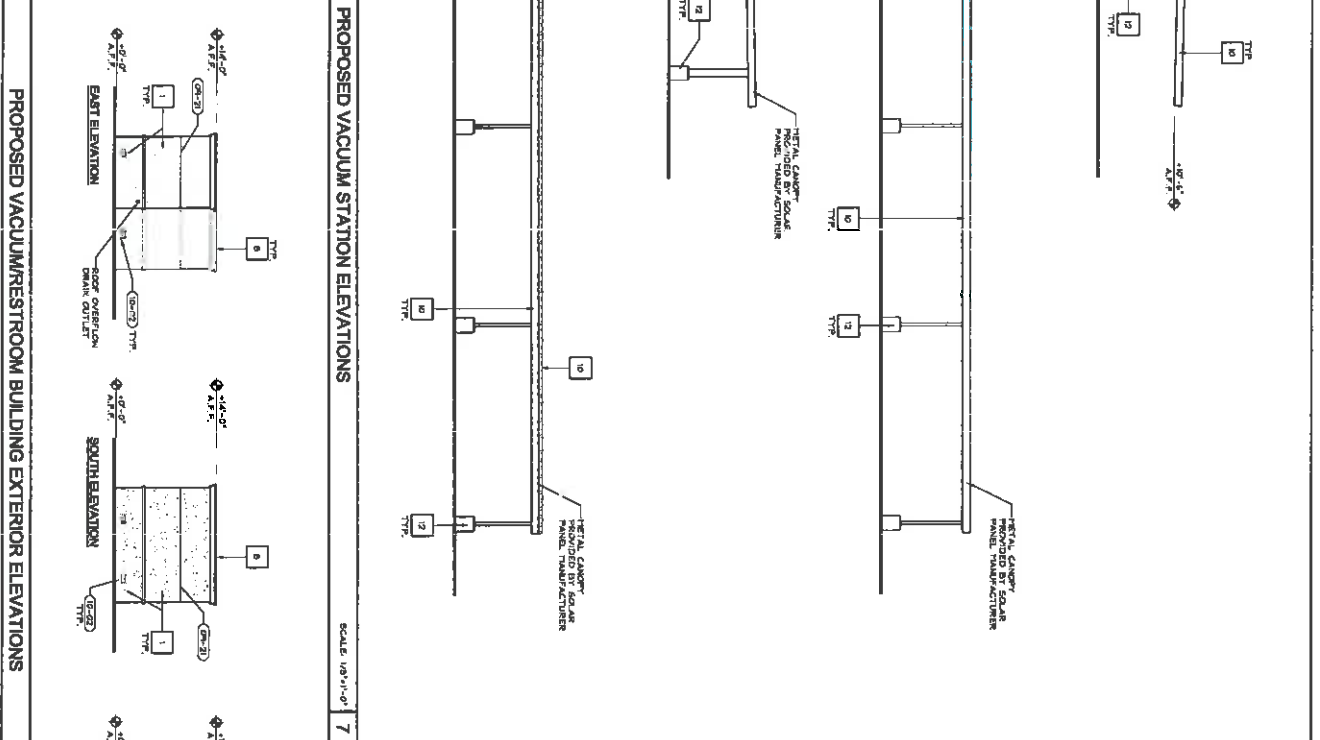
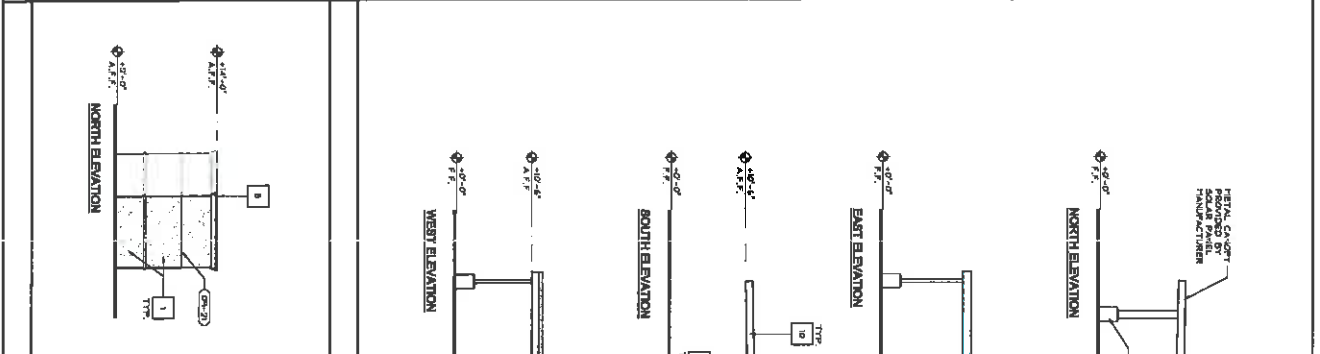
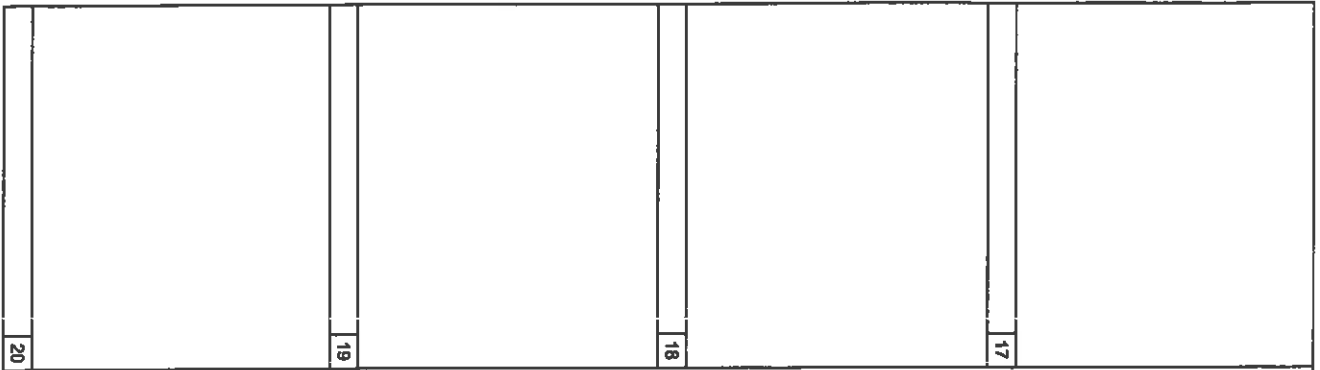
DATE: _____

REVISION: _____

DATE: _____

REVISION: _____

A300



REV	DATE	REVISION

REV	DATE	REVISION

REV	DATE	REVISION

KEYNOTES
 1 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 2 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 3 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 4 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
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 11 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 12 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER

EXTERIOR FINISH SCHEDULE
 1 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 2 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 3 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 4 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
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 11 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 12 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER

PROPOSED VACUUM RESTROOM BUILDING EXTERIOR ELEVATIONS
 SCALE: 1/8" = 1'-0"
 1 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
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PROPOSED VACUUM STATION ELEVATIONS
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PROPOSED VACUUM CAR WASH EXTERIOR ELEVATIONS
 SCALE: 1/8" = 1'-0"
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 12 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER

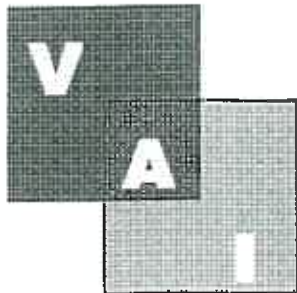
PROPOSED VACUUM RESTROOM BUILDING EXTERIOR ELEVATIONS
 SCALE: 1/8" = 1'-0"
 1 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
 2 METAL CANOPY PROVIDED BY SQUARE PANEL MANUFACTURER
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VERMELTFOORT ARCHITECTS, INC.
 Architecture and Planning
 8525 North Cedar Avenue
 Suite 116
 Fresno, California 93720
 Office: 559.432.6744
 Fax: 559.432.6745

(STAMP INVALID SEE 15 NET-SIGNED)

Surf Thru Car Wash
 N.W.C. OF CAMERON AVENUE & STONEBROOK STREET
 VISALIA, CALIFORNIA
 EXTERIOR ELEVATIONS - VACUUMS

A301



Vermeltfoort Architects Inc.
Architecture and Planning

MEMORANDUM

DATE: August 6, 2015
TO: City of Visalia
FROM: Robert Vermeltfoort
RE: Operational Statement
PROJECT #: 15049

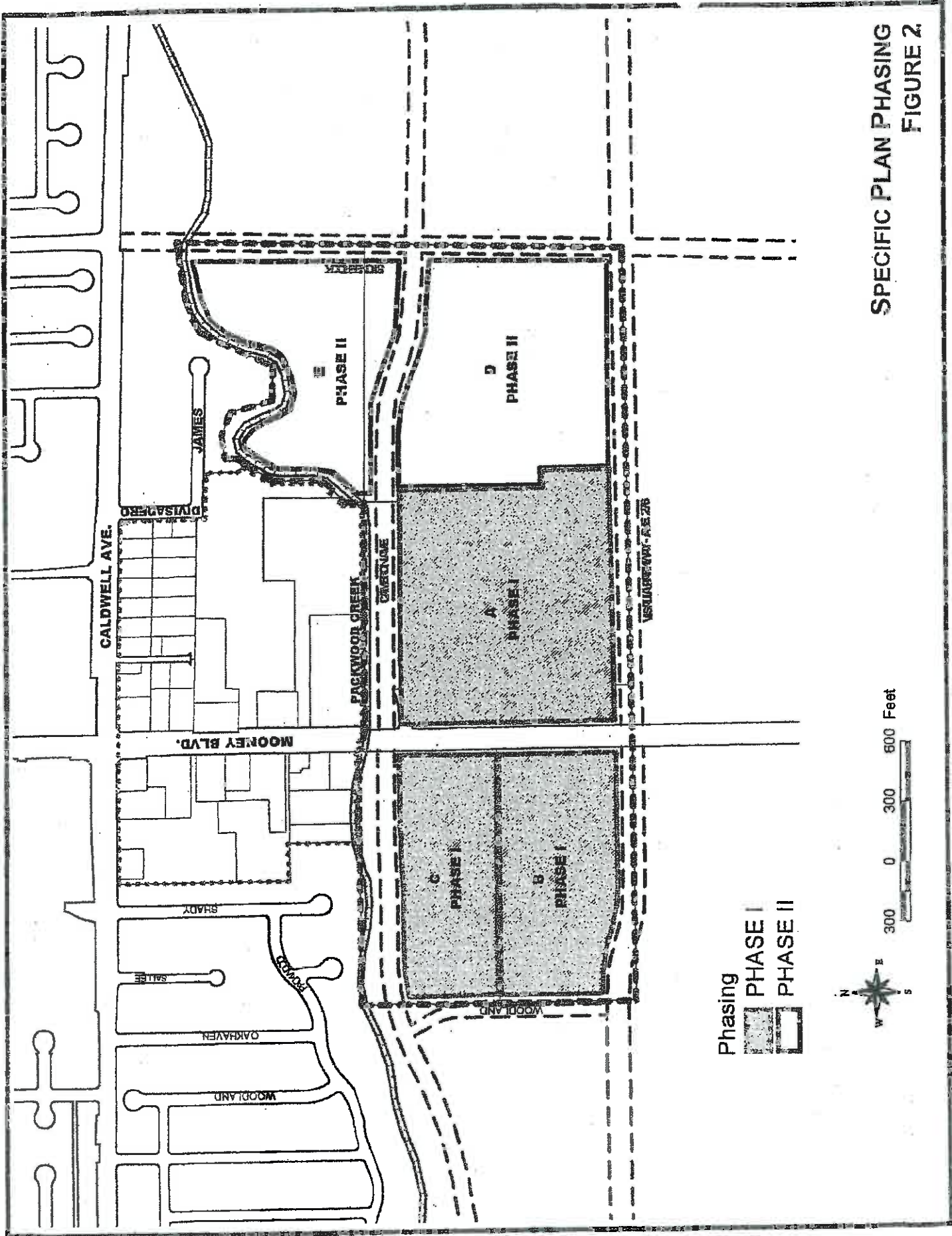
At the northwest corner of Cameron Ave. and Stonebrook St. (A.P.N. 126-062-095), we are proposing a 4,805 square foot car wash building and a 245 square foot restroom/vacuum building, along with 6,721 square feet of covered vacuum canopies.

The approximate hours of operation will be 7am to 8pm, seven days a week, 12 months a year, and will employ approximately 4 employees. The approximate expected number of cars washed is between 90 and 100 cars per day.

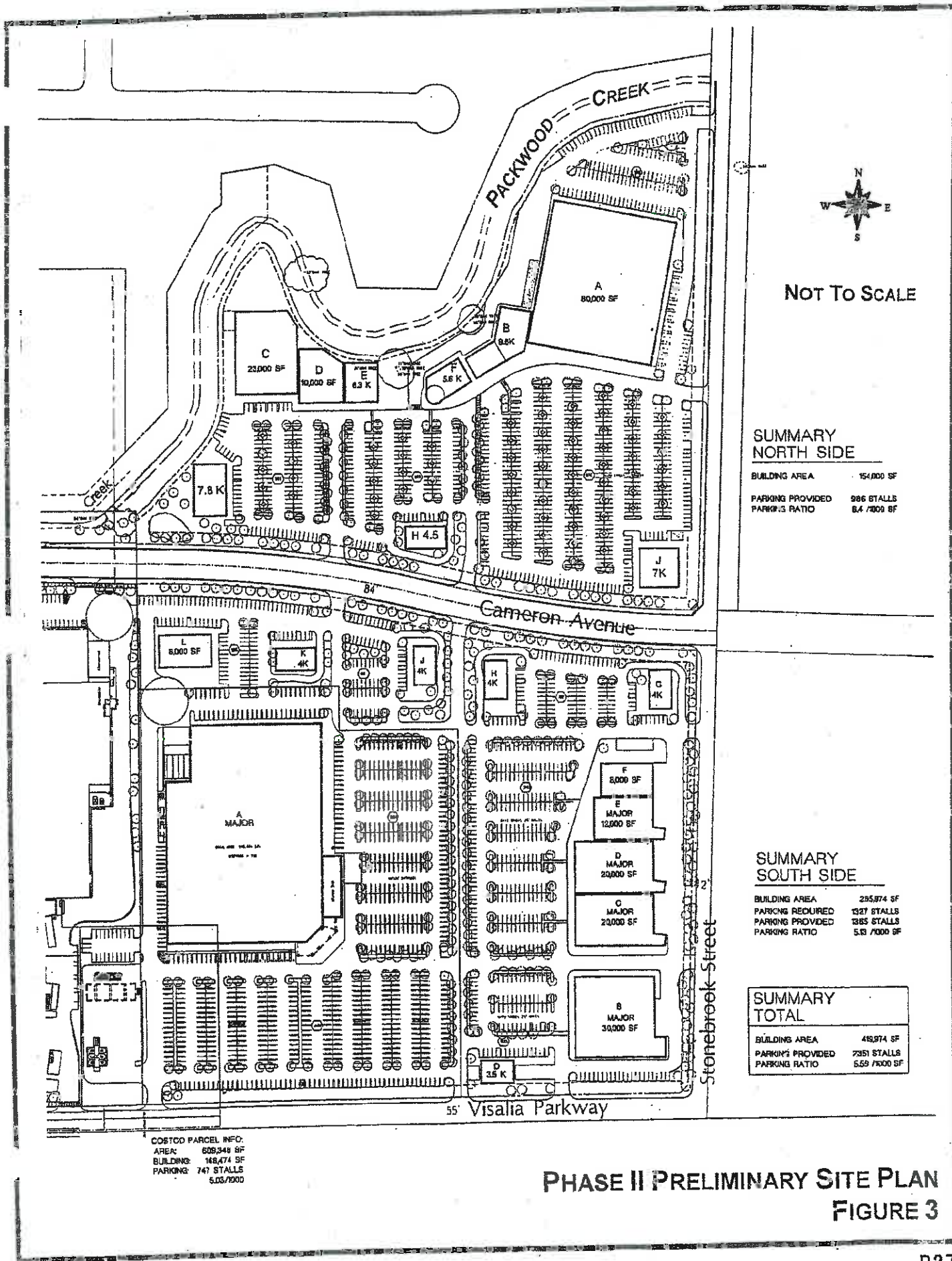
Deliveries will be by a small delivery truck (non-semi) to the breakaway driveway area, and will be approximately twice a month, with the deliveries being after the hours of operation. Noise generating activities will not exceed that of the traffic flow on Cameron Ave.

If you have any questions, please do not hesitate to contact this office.

Thank You,
Robert Vermeltfoort

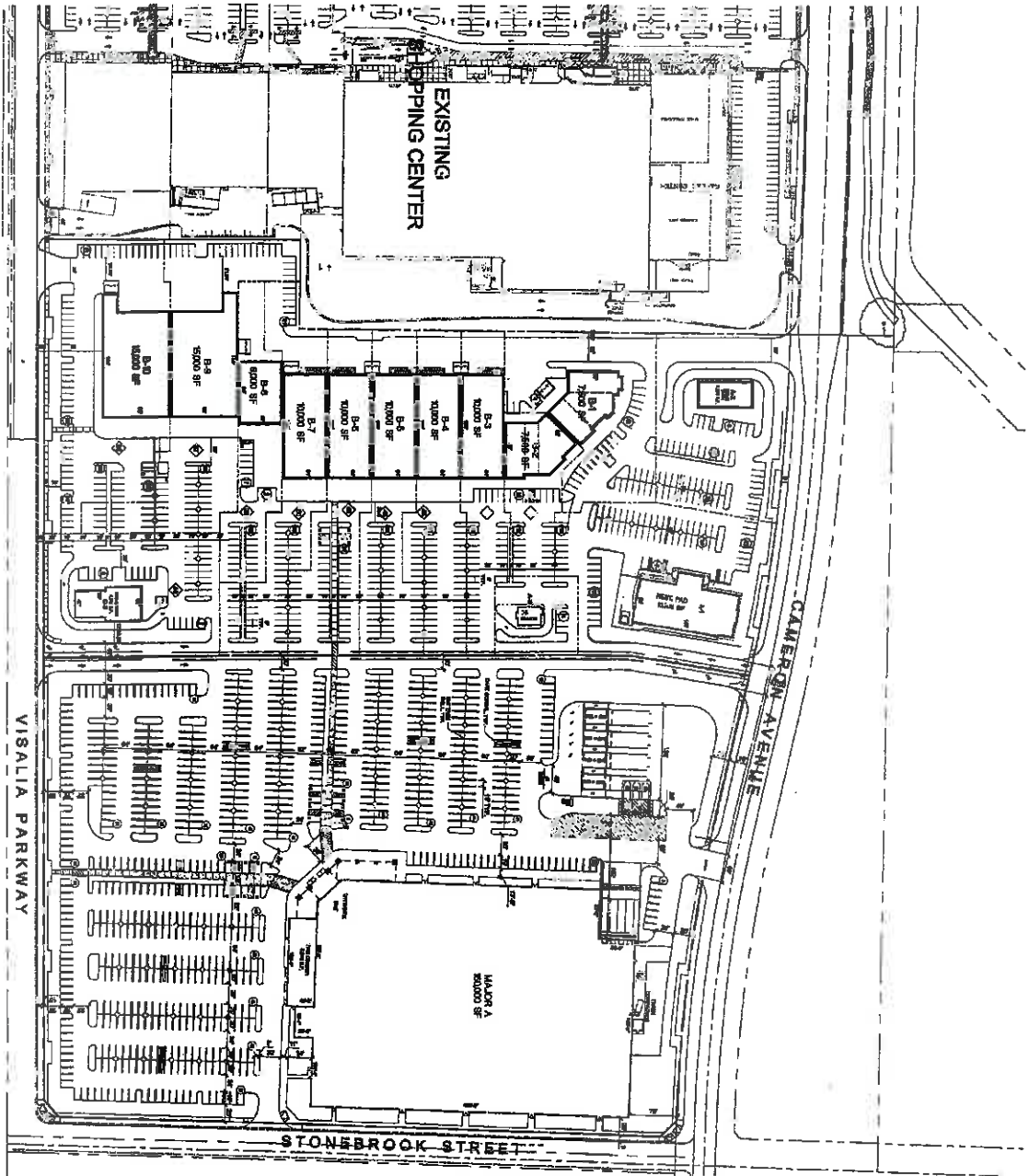


SPECIFIC PLAN PHASING
FIGURE 2



PHASE II PRELIMINARY SITE PLAN
 FIGURE 3


THIS PLAN HAS BEEN PREPARED WITHOUT REGARD TO THE ACCURACY OF THE INFORMATION ON WHICH IT IS BASED.



NO.	DESCRIPTION	AMOUNT	DATE
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Custom Site Design by
DAVID BABCOCK & ASSOCIATES
 ARCHITECTS
 1000 N. GARDEN ST., SUITE 100
 VISALIA, CA 93277
 TEL: 559.238.1111
 WWW.DBAAS.COM



Perkowitz + Ruth
 ARCHITECTS

**PACKWOOD CREEK - PHASE II
 RETAIL CENTER**

VISALIA PARKWAY &
 STONEBROOK ST.

111 W. Green Blvd
 2nd Floor
 1450 Foothill Blvd, Suite 200
 Visalia, CA 93277
 Tel: 559.238.1111
 www.perkowitz.com

Corporate Office
 Long Beach, CA
 Project Office
 Visalia, CA
 Los Angeles, CA

DATE: 02/08/11
 PREPARED BY: SJK
 PROJECT NUMBER: 10-0001

**CONCEPT
 SITE PLAN**



ASSOCIATED TRANSPORTATION ENGINEERS

100 N. Hope Avenue, Suite 4, Santa Barbara, CA 93110 • (805) 687-4418 • FAX (805) 682-8509

Since 1978

Richard L. Pool, P.E.
Scott A. Schell, AICP, PTP

April 8, 2016

16021L01.LTR

Robert Vermeltoort.
Vermeltoort Architects
8525 North Cedar, Suite #106
Fresno, CA 93720

SUPPLEMENTAL TRAFFIC ANALYSIS FOR PACKWOOD CREEK PHASE II, CITY OF VISALIA, CALIFORNIA

Associated Transportation Engineers (ATE) prepared a traffic and circulation study for the Packwood Creek Phase II. Phase II of the shopping center assumed a 262,500 square feet of retail commercial space would be built. Phase II (A) would consist of a 120,000 square feet of retail commercial space and Phase II (B) would consist of 142,500 square feet of retail commercial space. The current Phase II (B) would replace the some of the retail commercial space with a 4,800 square foot automated car wash, while Phase II (A) would be reduced slightly to 119,813 square feet of retail commercial space.

The following traffic analysis was prepared to address the City of Visalia's concerns regarding the traffic generated by only the proposed Surf Car Wash automated carwash project in Phase II (B) vs. the approved retail commercial space proposed in Phase II(B) and site access for the car wash. Figure 1 illustrates the project site plan (Attached).

Project Trip Generation

Table 1 summarizes the average daily, A.M. and P.M. peak hour trip generation estimates for the approved land use based on rates published in the Institute of Transportation (ITE), Trip Generation, 9th Edition. The average daily, A.M. and P.M. peak hour trip generation estimates for the automated car wash land use are based on rates published in the April 2002 San Diego Association of Governments (SANDAG), Traffic Generators. It's reasonable to expect that some customers of the other retail uses on-site would also be a customer of the car wash. So instead of two independent trips to the shopping center, there would be one linked internal trip to the shopping center.

**Table 1
Project Trip Generation**

Land Use	Size	Daily		A.M. Peak Hour		P.M. Peak Hour	
		Rate	Trips	Rate	Trips	Rate	Trips
Approved Phase II:							
Shopping Center (2A)	120,000 sq.ft.	42.70	5,124	0.96	115	3.71	445
Shopping Center (2B)	142,500 sq.ft.	42.70	6,085	0.96	137	3.71	529
Total Approved Phase II Trip Generation:			11,209		252		974
Proposed Phase II:							
Shopping Center (2A)	119,813 sq.ft.	42.70	5,116	0.96	115	3.71	445
Shopping Center (2B)	116,700 sq.ft.	42.70	4,983	0.96	112	3.71	433
Automated Car Wash (2B)	4,800 sq.ft.	900/Site	900	4% of ADT	36	9% of ADT	81
Total Proposed Phase II Trip Generation:			10,999		263		959
Net Trip Generation Difference:			-210		+11		-15

The proposed Packwood Creek Phase II with an automated car wash would generate less traffic than the approved Packwood Creek Phase II. Phase II as proposed would add 11 more A.M. peak hour trips and 15 fewer P.M. peak hour trips. The net addition of 11 A.M. peak hour trips would not significantly impact the adjacent street system. The car wash addition would generate a total of 900 average daily trips, 36 A.M. peak hour trips and 81 P.M. peak hour trips.

Existing Intersection Operations

Existing Levels of Service

Because traffic flow on urban arterial roadways is most constrained at intersections, detailed traffic flow analyses focus on the operating conditions of critical intersections during peak travel periods. In rating intersection operations, "Levels of Service" (LOS) A through F are used, with LOS A indicating free flow operations and LOS F indicating congested operations. The City of Visalia considers LOS D to be the minimum acceptable operating standard for intersections.

The existing levels of service for the study-area intersections were calculated utilizing the Highway Capacity Manual methodology for signalized and unsignalized intersections. The attached Figure 2 illustrates the existing traffic volumes. Worksheets showing the levels of service calculations are attached for review. Table 2 list the existing levels of service for the study-area intersections.

Table 2
Existing Intersection Levels of Service

Intersection	Control	A.M. Peak Hour		P.M. Peak Hour	
		Delay	LOS	Delay	LOS
Mooney Blvd./Cameron Ave.	Signal	18.3 sec.	LOS B	24.4 sec.	LOS C
Cameron Ave./Costco Western Dwy.	STOP-Sign	1.4 sec.	LOS A	4.9 sec.	LOS A
Cameron Ave./Costco Main Dwy.	STOP-Sign	1.8 sec.	LOS A	3.5 sec.	LOS A
Cameron Ave./Stonebrook St.	STOP-Sign	3.7 sec.	LOS A	6.0 sec.	LOS A

The study-area intersections currently operate at LOS "C" or better during the A.M. and P.M. peak hour periods, which is better than the City of Visalia LOS "D" minimum standard for intersections.

Existing Queuing Analysis

Cameron Avenue immediately adjacent to the existing Costco is a 2-lane roadway with a two-way left-turn lane. At the western Costco driveway, approximately 250' of westbound left-turn storage space is provided. At the main Costco driveway, approximately 120' of westbound left-turn storage space is provided.

The City of Visalia requested a queuing analysis of the two Costco driveway intersections adjacent to the project site. Queue lengths were forecast using the Synchro software. In addition to calculating levels of service, the Synchro software can forecast the 50th percentile and 95th percentile maximum queue length. The queuing analysis worksheets are attached for review. The results of the existing condition queuing analyses are summarized in Table 3.

Table 3
Existing Conditions Queuing Analysis Summary

Intersection	Condition	EBL	WBL	NBL	NBR
Cameron Ave./Costco Western Dwy.	Storage Length	N/A	250'	150'	150'
	A.M. Peak Queue	-	0	25'	25'
	P.M. Peak Queue	-	0	25'	25'
Cameron Ave./Main Dwy.	Storage Length	N/A	120'	150'	150'
	A.M. Peak Queue	-	25'	25'	25'
	P.M. Peak Queue	-	25'	25'	25'

Note: 95th percentile vehicle queues reported.

The 95th percentile vehicle queues are within the existing storage lengths during the A.M. and P.M. peak hour periods.

Existing + Project Intersection Operations

Traffic generated by the proposed car wash was distributed and assigned to the study-area intersections. The attached Figure 3 illustrates the existing + project traffic volumes. The existing + project levels of service for the study-area intersections were calculated utilizing the Highway Capacity Manual methodology for signalized and unsignalized intersections. Worksheets showing the levels of service calculations are attached for review. Table 4 List the levels of service for the study-area intersections.

Table 4
Existing + Project Intersection Levels of Service

Intersection	Control	A.M. Peak Hour		P.M. Peak Hour	
		Delay	LOS	Delay	LOS
Mooney Blvd./Cameron Ave.	Signal	18.5 sec.	LOS B	24.7 sec.	LOS C
Cameron Ave./Project Dwy.	STOP-Sign	0.5 sec.	LOS A	0.8 sec.	LOS A
Cameron Ave./Costco Western Dwy.	STOP-Sign	1.4 sec.	LOS A	5.3 sec.	LOS A
Cameron Ave./Costco Main Dwy.	STOP-Sign	1.8 sec.	LOS A	3.6 sec.	LOS A
Cameron Ave./Stonebrook St.	STOP-Sign	3.7 sec.	LOS A	6.1 sec.	LOS A

The study-area intersections would continue to operate at LOS "C" or better during the A.M. and P.M. peak hour periods with the addition of project traffic, which is better than the City of Visalia LOS "D" minimum acceptable operating standard for intersections.

Existing + Project Queuing Analysis

Cameron Avenue immediately adjacent to the project site is a 2-lane roadway with a two-way left-turn lane. At the project driveway, approximately 165' of eastbound left-turn storage space is provided.

The existing + project queue lengths at the project driveway and the two Costco driveways were forecast using the Synchro software. The queuing analysis worksheets are attached for review. The results of the existing + project condition queuing analyses are summarized in Table 5.

**Table 5
Existing + Project Conditions Queuing Analysis Summary**

Intersection	Condition	EBL	WBL	SBL	SBR	NBL	NBR
Cameron Ave./Car Wash Dwy.	Storage Length	150'	N/A	100'	100'	N/A	N/A
	A.M. Peak Queue	25'	-	25'	25'	-	-
	P.M. Peak Queue	25'	-	25'	25'	-	-
Cameron Ave./Costco Western Dwy.	Storage Length	N/A	120'	N/A	N/A	150'	150'
	A.M. Peak Queue	-	25'	-	-	25'	25'
	P.M. Peak Queue	-	25'	-	-	25'	25'
Cameron Ave./Main Dwy.	Storage Length	N/A	250'	N/A	N/A	150'	150'
	A.M. Peak Queue	-	25'	-	-	25'	25'
	P.M. Peak Queue	-	25'	-	-	25'	25'

Note: 95th percentile vehicle queues reported.

The 95th percentile vehicle queues are within the existing storage lengths during the A.M. and P.M. peak hour periods.

Site Access and Circulation

Site Access

As shown on the project site plan, access to the Surf Car Wash will be provided via a single driveway connection on Cameron Avenue. Cameron Avenue immediately adjacent to the project site is a 2-lane roadway with a two-way left-turn lane. No on-street parking is allowed on Cameron Avenue.

The two-way left-turn lane on Cameron Avenue will provide approximately 165 feet of eastbound left-turn storage into the project site. As shown in the queuing analysis, there is adequate storage space for left-turning vehicles into the car wash. The project access connection on Cameron Avenue is approximately 250 feet west of the existing western Costco driveway and would not result in any turning movement conflicts between the two driveways. The Cameron Avenue/Project Driveway intersection is projected to operate at LOS "A" during

the A.M. and P.M. peak hour periods. Given the estimated vehicle queue of 25 feet, the driveway throat depth of 60 feet is adequate.

Site Circulation

The car wash site is laid out such that entering vehicles circulate counter clockwise on-site. Vehicles enter the site from the south, circulate to the north then turn southbound counter clockwise entering the 2-lane vehicle staging area in a southbound direction. The entrance to 2-lane vehicle staging area for customers to pay for a car wash is more than 400' from the car wash driveway entry point. The vehicle staging area can accommodate up to 15 vehicles in 2-lanes without blocking the exit point of the car wash tunnel. After leaving the staging area, vehicles turn east then travel northbound entering the automated car wash tunnel. The car wash tunnel is oriented north-south such that vehicles enter from the south and exit at the north end on the project site away from the driveway entry point. After exiting the tunnel vehicle turn south to enter the detailing and vacuuming area before entering the exit lane to Cameron Avenue.

Associated Transportation Engineers



Richard L. Pool, P.E.
President

RLP/DFN/wp

Attachments: Figure 1 - Project Site Plan
Figure 2 - Existing Traffic Volumes
Figure 3 - Existing + Project Traffic Volumes
LOS/Queue Worksheets



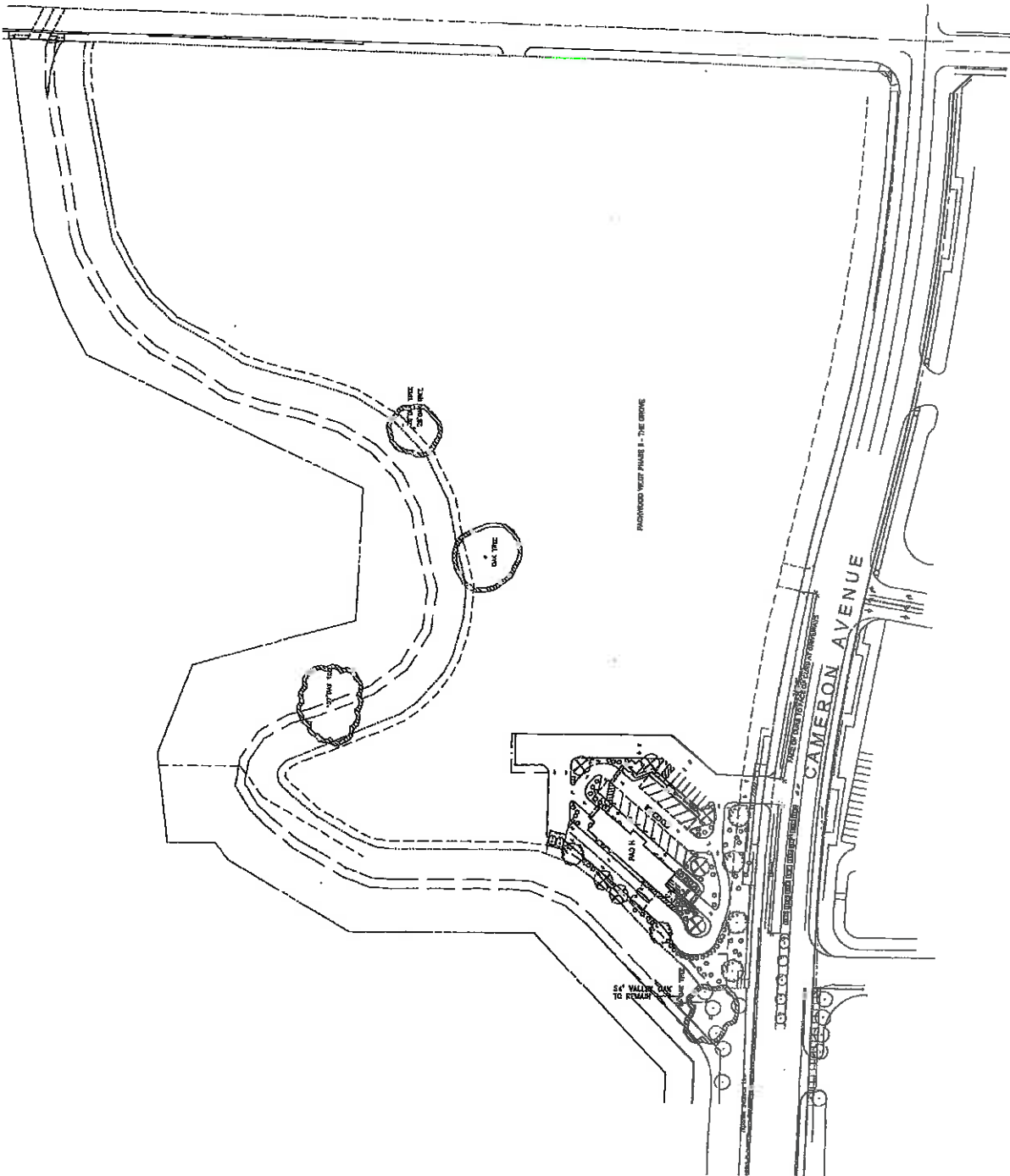
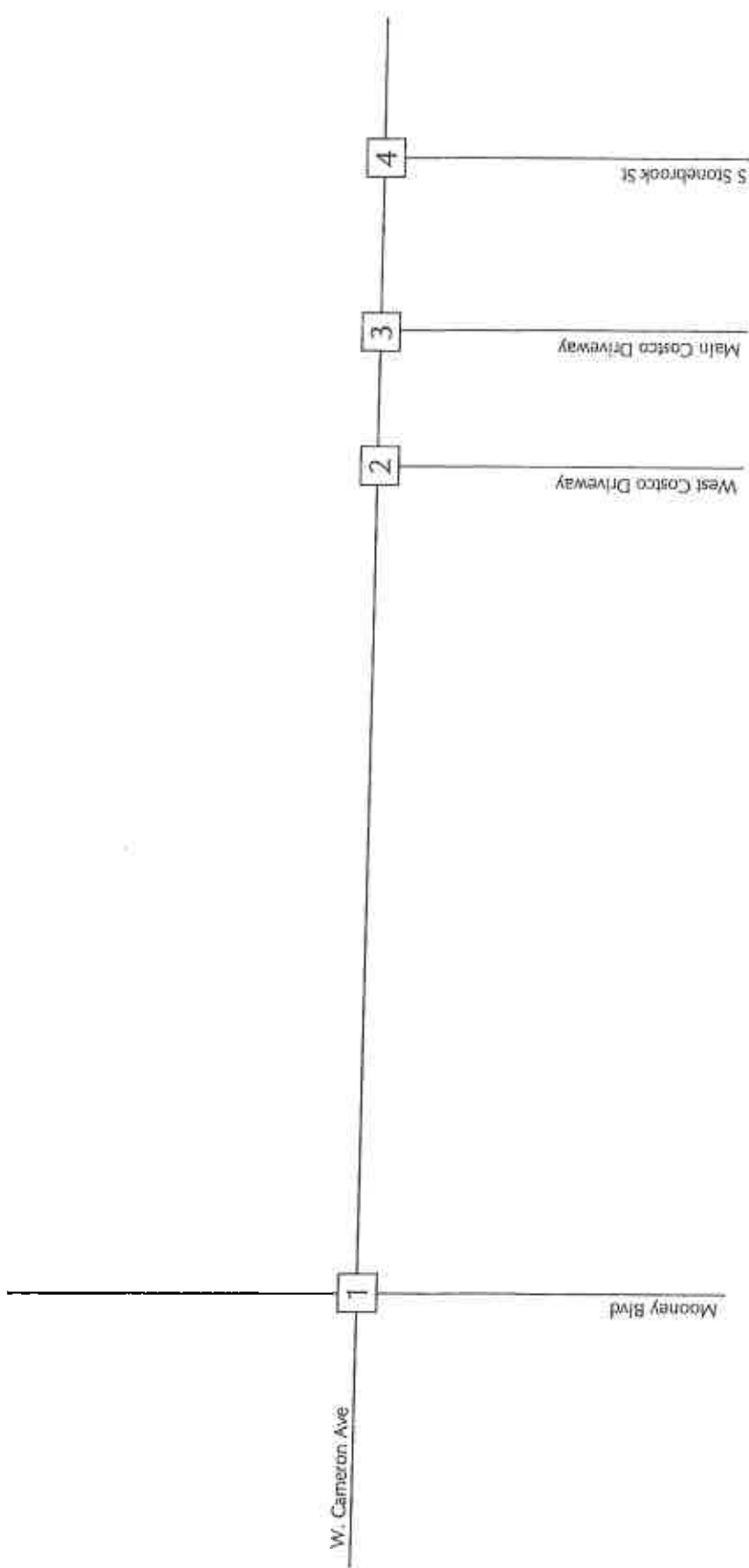


FIGURE 1

PROJECT SITE PLAN



1	<p>138(55)</p> <p>259(116)</p> <p>21(10)</p>	<p>273(64)</p> <p>924(401)</p> <p>138(32)</p>	<p>88(186)</p> <p>142(147)</p> <p>64(110)</p>	<p>287(348)</p> <p>5(3)</p>	<p>429(187)</p> <p>229(85)</p>	<p>27(304)</p> <p>65(106)</p>	<p>504(219)</p> <p>8(1)</p>	<p>336(406)</p> <p>266(190)</p>	<p>606(261)</p> <p>45(8)</p>	<p>(11)52</p> <p>(51)131</p>	<p>(34)83</p> <p>(21)46</p>	<p>(126)278</p> <p>(0)5</p>
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NOT TO SCALE

LEGEND

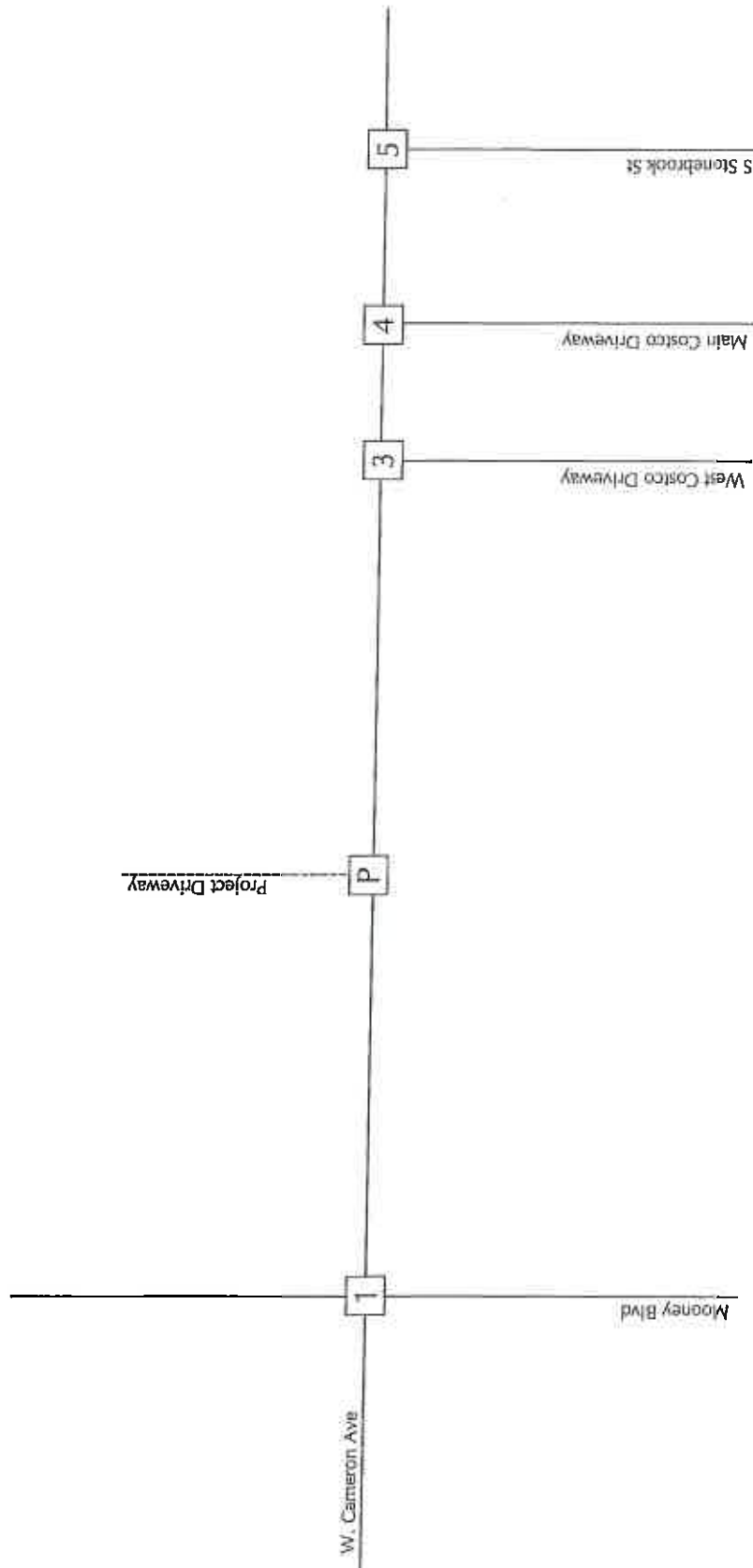
— (A.M.) P.M. Peak Hour Volume



ASSOCIATED
TRANSPORTATION
ENGINEERS

EXISTING TRAFFIC VOLUMES

FIGURE 2



NOT TO SCALE

LEGEND

XXXXXX - (A.M.)P.M. Peak Hour Volume

1	138(55) 274(122) 21(10)	273(64) 924(401) 138(32)	(88)186 (148)162 (67)115	20(9) 610(269)	20(9) 610(269)	(9)21 (338)479	626(270) 45(8)	(11)52 (51)131	449(196) 229(85)	(34)83 (21)46	524(228) 8(1)	(126)278 (0)5
2			(296)369 (5)3									
3			(280)325 (65)106									
4			(345)427 (266)190									



ASSOCIATED
TRANSPORTATION
ENGINEERS

EXISTING + PROJECT TRAFFIC VOLUMES

Lanes, Volumes, Timings

3: Mooney Boulevard/Mooney Boulevard & Cameron Avenue

A.M. Peak Hour

4/8/2016



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕		↖	↕		↖	↕	↗	↖	↕	↗
Traffic Volume (vph)	55	116	10	64	142	88	11	517	98	64	401	32
Future Volume (vph)	55	116	10	64	142	88	11	517	98	64	401	32
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	150		0	200		0	250		0	200		150
Storage Lanes	1		0	1		0	2		1	2		1
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	0.97	0.91	1.00	0.97	0.91	1.00
Ped Bike Factor												
Frnt		0.988			0.942				0.850			0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1770	3497	0	1770	3334	0	3433	5085	1583	3433	5085	1583
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1770	3497	0	1770	3334	0	3433	5085	1583	3433	5085	1583
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		9			96				127			127
Link Speed (mph)		30			30			30				30
Link Distance (ft)		213			1737			460				418
Travel Time (s)		4.8			39.5			10.5				9.5
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Adj. Flow (vph)	60	126	11	70	154	96	12	562	107	70	436	35
Shared Lane Traffic (%)												
Lane Group Flow (vph)	60	137	0	70	250	0	12	562	107	70	436	35
Turn Type	Prot	NA		Prot	NA		Prot	NA	Perm	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases									2			6
Detector Phase	7	4		3	8		5	2	2	1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5		9.5	22.5		9.5	22.5	22.5	9.5	22.5	22.5
Total Split (s)	18.0	26.0		18.0	26.0		11.0	33.0	33.0	13.0	35.0	35.0
Total Split (%)	20.0%	28.9%		20.0%	28.9%		12.2%	36.7%	36.7%	14.4%	38.9%	38.9%
Yellow Time (s)	3.5	3.5		3.5	3.5		3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0		1.0	1.0		1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5		4.5	4.5		4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	C-Max
Act Effect Green (s)	8.4	9.2		8.9	9.6		5.9	50.7	50.7	7.2	58.2	58.2

Lanes, Volumes, Timings
 3: Mooney Boulevard/Mooney Boulevard & Cameron Avenue

A.M. Peak Hour
 4/8/2016

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBH
Actuated g/C Ratio	0.09	0.10		0.10	0.11		0.07	0.56	0.56	0.08	0.65	0.65
v/c Ratio	0.36	0.38		0.40	0.57		0.05	0.20	0.11	0.26	0.13	0.03
Control Delay	43.6	37.5		44.1	28.0		39.7	12.1	2.4	40.7	8.2	0.1
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	43.6	37.5		44.1	28.0		39.7	12.1	2.4	40.7	8.2	0.1
LOS	D	D		D	C		D	B	A	D	A	A
Approach Delay		39.4			31.5			11.1			11.9	
Approach LOS		D			C			B			B	
Queue Length 50th (ft)	33	36		38	43		3	59	0	19	30	0
Queue Length 95th (ft)	69	62		76	78		11	98	22	39	72	0
Internal Link Dist (ft)		133			1657			380			338	
Turn Bay Length (ft)	150			200			250			200		150
Base Capacity (vph)	265	842		265	869		247	2865	947	327	3289	1068
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.23	0.16		0.26	0.29		0.05	0.20	0.11	0.21	0.13	0.03

Intersection Summary

Area Type: Other
 Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
 Natural Cycle: 65
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.57
 Intersection Signal Delay: 18.3
 Intersection Capacity Utilization 40.1%
 Analysis Period (min) 15
 Intersection LOS: B
 ICU Level of Service A

Splits and Phases: 3: Mooney Boulevard/Mooney Boulevard & Cameron Avenue



Lanes, Volumes, Timings
3: Mooney Boulevard & Cameron Avenue

Existing Conditions P.M.
4/8/2016

Lane Group	EBL	ERT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕		↖	↕		↖	↕		↖	↕	↖
Traffic Volume (vph)	138	259	21	110	147	186	49	807	107	273	924	138
Future Volume (vph)	138	259	21	110	147	186	49	807	107	273	924	138
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	150		0	200		0	250		0	200		150
Storage Lanes	1		0	1		0	2		1	2		1
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	0.97	0.91	1.00	0.97	0.91	1.00
Ped Bike Factor												
Frnt		0.989			0.916				0.850			0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1770	3500	0	1770	3242	0	3433	5085	1583	3433	5085	1583
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1770	3500	0	1770	3242	0	3433	5085	1583	3433	5085	1583
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		9			202				127			150
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		213			1737			460			418	
Travel Time (s)		4.8			39.5			10.5			9.5	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Adj. Flow (vph)	150	282	23	120	160	202	53	877	116	297	1004	150
Shared Lane Traffic (%)												
Lane Group Flow (vph)	150	305	0	120	362	0	53	877	116	297	1004	150
Turn Type	Prot	NA		Prot	NA		Prot	NA	Perm	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases									2			6
Detector Phase	7	4		3	8		5	2	2	1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5		9.5	22.5		9.5	22.5	22.5	9.5	22.5	22.5
Total Split (s)	18.0	26.0		18.0	26.0		11.0	33.0	33.0	13.0	35.0	35.0
Total Split (%)	20.0%	28.9%		20.0%	28.9%		12.2%	36.7%	36.7%	14.4%	38.9%	38.9%
Yellow Time (s)	3.5	3.5		3.5	3.5		3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0		1.0	1.0		1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5		4.5	4.5		4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	C-Max
Act Effct Green (s)	11.7	12.8		10.9	12.0		6.8	34.9	34.9	13.3	45.6	45.6

Lanes, Volumes, Timings
3: Mooney Boulevard & Cameron Avenue

Existing Conditions P.M.
4/8/2016

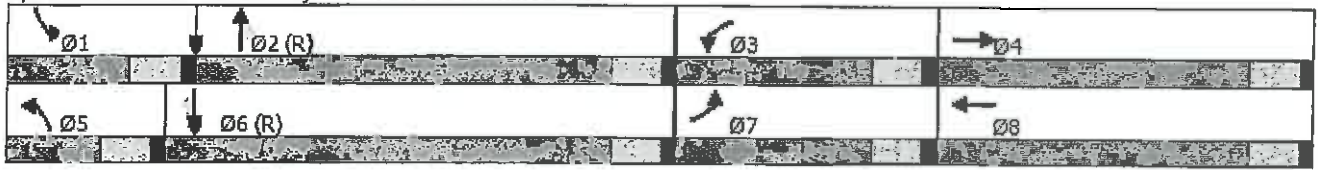


Lane Group	EBL	EBT	EBR	WBL	WBT	WBH	NBL	NBT	NBB	SBL	SBT	SBR
Actuated g/C Ratio	0.13	0.14		0.12	0.13		0.08	0.39	0.39	0.15	0.51	0.51
v/c Ratio	0.65	0.60		0.56	0.60		0.20	0.44	0.17	0.58	0.39	0.17
Control Delay	50.6	39.9		47.0	19.6		40.4	22.5	4.3	40.6	16.3	3.8
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	50.6	39.9		47.0	19.6		40.4	22.5	4.3	40.6	16.3	3.8
LOS	D	D		D	B		D	C	A	D	B	A
Approach Delay		43.4			26.4			21.4			20.0	
Approach LOS		D			C			C			C	
Queue Length 50th (ft)	81	84		65	44		14	136	0	81	137	0
Queue Length 95th (ft)	142	121		117	83		32	190	32	123	201	37
Internal Link Dist (ft)		133			1657			380			338	
Turn Bay Length (ft)	150			200			250			200		150
Base Capacity (vph)	265	842		265	928		270	1973	691	508	2574	875
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.57	0.36		0.45	0.39		0.20	0.44	0.17	0.58	0.39	0.17

Intersection Summary

Area Type: Other
 Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
 Natural Cycle: 70
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.65
 Intersection Signal Delay: 24.4
 Intersection Capacity Utilization 56.1%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service B

Splits and Phases: 3: Mooney Boulevard & Cameron Avenue



Lanes, Volumes, Timings
3: Mooney Boulevard & Cameron Avenue

Existing + Project Conditions A.M.

4/8/2016

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↕		↘	↕		↗	↗	↗	↘	↕	↘
Traffic Volume (vph)	55	122	10	67	148	88	11	517	101	64	401	32
Future Volume (vph)	55	122	10	67	148	88	11	517	101	64	401	32
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	150		0	200		0	250		0	200		150
Storage Lanes	1		0	1		0	2		1	2		1
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	0.97	0.91	1.00	0.97	0.91	1.00
Ped Bike Factor												
Frnt		0.989			0.944				0.850			0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1770	3500	0	1770	3341	0	3433	5085	1583	3433	5085	1583
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1770	3500	0	1770	3341	0	3433	5085	1583	3433	5085	1583
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		9			96				182			127
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		213			1475			460			418	
Travel Time (s)		4.8			33.5			10.5			9.5	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Adj. Flow (vph)	60	133	11	73	161	96	12	562	110	70	436	35
Shared Lane Traffic (%)												
Lane Group Flow (vph)	60	144	0	73	257	0	12	562	110	70	436	35
Turn Type	Prot	NA		Prot	NA		Prot	NA	Perm	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases									2			6
Detector Phase	7	4		3	8		5	2	2	1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5		9.5	22.5		9.5	22.5	22.5	9.5	22.5	22.5
Total Split (s)	20.0	25.0		18.0	23.0		9.5	29.0	29.0	18.0	37.5	37.5
Total Split (%)	22.2%	27.8%		20.0%	25.6%		10.6%	32.2%	32.2%	20.0%	41.7%	41.7%
Yellow Time (s)	3.5	3.5		3.5	3.5		3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0		1.0	1.0		1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5		4.5	4.5		4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	C-Max
Act Effect Green (s)	8.4	9.2		9.0	9.8		5.9	50.5	50.5	7.2	58.0	58.0

Lanes, Volumes, Timings
 3: Mooney Boulevard & Cameron Avenue

Existing + Project Conditions A.M.
 4/8/2016

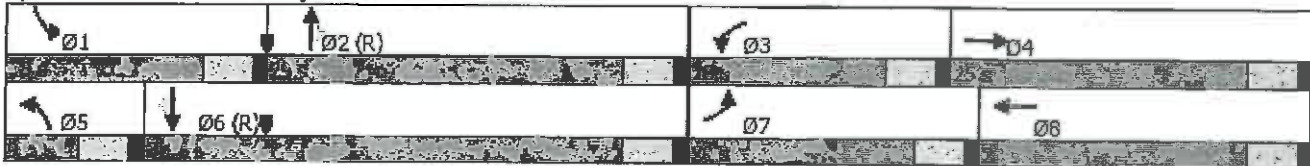


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SEB	SEB	SEB
Actuated g/C Ratio	0.09	0.10		0.10	0.11		0.07	0.56	0.56	0.08	0.64	0.64
v/c Ratio	0.36	0.39		0.41	0.57		0.05	0.20	0.11	0.25	0.13	0.09
Control Delay	43.6	37.9		44.2	28.3		39.7	12.2	0.6	40.7	8.3	0.1
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	43.6	37.9		44.2	28.3		39.7	12.2	0.6	40.7	8.3	0.1
LOS	D	D		D	C		D	B	A	D	A	A
Approach Delay		39.6			31.8			10.9			11.9	
Approach LOS		D			C			B			B	
Queue Length 50th (ft)	33	38		40	45		3	60	0	19	31	0
Queue Length 95th (ft)	69	65		79	81		11	98	5	39	73	0
Internal Link Dist (ft)		133			1395			380			338	
Turn Bay Length (ft)	150			200			250		200			150
Base Capacity (vph)	304	804		265	763		224	2854	968	514	3279	1065
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.20	0.18		0.28	0.34		0.05	0.20	0.11	0.14	0.13	0.03

Intersection Summary

Area Type: Other
 Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
 Natural Cycle: 65
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.57
 Intersection Signal Delay: 18.5
 Intersection Capacity Utilization 40.2%
 Analysis Period (min) 15
 Intersection LOS: B
 ICU Level of Service A

Splits and Phases: 3: Mooney Boulevard & Cameron Avenue



Lanes, Volumes, Timings
3: Mooney Boulevard & Cameron Avenue

Existing + Project Conditions P.M.
4/8/2016

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↕		↙	↕		↗	↕		↖	↕	↘
Traffic Volume (vph)	138	274	21	115	162	186	49	807	112	273	924	138
Future Volume (vph)	138	274	21	115	162	186	49	807	112	273	924	138
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	150		0	200		0	250		0	200		150
Storage Lanes	1		0	1		0	2		1	2		1
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	0.97	0.91	1.00	0.97	0.91	1.00
Ped Bike Factor												
Frt		0.989			0.920				0.850			0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1770	3500	0	1770	3256	0	3433	5085	1583	3433	5085	1583
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1770	3500	0	1770	3256	0	3433	5085	1583	3433	5085	1583
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			202				182			150
Link Speed (mph)		30			30			30			30	
Link Distance (ft)		213			1475			460			418	
Travel Time (s)		4.8			33.5			10.5			9.5	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Adj. Flow (vph)	150	298	23	125	176	202	53	877	122	297	1004	150
Shared Lane Traffic (%)												
Lane Group Flow (vph)	150	321	0	125	378	0	53	877	122	297	1004	150
Turn Type	Prot	NA		Prot	NA		Prot	NA	Perm	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases									2			6
Detector Phase	7	4		3	8		5	2	2	1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	9.5	22.5		9.5	22.5		9.5	22.5	22.5	9.5	22.5	22.5
Total Split (s)	20.0	25.0		18.0	23.0		9.5	29.0	29.0	18.0	37.5	37.5
Total Split (%)	22.2%	27.8%		20.0%	25.6%		10.6%	32.2%	32.2%	20.0%	41.7%	41.7%
Yellow Time (s)	3.5	3.5		3.5	3.5		3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.0	1.0		1.0	1.0		1.0	1.0	1.0	1.0	1.0	1.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	4.5	4.5		4.5	4.5		4.5	4.5	4.5	4.5	4.5	4.5
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lag	Lag	Lead	Lag	Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes	Yes	Yes	Yes	Yes
Recall Mode	None	None		None	None		None	C-Max	C-Max	None	C-Max	C-Max
Act Effect Green (s)	12.4	13.5		11.1	12.2		6.7	34.5	34.5	12.9	44.9	44.9

Lanes, Volumes, Timings
3: Mooney Boulevard & Cameron Avenue

Existing + Project Conditions P.M.
4/8/2016

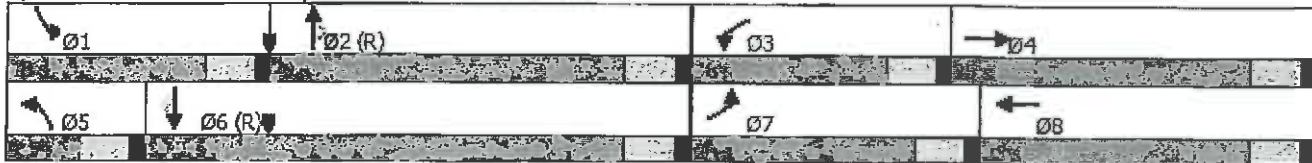


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Actuated g/C Ratio	0.14	0.15		0.12	0.14		0.07	0.38	0.38	0.14	0.50	0.50
v/c Ratio	0.62	0.60		0.58	0.61		0.21	0.45	0.17	0.60	0.40	0.17
Control Delay	47.3	39.4		47.5	20.9		40.8	23.2	1.7	41.3	16.8	3.9
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	47.3	39.4		47.5	20.9		40.8	23.2	1.7	41.3	16.8	3.9
LOS	D	D		D	C		D	C	A	D	B	A
Approach Delay		41.9			27.5			21.6			20.5	
Approach LOS		D			C			C			C	
Queue Length 50th (ft)	81	88		68	48		14	134	0	82	139	0
Queue Length 95th (ft)	138	124		121	89		32	204	13	119	204	37
Internal Link Dist (ft)		133			1395			380			338	
Turn Bay Length (ft)	150			200			250			200		150
Base Capacity (vph)	304	803		265	829		255	1951	719	541	2534	864
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.49	0.40		0.47	0.46		0.21	0.45	0.17	0.55	0.40	0.17

Intersection Summary

Area Type: Other
 Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBT, Start of Green
 Natural Cycle: 70
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.62
 Intersection Signal Delay: 24.7
 Intersection Capacity Utilization 56.5%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service B

Splits and Phases: 3: Mooney Boulevard & Cameron Avenue



Intersection

Int Delay, s/veh 0.5

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Traffic Vol, veh/h	9	269	338	9	9	9
Future Vol, veh/h	9	269	338	9	9	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	165	-	-	-	-	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	10	292	367	10	10	10

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	377	0	684	372
Stage 1	-	-	372	-
Stage 2	-	-	312	-
Critical Hdwy	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	5.42	-
Critical Hdwy Stg 2	-	-	5.42	-
Follow-up Hdwy	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	1181	-	414	674
Stage 1	-	-	697	-
Stage 2	-	-	742	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	1181	-	410	674
Mov Cap-2 Maneuver	-	-	410	-
Stage 1	-	-	697	-
Stage 2	-	-	736	-

Approach	EB	WB	SB
HCM Control Delay, s	0.3	0	12.3
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBL	SBR
Capacity (veh/h)	1181	-	-	-	510	-
HCM Lane V/C Ratio	0.008	-	-	-	0.038	-
HCM Control Delay (s)	8.1	-	-	-	12.3	-
HCM Lane LOS	A	-	-	-	B	-
HCM 95th %tile Q(veh)	0	-	-	-	0.1	-

Intersection	
Int Delay, s/veh	0.8

Movement	EBL	EBT	WBT	WBR	SEB	SEB
Traffic Vol, veh/h	20	610	479	21	20	20
Future Vol, veh/h	20	610	479	21	20	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	165	-	-	-	-	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	663	521	23	22	22

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	543	0	1239
Stage 1	-	-	532
Stage 2	-	-	707
Critical Hdwy	4.12	-	6.42
Critical Hdwy Stg 1	-	-	5.42
Critical Hdwy Stg 2	-	-	5.42
Follow-up Hdwy	2.218	-	3.518
Pot Cap-1 Maneuver	1026	-	194
Stage 1	-	-	589
Stage 2	-	-	489
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	1026	-	190
Mov Cap-2 Maneuver	-	-	190
Stage 1	-	-	589
Stage 2	-	-	479

Approach	EB	WB	SB
HCM Control Delay, s	0.3	0	20.1
HCM LOS			C

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SEB
Capacity (veh/h)	1026	-	-	-	282
HCM Lane V/C Ratio	0.021	-	-	-	0.154
HCM Control Delay (s)	8.6	-	-	-	20.1
HCM Lane LOS	A	-	-	-	C
HCM 95th %tile Q(veh)	0.1	-	-	-	0.5

Intersection	
Int Delay, s/veh	1.4

Movement	EBT	EBP	WBL	WBT	NBL	NBT
Traffic Vol, veh/h	261	8	5	287	51	11
Future Vol, veh/h	261	8	5	287	51	11
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	300	-	150	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	284	9	5	312	55	12

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	292	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	1270	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	1270	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.1	13.3
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	NBLn2	EPT	EBR	WBL	WBT
Capacity (veh/h)	455	751	-	-	1270	-
HCM Lane V/C Ratio	0.122	0.016	-	-	0.004	-
HCM Control Delay (s)	14	9.9	-	-	7.8	-
HCM Lane LOS	B	A	-	-	A	-
HCM 95th %tile Q(veh)	0.4	0	-	-	0	-

Intersection	
Int Delay, s/veh	4.9

Movement	EBT	EBR	WBT	WBR	NBT	NBR
Traffic Vol, veh/h	606	45	3	348	131	52
Future Vol, veh/h	606	45	3	348	131	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	250	-	150	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	659	49	3	378	142	57

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	708	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	891	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	891	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

App/Cash	EB	WB	NB
HCM Control Delay, s	0	0.1	31.6
HCM LOS			D

Minor Lane/Major Mvmt	NBL1	NBL2	EBT	EBR	WBL1	WBL2
Capacity (veh/h)	244	449	-	-	891	-
HCM Lane V/C Ratio	0.584	0.126	-	-	0.004	-
HCM Control Delay (s)	38.5	14.2	-	-	9.1	-
HCM Lane LOS	E	B	-	-	A	-
HCM 95th %tile Q(veh)	3.3	0.4	-	-	0	-

Intersection	
Int Delay, s/veh	1.4

Movement	EBT	EBP	WBL	WBT	NB1	NBR
Traffic Vol, veh/h	270	8	5	269	51	11
Future Vol, veh/h	270	8	5	269	51	11
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	250	-	150	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	293	9	5	292	55	12

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	302	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	1259	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	1259	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.1	13.2
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	NBLn2	EBT	EBP	WBL	WBT
Capacity (veh/h)	461	741	-	-	1259	-
HCM Lane V/C Ratio	0.12	0.016	-	-	0.004	-
HCM Control Delay (s)	13.9	9.9	-	-	7.9	-
HCM Lane LOS	B	A	-	-	A	-
HCM 95th %tile Q(veh)	0.4	0	-	-	0	-

Intersection	
Int Delay, s/veh	5.3

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Traffic Vol, veh/h	626	45	3	369	131	52
Future Vol, veh/h	626	45	3	369	131	52
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	250	-	150	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	680	49	3	401	142	57

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	729	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	875	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	875	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	0.1	35
HCM LOS			E

Minor Lane / Major Mvmt	NBLn1	NBLn2	EBT	EBR	WBL	WBT
Capacity (veh/h)	230	436	-	-	875	-
HCM Lane V/C Ratio	0.619	0.13	-	-	0.004	-
HCM Control Delay (s)	43.1	14.5	-	-	9.1	-
HCM Lane LOS	E	B	-	-	A	-
HCM 95th %tile Q(veh)	3.7	0.4	-	-	0	-

Intersection

Int Delay, s/veh 1.8

Movement	EBT	EBR	WBL	WBT	NBL	NEB
Traffic Vol, veh/h	187	85	65	271	21	34
Future Vol, veh/h	187	85	65	271	21	34
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	120	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	203	92	71	295	23	37

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	296	685
Stage 1	-	-	249
Stage 2	-	-	436
Critical Hdwy	-	4.12	6.42
Critical Hdwy Stg 1	-	-	5.42
Critical Hdwy Stg 2	-	-	5.42
Follow-up Hdwy	-	2.218	3.518
Pot Cap-1 Maneuver	-	1265	414
Stage 1	-	-	792
Stage 2	-	-	652
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	1265	391
Mov Cap-2 Maneuver	-	-	391
Stage 1	-	-	792
Stage 2	-	-	615

Approach	EB	WB	NE
HCM Control Delay, s	0	1.6	12.1
HCM LOS			B

Minor Lane/Major Mvmt	NBLM	EST	EBR	WBL	WBT
Capacity (veh/h)	568	-	-	1265	-
HCM Lane V/C Ratio	0.105	-	-	0.056	-
HCM Control Delay (s)	12.1	-	-	8	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.4	-	-	0.2	-

Intersection	
Int Delay, s/veh	3.5

Movement	EBT	EBR	WBL	WBT	NBL	NBH
Traffic Vol, veh/h	429	229	106	304	46	83
Future Vol, veh/h	429	229	106	304	46	83
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	120	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	466	249	115	330	50	90

Major/Minor	Major 1	Major 2	Minor 1	Minor 2
Conflicting Flow All	0	0	715	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	885	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	885	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	2.5	24.8
HCM LOS			C

Minor Lane/Major Mvmt	NBL	EBT	EBR	WBL	WBT
Capacity (veh/h)	319	-	-	885	-
HCM Lane V/C Ratio	0.44	-	-	0.13	-
HCM Control Delay (s)	24.8	-	-	9.7	-
HCM Lane LOS	C	-	-	A	-
HCM 95th %tile Q(veh)	2.1	-	-	0.4	-

Intersection	
Int Delay, s/veh	1.8

Movement	EBL	EBR	WBL	WBT	NBL	NBR
Traffic Vol, veh/h	196	85	65	280	21	34
Future Vol, veh/h	196	85	65	280	21	34
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	120	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	213	92	71	304	23	37

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	705
Stage 1	-	-	259
Stage 2	-	-	446
Critical Hdwy	-	4.12	6.42
Critical Hdwy Stg 1	-	-	5.42
Critical Hdwy Stg 2	-	-	5.42
Follow-up Hdwy	-	2.218	3.518
Pot Cap-1 Maneuver	-	1256	403
Stage 1	-	-	784
Stage 2	-	-	645
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	1256	380
Mov Cap-2 Maneuver	-	-	380
Stage 1	-	-	784
Stage 2	-	-	609

Approach	EB	WB	NB
HCM Control Delay, s	0	1.5	12.3
HCM LOS			B

Min. Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	556	-	-	1256	-
HCM Lane V/C Ratio	0.108	-	-	0.056	-
HCM Control Delay (s)	12.3	-	-	8	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.4	-	-	0.2	-

Intersection	
Int Delay, s/veh	3.6

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Traffic Vol, veh/h	449	229	106	325	46	83
Future Vol, veh/h	449	229	106	325	46	83
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	120	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	488	249	115	353	50	90

Major/Mirror	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	737	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	869	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	869	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	2.4	26.8
HCM LOS			D

Minor Lane/Major Mvmt	NBL	EBT	EBR	WBL	WBT
Capacity (veh/h)	302	-	-	869	-
HCM Lane V/C Ratio	0.464	-	-	0.133	-
HCM Control Delay (s)	26.8	-	-	9.8	-
HCM Lane LOS	D	-	-	A	-
HCM 95th %tile Q(veh)	2.3	-	-	0.5	-

Intersection	
Int Delay, s/veh	3.7

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Traffic Vol, veh/h	219	1	266	336	0	126
Future Vol, veh/h	219	1	266	336	0	126
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	238	1	289	365	0	137

Major/Minor	Major1	Major2	Minor1
Conflicting Flow All	0	0	239
Stage 1	-	-	239
Stage 2	-	-	943
Critical Hdwy	-	4.12	6.42
Critical Hdwy Stg 1	-	-	5.42
Critical Hdwy Stg 2	-	-	5.42
Follow-up Hdwy	-	2.218	3.518
Pot Cap-1 Maneuver	-	1328	210
Stage 1	-	-	801
Stage 2	-	-	379
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	1328	153
Mov Cap-2 Maneuver	-	-	153
Stage 1	-	-	801
Stage 2	-	-	276

Approach	EB	WB	NB
HCM Control Delay, s	0	3.7	10.4
HCM LOS			B

Minor Lane/Major Mvmt	NBL	EBT	EBR	WBL	WBT
Capacity (veh/h)	800	-	-	1328	-
HCM Lane V/C Ratio	0.171	-	-	0.218	-
HCM Control Delay (s)	10.4	-	-	8.5	0
HCM Lane LOS	B	-	-	A	A
HCM 95th %tile Q(veh)	0.6	-	-	0.8	-

Intersection	
Int Delay, s/veh	6

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Traffic Vol, veh/h	504	8	190	406	5	278
Future Vol, veh/h	504	8	190	406	5	278
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	548	9	207	441	5	302

Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	557	0	1406	552
Stage 1	-	-	-	-	552	-
Stage 2	-	-	-	-	854	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1014	-	153	533
Stage 1	-	-	-	-	577	-
Stage 2	-	-	-	-	417	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1014	-	112	533
Mov Cap-2 Maneuver	-	-	-	-	112	-
Stage 1	-	-	-	-	577	-
Stage 2	-	-	-	-	304	-

Approach	EB	WB	NB
HCM Control Delay, s	0	3	23
HCM LOS			C

Minor Lane/Major Mvmt	NBL	EBT	EBR	WBL	WBT
Capacity (veh/h)	500	-	-	1014	-
HCM Lane V/C Ratio	0.615	-	-	0.204	-
HCM Control Delay (s)	23	-	-	9.5	0
HCM Lane LOS	C	-	-	A	A
HCM 95th %tile Q(veh)	4.1	-	-	0.8	-

Intersection	
Int Delay, s/veh	3.7

Movement	EBT	EBF	WBT	WBF	NBT	NBF
Traffic Vol, veh/h	228	1	266	345	0	126
Future Vol, veh/h	228	1	266	345	0	126
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	248	1	289	375	0	137

Major/Minor	Major1	Major2	Minor
Conflicting Flow All	0	0	1201
Stage 1	-	-	248
Stage 2	-	-	953
Critical Hdwy	-	4.12	6.42
Critical Hdwy Stg 1	-	-	5.42
Critical Hdwy Stg 2	-	-	5.42
Follow-up Hdwy	-	2.218	3.518
Pot Cap-1 Maneuver	-	1317	204
Stage 1	-	-	793
Stage 2	-	-	375
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	1317	147
Mov Cap-2 Maneuver	-	-	147
Stage 1	-	-	793
Stage 2	-	-	271

Approach	EB	WB	NB
HCM Control Delay, s	0	3.7	10.5
HCM LOS			B

Minor Lane/Major Mvmt	NBL1	EBT	EBR	WBL	WBT
Capacity (veh/h)	791	-	-	1317	-
HCM Lane V/C Ratio	0.173	-	-	0.22	-
HCM Control Delay (s)	10.5	-	-	8.5	0
HCM Lane LOS	B	-	-	A	A
HCM 95th %tile Q(veh)	0.6	-	-	0.8	-

Intersection 1	
Int Delay, s/veh	6.1

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Traffic Vol, veh/h	524	8	190	427	5	278
Future Vol, veh/h	524	8	190	427	5	278
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	570	9	207	464	5	302

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	578	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	2.218	-
Pot Cap-1 Maneuver	-	-	996	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	996	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s	0	2.9	24.5
HCM LOS			C

Minor Lane/Major Mvmt	NBLn	EBT	EBR	WBL	WBT
Capacity (veh/h)	484	-	-	996	-
HCM Lane V/C Ratio	0.636	-	-	0.207	-
HCM Control Delay (s)	24.5	-	-	9.6	0
HCM Lane LOS	C	-	-	A	A
HCM 95th %tile Q(veh)	4.4	-	-	0.8	-



CALIFORNIA WATER SERVICE

Visalia District 216 North Valley Oaks Drive
Visalia, CA 93292 Tel: (559) 624-1600

May 26, 2016

City of Visalia
Planning Division
315 E Acequia Ave
Visalia, CA 93291

Will Serve Letter
Tentative Parcel Map No, 2016-06, APN 126-062-095 & 126-730-039
Developer: Lane Engineers

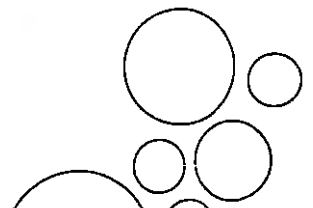
Gentlemen:

As a regulated utility, California Water Service Company Visalia district ("Cal Water") has an obligation to provide water service in accordance with the rules and regulations of the California Public Utility Commission (CPUC). Assuming you receive all required permits from City of Visalia, Cal Water will provide water service to the above referenced project. Cal Water agrees to operate the water system and provide service in accordance with the rules and regulations of the California Public Utilities Commission (CPUC) and the company's approved tariffs on file with the CPUC. This will serve letter shall remain valid for **two years** from the date of this letter. If construction of the project has not commenced within this **two year** time frame, Cal Water will be under no further obligation to serve the project unless the developer receives an updated letter from Cal Water reconfirming our commitment to serve the above mentioned project. Additionally, Cal Water reserves the right to rescind this letter at any time in the event its water supply is severely reduced by legislative, regulatory or environmental actions.

Cal Water will provide such potable¹ water at such pressure as may be available from time to time as a result of its normal operations per the company's tariffs on file with the CPUC. Installation of facilities through developer funding shall be made in accordance with the current rules and regulations of the CPUC including, among others, Tariff Rules 15 and 16 and General Order 103-A. In order for us to provide adequate water for domestic use as well as fire service protection, it may be necessary for the developer to fund the cost of special facilities, such as, but not limited to, booster pumps, storage tanks and/or water wells,² in addition to the cost of mains and services. Cal Water will provide more specific information regarding special facilities and fees after you provide us with your improvement plans, fire department requirements, and engineering fees for this project.

¹ This portion of the letter to be modified accordingly in the event the development for which this letter is being generated is to be served with potable and non potable water.

² For the districts that collect facility fees on a per lot basis, delete the reference to wells as a special facility here and add in the following sentence, "Developer will also be required to contribute towards Cal Water's water supply by paying facilities fees on a per lot basis as described in Rule 15"





CALIFORNIA WATER SERVICE

This letter shall at all times be subject to such changes or modifications by the CPUC as said Commission may, from time to time, require in the exercise of its jurisdiction.

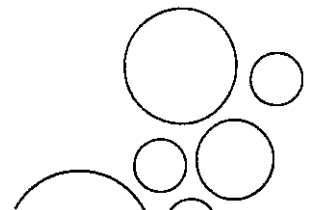
If you have any questions regarding the above, please call me at (559) 624-1600.

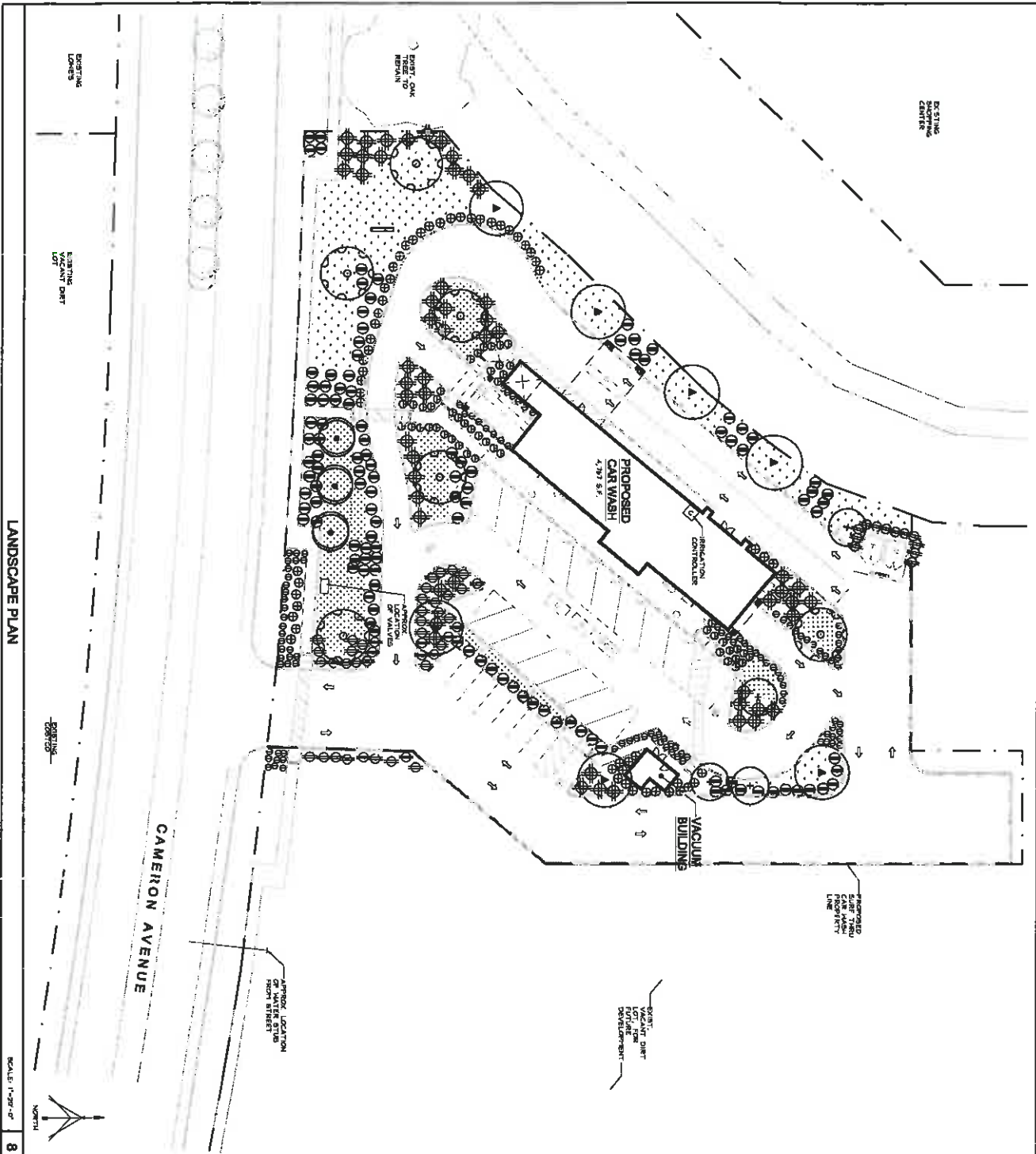
Sincerely,

A handwritten signature in black ink, appearing to read "Eric Charles", written over a faint rectangular box.

Eric Charles
Acting Assistant District Manager

cc: Ting He – Cal Water Engineering Dept
File





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SURF THRU CAR WASH
 N.W.C. OF CAMERON AVENUE &
 STONEBROOK STREET
 VISALIA, CALIFORNIA
LANDSCAPE PLAN

VAI

(STAMP IS VALID UNLESS NET-SIGNED)

Vermeltoot Architects, Inc.
 Architecture and Planning
 8525 North Cedar Avenue Suite 106
 Fresno, California 93720
 Office: 559.432.6744
 Fax: 559.432.6715

CONDITIONAL USE PERMIT

DATE	REVISION

L100

DATE: 01-15-10
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 PROJECT NO: [Number]
 SHEET NO: [Number]

INITIAL STUDY

I. GENERAL

A. Project Description:

Conditional Use Permit No. 2016-09: A request by Vermeltoort Architects Inc. to construct a new 4,767 square foot drive-thru carwash with an attached pay station, vacuum canopy area and a detached 245 square foot vacuum equipment building on 1.51-acres of an overall 17.01-acre site. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area.

Tentative Parcel Map No. 2016-06: A request by Lane Engineers Inc., to subdivide 17.01 acres into four parcels. The overall 17.01-acre site is defined as Phase 2E of the Packwood Shopping Center Specific Plan area.

B. Identification of the Environmental Setting:

The project site is located on the north side of West Cameron Avenue between South Mooney Boulevard and South Stonebrook Street (APN: 126-062-095 & 126-730-039). The site is bounded by a collector roadway to the south (Cameron Avenue) and Packwood Creek to the north and west and vacant property to the east. The project site is currently vacant and has been fallow for the past 10 years.

The surrounding uses, Zoning, and General Plan are as follows:

	General Plan (2014 Land Use)	Zoning (1993)	Existing uses
North:	Regional Commercial	P-C-R (Planned Regional Retail Commercial)	Packwood Creek and Packwood Creek Trail
South:	Cameron Avenue / Regional Commercial	P-C-R (Planned Regional Retail Commercial)	Cameron Avenue / Packwood Creek Shopping Center (Lowe's) & Costco
East:	Regional Commercial	P-C-R (Planned Regional Retail Commercial)	Vacant / Phase 2E of the South Packwood Creek Specific Plan
West:	Public Trail / Conservation	Conservation	Packwood Creek and Packwood Creek Trail

Fire and police protection services, street maintenance of public streets, refuse collection, and wastewater treatment will be provided by the City of Visalia upon the development of the area.

C. Plans and Policies: The General Plan Land Use Diagram, adopted October 14, 2014, designates the site as Regional Retail. The Zoning Map, adopted in 1993, designates the site as Planned Regional Retail Commercial (P-C-R). The proposed project is consistent with the Land Use Element of the General Plan. Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east. The City's General Plan Land Use Map designates the site as Regional Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.

II. ENVIRONMENTAL IMPACTS

No significant adverse environmental impacts have been identified for this project. The City of Visalia Land Use Element and Zoning Ordinance contain policies and regulations that are designed to mitigate impacts to a level of non-significance.

III. MITIGATION MEASURES

There are no mitigation measures for this project. The City of Visalia Zoning Ordinance contains guidelines, criteria, and requirements for the mitigation of potential impacts related to light/glare, visibility screening, noise, and traffic/parking to eliminate and/or reduce potential impacts to a level of non-significance.

IV. PROJECT COMPATIBILITY WITH EXISTING ZONES AND PLANS

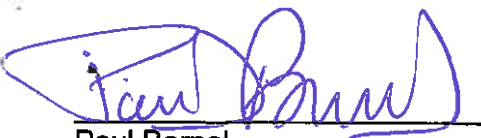
The project is compatible with the General Plan as the project relates to surrounding properties.

V. SUPPORTING DOCUMENTATION

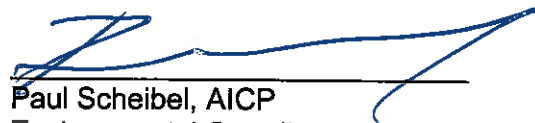
The following documents are hereby incorporated into this Negative Declaration and Initial Study by reference:

- Visalia General Plan Update. Dyett & Bhatia, October 2014.
- Visalia City Council Resolution No. 2014-38 (Certifying the Visalia General Plan Update), passed and adopted October 14, 2014.
- Visalia General Plan Update Final Environmental Impact Report (SCH No. 2010041078). Dyett & Bhatia, June 2014.
- Visalia General Plan Update Draft Environmental Impact Report (SCH No. 2010041078). Dyett & Bhatia, March 2014.
- Visalia City Council Resolution No. 2014-37 (Certifying the EIR for the Visalia General Plan Update), passed and adopted October 14, 2014.
- Visalia Municipal Code, including Title 17 (Zoning Ordinance).
- California Environmental Quality Act Guidelines.
- City of Visalia, California, Climate Action Plan, Draft Final. Strategic Energy Innovations, December 2013.
- Visalia City Council Resolution No. 2014-36 (Certifying the Visalia Climate Action Plan), passed and adopted October 14, 2014.
- City of Visalia Storm Water Master Plan. Boyle Engineering Corporation, September 1994.
- City of Visalia Sanitary Sewer Master Plan. City of Visalia, 1994.
- Traffic Impact Analysis for Packwood Creek Phase 2, Associated Transportation Engineers, April 8, 2016

VI. NAME OF PERSON WHO PREPARED INITIAL STUDY



Paul Bernal
Principal Planner



Paul Scheibel, AICP
Environmental Coordinator

**INITIAL STUDY
ENVIRONMENTAL CHECKLIST**

Name of Proposal	Conditional Use Permit No. 2016-09 & Tentative Parcel Map No. 2016-06		
NAME OF PROPONENT:	Vermeltoort Architects, Inc.	NAME OF AGENT:	Vermeltoort Architects, Inc.
Address of Proponent:	8525 N. Cedar Ave., Suite 106 Fresno, CA 93720	Address of Agent:	8525 N. Cedar Ave., Suite 106 Fresno, CA 93720
Telephone Number:	(559) 432-6744	Telephone Number:	(559) 432-6744
Date of Review	June 2, 2016	Lead Agency:	City of Visalia

The following checklist is used to determine if the proposed project could potentially have a significant effect on the environment. Explanations and information regarding each question follow the checklist.

1 = No Impact 2 = Less Than Significant Impact
3 = Less Than Significant Impact with Mitigation Incorporated 4 = Potentially Significant Impact

I. AESTHETICS

Would the project:

- 2 a) Have a substantial adverse effect on a scenic vista?
- 1 b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?
- 2 c) Substantially degrade the existing visual character or quality of the site and its surroundings?
- 2 d) Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

II. AGRICULTURAL RESOURCES

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

- 2 a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency to non-agricultural use?
- 1 b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?
- 1 c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?
- 1 d) Result in the loss of forest land or conversion of forest land to non-forest use?
- 1 e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to nonagricultural use?

III. AIR QUALITY

Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

- 2 a) Conflict with or obstruct implementation of the applicable air quality plan?
- 2 b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?
- 2 c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?
- 1 d) Expose sensitive receptors to substantial pollutant concentrations?
- 1 e) Create objectionable odors affecting a substantial number of people?

IV. BIOLOGICAL RESOURCES

Would the project:

- 2 a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 2 b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?
- 2 c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?
- 2 d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

- 1 e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?
- 1 f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

V CULTURAL RESOURCES

Would the project:

- 1 a) Cause a substantial adverse change in the significance of a historical resource as defined in Public Resources Code Section 15064.5?
- 1 b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to Public Resources Code Section 15064.5?
- 1 c) Directly or indirectly destroy a unique paleontological resource or site, or unique geologic feature?
- 1 d) Disturb any human remains, including those interred outside of formal cemeteries?

VI GEOLOGY AND SOILS

Would the project:

- a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:
 - 1 i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?
 - 1 ii) Strong seismic ground shaking?
 - 1 iii) Seismic-related ground failure, including liquefaction?
 - 1 iv) Landslides?
- 1 b) Result in substantial soil erosion or loss of topsoil?
- 1 c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?
- 1 d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?
- 1 e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

VII GREENHOUSE GAS EMISSIONS

Would the project:

- 2 a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?
- 2 b) Conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

VIII HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- 1 a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

- 1 b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?
- 1 c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?
- 1 d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?
- 1 f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?
- 1 g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?
- 1 h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

IX HYDROLOGY AND WATER QUALITY

Would the project:

- 2 a) Violate any water quality standards of waste discharge requirements?
- 2 b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?
- 2 c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?
- 2 d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?
- 2 e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?
- 2 f) Otherwise substantially degrade water quality?
- 2 g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?
- 2 h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?
- 2 i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?
- 1 j) Inundation by seiche, tsunami, or mudflow?

X LAND USE AND PLANNING

Would the project:

- 1 a) Physically divide an established community?
- 1 b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?
- 1 c) Conflict with any applicable habitat conservation plan or natural community conservation plan?

XI MINERAL RESOURCES

Would the project:

- 1 a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?
- 1 b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

XII NOISE

Would the project:

- 1 a) Cause exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?
- 1 b) Cause exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?
- 1 c) Cause a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?
- 1 d) Cause a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?
- 1 f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

XIII POPULATION AND HOUSING

Would the project:

- 2 a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?
- 1 b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?
- 1 c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

XIV PUBLIC SERVICES

Would the project:

- 1 a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically

altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

- 1 i) Fire protection?
- 1 ii) Police protection?
- 1 iii) Schools?
- 1 iv) Parks?
- 1 v) Other public facilities?

XV RECREATION

Would the project:

- 1 a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?
- 1 b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

XVI TRANSPORTATION / TRAFFIC

Would the project:

- 1 a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?
- 1 b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?
- 1 c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?
- 1 d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?
- 1 e) Result in inadequate emergency access?
- 1 f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?

XVII UTILITIES AND SERVICE SYSTEMS

Would the project:

- 1 a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?
- 2 b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 2 c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?
- 1 d) Have sufficient water supplies available to service the project from existing entitlements and resources, or are new or expanded entitlements needed?

- 1 e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?
- 1 f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?
- 1 g) Comply with federal, state, and local statutes and regulations related to solid waste?

XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

Would the project:

- 2 a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?
- 2 b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?
- 2 c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Note: Authority cited: Sections 21083 and 21083.05, Public Resources Code. Reference: Section 65088.4, Gov. Code; Sections 21080(c), 21080.1, 21080.3, 21083, 21083.05, 21083.3, 21093, 21094, 21095, and 21151, Public Resources Code; *Sundstrom v. County of Mendocino*, (1988) 202 Cal.App.3d 296; *Leonoff v. Monterey Board of Supervisors*, (1990) 222 Cal.App.3d 1337; *Eureka Citizens for Responsible Govt. v. City of Eureka* (2007) 147 Cal.App.4th 357; *Protect the Historic Amador Waterways v. Amador Water Agency* (2004) 116 Cal.App.4th at 1109; *San Franciscans Upholding the Downtown Plan v. City and County of San Francisco* (2002) 102 Cal.App.4th 656.

Revised 2009

DISCUSSION OF ENVIRONMENTAL EVALUATION

I. AESTHETICS

- a. This project will not adversely affect the view of any scenic vistas. The Sierra Nevada mountain range may be considered a scenic vista, but views of the range will not be adversely impacted or significantly by the project. Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east. The City's General Plan Land Use Map designates the site as Reginal Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.
- b. There are no scenic resources on the site.
- c. The City has development standards related to landscaping and other amenities that will ensure that the visual character of the area is not degraded upon any future development. The proposed project in itself will not change the visual character or quality of the site.
- d. The project will facilitate for the future development of reginal retail commercial property. New sources of light would be created upon the site's future development that is typical of urban development. The City's existing development standards require that light be directed and/or shielded so it does not fall upon adjacent properties upon the site's future development.

II. AGRICULTURAL RESOURCES

- a. The project is located on property that is identified as Farmland of Local Importance on maps prepared by the California Resources, and will involve the conversion of the property to non-agricultural use.

The Visalia General Plan Update Environmental Impact Report (EIR) has already considered the environmental impacts of the conversion of properties within the Planning Area, which includes the subject property, into non-agriculture uses. Overall, the General Plan results in the conversion of over 14,000 acres of Important Farmland to urban uses, which is considered significant and unavoidable. Aside from preventing development altogether the conversion of Important Farmland to urban uses cannot be directly mitigated, through the use of agricultural conservation easements or by other means. However, the General Plan contains multiple polices that together work to limit conversion only to the extent needed to accommodate long-term growth. The General Plan policies identified under Impact 3.5-1 of the EIR serve as the mitigation which assists in reducing the severity of the impact to the extent possible while still achieving the General Plan's goals of accommodating a certain amount of growth to occur within the Planning Area. These policies include the implementation of a three-tier growth

boundary system that assists in protecting open space around the City fringe and maintaining compact development within the City limits.

Because there is still a significant impact to loss of agricultural resources after conversion of properties within the General Plan Planning Area to non-agricultural uses, a Statement of Overriding Considerations was previously adopted with the Visalia General Plan Update EIR.

- b. The project site is zoned Planned Reginal Retail Commercial which is consistent with the land use designation of Regional Commercial for this property. The project is bordered by urban development or non-producing vacant land on one or more sides. There are no known Williamson Act contracts on any areas within the subject property.
- c. There is no forest land or timberland currently located on the site, nor does the site conflict with a zoning for forest land, timberland, or timberland zoned Timberland Production.
- d. There is no forest or timberland currently located on the site.
- e. The project will involve changes that would promote or result in the conversion of farmland to non-agriculture use. The subject property is currently designated for an urban rather than agricultural land use. Properties that are vacant may develop in a way that is consistent with their zoning and land use designated at any time. The adopted Visalia General Plan's implementation of a three-tier growth boundary system further assists in protecting open space around the City fringe to ensure that premature conversion of farmland to non-agricultural uses does not occur. The proposed carwash facility and development of 1.51-acres will result in conversion of farmland of local importance to a non-agricultural use. However, the 25-acre parcel has not been actively farmed and/or has not been in active agricultural production for the past 10 years. The City's General Plan designates this property for urban development by designating the site for Regional Commercial. Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east. The City's General Plan Land Use Map designates the site as Reginal Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.

III. AIR QUALITY

- a. The project site is located in an area that is under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The project in itself does not disrupt implementation of the San Joaquin Regional Air Quality Management Plan, and will therefore be a less than significant impact.
- b. Future development of the site under the Visalia General Plan will result in emissions that will exceed thresholds established by the SJVAPCD for PM10 and PM2.5. The development of this property may contribute to a net increase of criteria pollutants and will therefore contribute to exceeding the thresholds. Also the project could result in short-term air quality impacts related to dust generation and exhaust due to construction and grading activities. This site was evaluated in the Visalia General Plan Update EIR for conversion into urban development. Development under the General Plan will result in increases of construction and operation-related criteria pollutant impacts, which are considered significant and unavoidable. General Plan policies identified under Impacts 3.3-1 and 3.3-2 serve as the mitigation which assists in reducing the severity of the impact to the extent possible while still achieving the General Plan's goals of accommodating a certain amount of growth to occur within the Planning Area.

Development of this site is required to adhere to requirements administered by the SJVAPCD to reduce emissions to a level of compliance consistent with the District's grading regulations. Compliance with the SJVAPCD's rules and regulations will reduce potential impacts associated with air quality standard violations to a less than significant level.

In addition, development of the project site may be subject to the SJVAPCD Indirect Source Review (Rule 9510) procedures that became effective on March 1, 2006. The Applicant will be required to obtain permits demonstrating compliance with Rule 9510, or payment of mitigation fees to the SJVAPCD, when warranted.

- c. Tulare County is designated non-attainment for certain federal ozone and state ozone levels. Development of the project site, may will result in a net increase of criteria pollutants. This site was evaluated in the Visalia General Plan Update EIR for conversion into urban development. Development under the General Plan will result in increases of construction and operation-related criteria pollutant impacts, which are considered significant and unavoidable. General Plan policies identified under Impacts 3.3-1, 3.3-2, and 3.3-3 serve as the mitigation which assists in reducing the severity of the impact to the extent possible while still achieving the General Plan's goals of accommodating a certain amount of growth to occur within the Planning Area.

Development of the project site may be required to adhere to requirements administered by the SJVAPCD to reduce emissions to a level of compliance consistent with the District's grading regulations. Compliance with the SJVAPCD's rules and regulations will reduce potential impacts associated with air quality standard violations to a less than significant level.

In addition, development of the project site may be subject to the SJVAPCD Indirect Source Review (Rule 9510) procedures that became effective on March 1, 2006. The Applicant would be required to obtain permits demonstrating compliance with Rule 9510, or payment of mitigation fees to the SJVAPCD, when warranted.

- d. Residences located near the proposed project may be exposed to pollutant concentrations due to future construction activities. The use of construction equipment will be temporary and is subject to SJVAPCD rules and regulations. The impact is considered as less than significant.
- e. The proposed project will not involve the generation of objectionable odors that would affect a substantial number of people.

IV. BIOLOGICAL RESOURCES

- a. The site has no known species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. The project would therefore not have a substantial adverse effect on a sensitive, candidate, or special species.

In addition, staff had conducted an on-site visit to the site in May 2016 to observe biological conditions and did not observe any evidence or symptoms that would suggest the presence of a sensitive, candidate, or special species.

City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR). The EIR concluded that certain special-status species or their habitats may be directly or indirectly affected by future development within the General Plan Planning Area. This may be through the removal of or disturbance to habitat. Such effects would be considered significant. However, the General Plan contains multiple polices, identified under Impact 3.8-1 of the EIR, that together work to reduce the potential for impacts on special-status species likely to occur in the Planning Area. With implementation of these polices, impacts on special-status species will be less than significant.

- b. The project is located within or adjacent to an identified sensitive riparian habitat or other natural community. The project will comply with the 50-foot setback requirement from the top of the hinge-point of Packwood Creek as identified in the Conversation Land Use Policy.

City-wide biological resources were evaluated in the Visalia General Plan Update Environmental impact Report (EIR). The EIR concluded that certain sensitive natural communities may be directly or indirectly affected by future development within the General Plan Planning Area, particularly valley oak woodlands and valley oak riparian woodlands. Such effects would be considered significant. However, the General Plan contains multiple polices, identified under Impact 3.8-2 of the EIR, that together work to reduce the potential for impacts on woodlands located within in the Planning Area. With implementation of these policies, impacts on woodlands will be less than significant.

- c. The project is not located within or adjacent to federally protected wetlands as defined by Section 404 of the Clean Water Act.

City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR). The EIR concluded that certain protected wetlands and other waters may be directly or indirectly affected by future development within the General Plan Planning Area. Such effects would be considered significant. However, the General Plan contains multiple policies, identified under Impact 3.8-3 of the EIR, that together work to reduce the potential for impacts on wetlands and other waters located within in the Planning Area. With implementation of these policies, impacts on wetlands will be less than significant.

- d. City-wide biological resources were evaluated in the Visalia General Plan Update Environmental Impact Report (EIR). The EIR concluded that the movement of wildlife species may be directly or indirectly affected by future development within the General Plan Planning. Such effects would be considered significant. However, the General Plan contains multiple policies, identified under Impact 3.8-4 of the EIR, that together work to reduce the potential for impacts on wildlife movement corridors located within in the Planning Area. With implementation of these policies, impacts on wildlife movement corridors will be less than significant.
- e. The City has a municipal ordinance in place to protect valley oak trees. All existing valley oak trees on the project site will be under the jurisdiction of this ordinance. Any oak trees to be removed from the site are subject to the jurisdiction of the municipal ordinance.

There are no Valley Oak trees onsite but there is a large native Valley Oak located to the west of the site within the City's Packwood Creek Trail area. This project will not impact this Valley Oak.

- f. There are no local or regional habitat conservation plans for the area.

V. CULTURAL RESOURCES

- a. There are no known historical resources located within the project area. If some potentially historical or cultural resource is unearthed during development all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- b. There are no known archaeological resources located within the project area. If some archaeological resource is unearthed during development all work should cease until a qualified professional archaeologist can evaluate the finding and make necessary mitigation recommendations.
- c. There are no known unique paleontological resources or geologic features located within the project area.
- d. There are no known human remains buried in the project vicinity. If human remains are unearthed during development all work should cease until the proper authorities are notified and a qualified professional archaeologist can evaluate the finding and make any necessary mitigation recommendations.

VI. GEOLOGY AND SOILS

- a. The State Geologist has not issued an Alquist-Priolo Earthquake Fault Map for Tulare County. The project area is not located on or near any known earthquake fault lines. Therefore, the project will not expose people or structures to potential substantial adverse impacts involving earthquakes.
- b. Development of the site will require movement of topsoil. Existing City Engineering Division standards require that a grading and drainage plan be submitted for review to the City to ensure that off- and on-site improvements will be designed to meet City standards.
- c. The project area is relatively flat and the underlying soil is not known to be unstable. Soils in the Visalia area have few limitations with regard to development. Due to low clay content and limited topographic relief, soils in the Visalia area have low expansion characteristics.
- d. Due to low clay content, soils in the Visalia area have an expansion index of 0-20, which is defined as very low potential expansion.
- e. The project does not involve the use of septic tanks or alternative waste water disposal systems since sanitary sewer lines are used for the disposal of waste water at this location.

VII. GREENHOUSE GAS EMISSIONS

- a. The project is expected to generate Greenhouse Gas (GHG) emissions in the short-term as a result of the construction of industrial development and long-term as a result of day-to-day operation of the future development.

The City has prepared and adopted a Climate Action Plan (CAP), which includes a baseline GHG emissions inventories, reduction measures, and reduction targets consistent with local and State goals. The CAP was prepared concurrently with the proposed General Plan and its impacts are also evaluated in the Visalia General Plan Update EIR.

The Visalia General Plan and the CAP both include policies that aim to reduce the level of GHG emissions emitted in association with buildout conditions under the General Plan. Implementation of the General Plan and CAP policies will result in fewer emissions than would be associated with a continuation of baseline conditions. Thus, the impact to GHG emissions will be less than significant.

- b. The State of California has enacted the Global Warming Solutions Act of 2006 (AB 32), which included provisions for reducing the GHG emission levels to 1990 "baseline" levels by 2020.

VIII. HAZARDS AND HAZARDOUS MATERIALS

- a. No hazardous materials are anticipated with the project.
- b. Construction activities associated with the development of the project may include maintenance of on-site construction equipment, which could lead to minor fuel and oil spills. The use and handling of any hazardous materials during construction activities would occur in accordance with applicable federal, state, regional, and local laws. Therefore, impacts are considered to be less than significant.

- c. There is one school located within one-half mile from the project site (Cottonwood Creek Elementary Middle School). However, there is no reasonably foreseeable condition or incident involving the project that could affect existing or proposed school sites or areas within one-half mile of school sites.
- d. The project area does not include any sites listed as hazardous materials sites pursuant to Government Code Section 65692.5.
- e. The City's adopted Airport Master Plan shows the project area is located outside of all Airport Zones. There are no restrictions for the proposed project related to Airport Zone requirements.

The project area is not located within 2 miles of a public airport.

- f. The project area is not within the vicinity of any private airstrip.
- g. The project will not interfere with the implementation of any adopted emergency response plan or evacuation plan.
- h. There are no wild lands within or near the project area.

IX. HYDROLOGY AND WATER QUALITY

- a. Development projects associated with buildout under the Visalia General Plan have the potential to result in short term impacts due to erosion and sedimentation during construction activities and long-term impacts through the expansion of impervious surfaces. The City's existing standards will require the project to uphold water quality standards of waste discharge requirements consistent with the requirements of the State Water Resources Control Board's (SWRCB's) General Construction Permit process. This may involve the preparation and implementation of a Storm Water Pollution Prevention Plan (SWPPP) and/or the use of best management practices. The project will be required to meet municipal storm water requirements set by the SWRCB.

Furthermore, the Visalia General Plan contains multiple polices, identified under Impact 3.6-2 of the EIR, that together work to reduce the potential for impacts to water quality. With implementation of these policies and the existing City standards, impacts to water quality will be less than significant.

- b. The project area overlies the southern portion of the San Joaquin unit of the Central Valley groundwater aquifer. Development of the site will result in an increase of impervious surfaces on the project site, which might affect the amount of precipitation that is recharged to the aquifer.
- c. The project will not result in substantial erosion on- or off-site.
- d. The project will not substantially alter the existing drainage pattern of the site or area, alter the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site.
- e. The project will not create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff.

- f. There are no reasonably foreseeable reasons why the project would result in the degradation of water quality.
- g. The project area is not located within a flood zone.
- h. The project area is not located within a flood zone.
- i. The project would not expose people or structures to risks from failure of levee or dam. The project is located downstream from the Terminus Dam; in the case of dam failure, there will be 4 hours of warning to evacuate the site.
- j. Seiche and tsunami impacts do not occur in the Visalia area. The site is relatively flat, which will contribute to the lack of impacts by mudflow occurrence.

X. LAND USE AND PLANNING

- a. The project will not physically divide an established community. Drive-thru carwash facilities are considered compatible uses in commercial areas where potential impacts can be addressed through the CUP process. The site is located along Cameron Avenue, which is a designated collector status roadway. Regional Retail Commercial land use designations bound this portion of Cameron Avenue roadway segment between Mooney Boulevard to the west and Stonebrook Street to the east. The City's General Plan Land Use Map designates the site as Reginal Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility.
- b. The project does not conflict with any land use plan, policy or regulation of the City of Visalia. The recently adopted General Plan did not rezone or otherwise disrupt residential communities or commercial areas, and provides additional space to accommodate any potentially displaced residents or businesses.

The City's General Plan Land Use Map designates the site as Reginal Retail. Staff believes that the proposed drive-thru carwash is consistent in nature and character with existing and future uses surrounding the project site, subject to the Conditions of Project Approval for this carwash facility. Additionally, the South Packwood Creek Specific Plan provides a number of plans and policies regarding the development of this area. The policies cover issues that include streets and roadways, utility lines, storm drainage, oak tree protection, aesthetics, and parking.

The Specific Plan includes detailed plans and building layouts for Phase I located on the east and west sides of Mooney Boulevard but because of the long-term vision of Phase II (i.e., project site), detailed site plans were not included in the Specific Plan. The South Packwood Creek Specific Plan included a conceptual site plan exhibit for the Phase 2E area (see Exhibit "G") but the plans lack of detail, including potential uses, did not give approval for the depicted development pattern on the conceptual site plan. Rather, the Specific Plan as related to Phase 2E summarizes standards and guidelines for allowed building area, street and roadway improvements (not including driveways and median breaks), infrastructure, setbacks and development standards, signage, and the development of Packwood Creek. It should be noted, Phase 2D is fully entitled based on the approval of CUP

No. 2007-12 (Costco development and future retail pads, see Exhibit "H").

The South Packwood Creek Specific Plan and related Environmental Impact Report were prepared to allow for a total of 1,076,946 square feet of building area between Phases 1 and 2. Based on an actual developed area of Phase 1 (east and west side of Mooney Blvd.), the Phase 2 area has a remaining allotment of 422,756 square feet of building area. As illustrated in Exhibit "H", the development of Phase 2D, including the Costco building, provides for a total of 280,300 square feet of building area in Phase 2D south of Cameron Avenue, which leaves 142,456 square feet of remaining developable commercial space in the Phase 2E north of Cameron Avenue.

The South Packwood Creek Specific Plan projected that the development of the Phase 2 area would be completed over a five to twenty-year period (between 2006 and 2020) dating from when the Plan was authored. The Specific Plan contains analysis of technical studies, which consider long-term impacts resulting from the buildout of Phases 1 and 2. An economic study prepared in 2001 concluded that projected demand for leasable commercial space could accommodate development of 1,016,000 square feet by the year 2010. Thus, Staff finds that the development of the carwash at this time is consistent with the policies of the Specific Plan.

- c. The project does not conflict with any applicable habitat conservation plan or natural community conservation plan as the project site is vacant dirt lot with no significant natural habitat present.

XI. MINERAL RESOURCES

- a. No mineral areas of regional or statewide importance exist within the Visalia area.
- b. There are no mineral resource recovery sites delineated in the Visalia area.

XII. NOISE

- a. The project will facilitate the development of a carwash use. Such development will result in noise generation typical of urban development. There will be noise generated by traffic along designated collector street. The City's standards for setbacks along major streets will reduce noise levels to a level that is less than significant.

Furthermore, the Visalia General Plan contains multiple policies, identified under Impact N-P-3 through N-P-5, that work to reduce the potential for noise impacts to sensitive land uses. With implementation of Noise Impact Policies and existing City Standards, noise impacts to new noise sensitive lands uses would be less than significant.

- b. Ground-borne vibration or ground-borne noise levels may occur as part of construction activities associated with the development of the site. Construction activities will be temporary and will not expose persons to such vibration or noise levels for an extended period of time; thus the impacts will be less than significant. There are no existing uses near the project area that create ground-borne vibration or ground-borne noise levels.
- c. Ambient noise levels will increase beyond current levels as a result of development of the project site, however these levels will be typical of noise levels associated with urban development and not in excess of standards

established in the City of Visalia's General Plan or Noise Ordinance. Noise associated with the establishment of new urban uses was previously evaluated with the General Plan for the conversion of land to urban uses.

Furthermore, the Visalia General Plan contains multiple policies, identified under Impact N-P-3 through N-P-5, that work to reduce the potential for noise impacts to sensitive land uses. With implementation of Noise Impact Policies and existing City Standards, noise impacts to new noise sensitive lands uses would be less than significant.

- d. Noise levels will increase during construction activities; however, there are no construction activities associated with this project.
- e. The project area is not within 2 miles of a public airport. The project will not expose people residing or working in the project area to excessive noise levels.
- f. There is no private airstrip near the project area.

XIII. POPULATION AND HOUSING

- a. The project will not directly induce substantial population growth that is in excess of that planned in the General Plan.
- b. Future development of the site will not displace any housing on the site.
- c. Development of the site will not displace any people on the site.

XIV. PUBLIC SERVICES

- a.
 - i. Current fire protection facilities are located at the Visalia Station 52 and can adequately serve the site without a need for alteration. Impact fees will be paid to mitigate the project's proportionate impact on these facilities.
 - ii. Current police protection facilities can adequately serve the site without a need for alteration. Impact fees will be paid to mitigate the project's proportionate impact on these facilities.
 - iii. The project will not generate new students for which existing schools in the area may accommodate. In addition, to address direct impacts, the future development of the site will be required to pay residential impact fees. These fees are considered to be conclusive mitigation for direct impacts. The project includes residential units that will create a need for park facilities.
 - iv. Other public facilities can adequately serve the site without a need for alteration.

XV. RECREATION

- a. The project will not directly generate new residents.
- b. The proposed project does not include recreational facilities or require the construction or expansion of recreational facilities within the area that might have an adverse physical effect on the environment.

XVI. TRANSPORTATION AND TRAFFIC

- a. Future development of the site and operation of the project site is not anticipated to conflict with applicable plans, ordinances, or policies establishing measures of effectiveness of the City's circulation system. Cameron Avenue bounds the site to the south, which is a designated collector status roadway per the City of Visalia's Circulation Element. Pursuant to the circulation element, collector streets are intended to provide connectivity between local streets and the arterial street system, and provide access to adjacent land uses.

The applicant prepared a Traffic Analysis for this project detailing the traffic generation and traffic patterns for the proposed carwash facility (see Exhibit "I"). The analysis reviewed impacts of the proposed use on the existing intersections and conducted a vehicle queuing analysis for left turn movements and vehicle stacking between the proposed carwash driveway and the Costco driveway. The analysis determined the project would not have significant impacts to the intersections studied as listed on Table 4 of the Traffic Analysis.

Additionally, the left turn lane in Cameron Avenue provides an ample storage length for vehicles waiting to make a left turn into the carwash site. The City's Traffic Engineer concurs with the findings provided in the traffic analysis.

There are approximately 12,419 AADT (Average Annual Daily Trips) along this segment of Cameron Avenue as identified in the City's Circulation Element of the General Plan. Collector streets are designed to accommodate 15,000 AADT's. It should be noted the Cameron / Stonebrook intersection is planned to be signalized at a future date per the City's Capital Improvement Project program. The signalization of this intersection could be expedited if the remaining balance of Phase 2E is developed, any of the abutting commercial or multi-family zoned parcels are developed or if Stonebrook Street is constructed north of Cameron Avenue to Caldwell Avenue.

- b. Development of the site will result in increased traffic in the area, but will not cause a substantial increase in traffic on the city's existing circulation pattern. This site was evaluated in the Visalia General Plan Update Environmental Impact Report (EIR) for urban use.
- c. The project will not result in nor require a need to change air traffic patterns.
- d. There are no planned designs that are considered hazardous.
- e. The project will not result in inadequate emergency access.
- f. The project will not conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities.

XVII. UTILITIES AND SERVICE SYSTEMS

- a. Development of the site will connect and/or extended City sanitary sewer lines, consistent with the City Sewer Master Plan. The Visalia wastewater treatment plant has a current rated capacity of 22 million gallons per day, but currently treats an average daily maximum month flow of 12.5 million gallons per day. With buildout of the project site, the plant has more than sufficient capacity to accommodate impacts associated with the proposed project. The proposed project will therefore not cause significant environmental impacts.
- b. The project will not result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects.
- c. The project site will be accommodated by existing City storm water drainage lines that handle on-site and street runoff. Usage of these lines is consistent with the City Storm Drain Master Plan. These improvements will not cause significant environmental impacts.
- d. California Water Service Company has determined that there are sufficient water supplies to support the site, and that service can be extended to the site.
- e. The City has determined that there is adequate capacity existing to serve site within the City with projected wastewater treatment demands at the City wastewater treatment plant.
- f. Current solid waste disposal facilities can adequately serve the site without a need for alteration.
- g. The project will be able to meet the applicable regulations for solid waste. Removal of debris from construction will be subject to the City's waste disposal requirements.


XVIII. MANDATORY FINDINGS OF SIGNIFICANCE

- a. The project will not affect the habitat of a fish or wildlife species or a plant or animal community. This site was evaluated in the Program EIR (SCH No. 2010041078) for the City of Visalia's General Plan Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- b. This site was evaluated in the Program EIR (SCH No. 2010041078) for the City of Visalia General Plan Update for the area's conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.
- c. This site was evaluated in the Program EIR (SCH No. 2010041078) for the City of Visalia General Plan Update for conversion to urban use. The City adopted mitigation measures for conversion to urban development. Where effects were still determined to be significant a statement of overriding considerations was made.

DETERMINATION OF REQUIRED ENVIRONMENTAL DOCUMENT

On the basis of this initial evaluation:

- I find that the proposed project **COULD NOT** have a significant effect on the environment. **A NEGATIVE DECLARATION WILL BE PREPARED.**
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the mitigation measures described on the attached sheet have been added to the project. **A MITIGATED NEGATIVE DECLARATION WILL BE PREPARED.**
- I find the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- I find that the proposed project **MAY** have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- I find that as a result of the proposed project no new effects could occur, or new mitigation measures would be required that have not been addressed within the scope of the Program Environmental Impact Report (SCH No. 2010041078). The Environmental Impact Report prepared for the City of Visalia General Plan was certified by Resolution No. 2014-37 adopted on October 14, 2014. **THE PROGRAM ENVIRONMENTAL IMPACT REPORT WILL BE UTILIZED.**



Paul Scheibel, AICP
Environmental Coordinator

June 2, 2016
Date

15049



MEETING DATE December 16, 2015
 SITE PLAN NO. 15-127
 PARCEL MAP NO.
 SUBDIVISION
 LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

- RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.
- During site plan design/policy concerns were identified, schedule a meeting with
- Planning Engineering prior to resubmittal plans for Site Plan Review.
- Solid Waste Parks and Recreation Fire Dept.

REVISE AND PROCEED (see below)

- A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.
- Submit plans for a building permit between the hours of 9:00 a.m. and 4:00 p.m., Monday through Friday.
- Your plans must be reviewed by:
- CITY COUNCIL REDEVELOPMENT
- PLANNING COMMISSION PARK/RECREATION
- CUP
- HISTORIC PRESERVATION OTHER

ADDITIONAL COMMENTS _____

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

SITE PLAN REVIEW COMMENTS

Paul Bernal, Planning Division (559) 713-4025

Date: December 16, 2015

SITE PLAN NO: 2015-127 – RESUBMITTAL
PROJECT TITLE: SURF THRU CAR WASH
DESCRIPTION: PROPOSED CAR WASH & VACUUM CANOPIES AND SITE IMPROVEMENTS ON A VACANT DIRT LOT (CR) (X) (A)
APPLICANT: VERMELTFOORT ROBERT
PROP. OWNER: DBO DEVELOPMENT NO 33
LOCATION TITLE: NORTH SIDE OF CAMERON AVE IN PHASE 3 OF PACKWOOD CREEK SHOPPING CENTER
APN TITLE: 126-062-095
GENERAL PLAN: Regional Retail Commercial
EXISTING ZONING: C-R – Regional Retail Commercial

Planning Division Recommendation:

- Revise and Proceed
 Resubmit

Project Requirements

- Conditional Use Permit – for carwash
- Master Site Plan for entire area
- Initial Study (Negative Declaration or Mitigated Negative Declaration)
- Building/Parking Analysis per EIR
- Traffic Study
- Lighting Study
- Additional Information as Needed

PROJECT SPECIFIC INFORMATION: 12/16/2015

1. When filing the CUP application, include the master site plan exhibit detailing the conceptual development of the entire Phase 2B of the Packwood Shopping Center development, and the detailed site plan for the drive-thru carwash facility.
2. Provide elevations and photometric study with the CUP application.
3. The vacuum equipment building shall be architecturally compatible with the primary carwash building.
4. The site plan exhibit shall depict car stacking from the left-turn pocket for east bound vehicles entering the site.
5. General Plan policies OSP-8 through OSP-17 establish a directive to provide a visual orientation and active interface along creeks and that building service areas are setback or screened from the creek.
6. All signs will need to be a part of the sign program for the entire center. The center does not appear to have any individual signs for small-pad non-major tenants.
7. Is a lot line adjustment proposed or is parcel map going to be submitted to create this parcel.
8. This site will be conditioned to provide vehicular cross-access to adjoining parcels / development.
9. The solid waste enclosure shall have vines around the enclosure to limit the exposure of the split face block material from the Packwood Creek trail.
10. Side walk connections are required as a part of this project.
11. Comply with previous Site Plan Review comments.

Previous Comments

PROJECT SPECIFIC INFORMATION: 09/30/2015

1. Staff comments from the previous meetings are still applicable. The applicant is revising their development plan to address staffs comments.

Previous Comments

PROJECT SPECIFIC INFORMATION: 09/16/2015

2. Comply with all comments from August 12, 2015 meeting.
3. ~~The Master CUP site plan was not submitted.~~
4. Staff has several concerns regarding vehicular access points along Cameron Avenue and onsite circulation that facilities the carwash use.
5. The site is Phase 2B of the Packwood Shopping Center development and an emphasis to properly planning access points needs to be resolved given the amount of traffic using Cameron Avenue.

Previous Comments

PROJECT SPECIFIC INFORMATION: 08/12/2015

1. Car Wash uses require a Conditional Use Permit in the CR zoning designation.
2. A Master Plan (Master CUP) is required for the entire area north of Cameron and west of Stonebrook. This area was included in the South Packwood Creek Specific Plan, but was not Master Planned as part of the Master CUP's for the east and west sides south of Cameron Avenue.
3. General Plan policies OSP-8 through OSP-17 establish a directive to provide a visual orientation and active interface along creeks and that building service areas are setback or screened from the creek.
4. The operational statement indicates that there would be approximately 4 employees; staff review of the existing facility at Mooney/Tulare appeared to have more employees.
5. Public stalls, minimum of two, need to be provided in addition to 4-5 for employees and the HC stall for a total of 7 minimum stalls for this use. They will all be required on the car wash site if the adjacent commercial parking area is not developed prior to the car wash.
6. See Engineering comments related to the driveway location.
7. All signs will need to be a part of the sign program for the entire center. The center does not appear to have any individual signs for small-pad non-major tenants.
8. Meet Community Noise Standards.
9. Lot Line Adjustment required for the parcel line running though the site.
10. Connections to the Packwood trail are required for this portion of the center. Exact locations will be reviewed and established though the Master Plan.
11. Show a construction phasing line for this portion of the development, should be included on the Master Plan when it is established.
12. Provide a public pedestrian access to the sidewalk along Cameron Avenue.
13. Landscape and irrigation plans are required as a part of the building permit package.
14. A row of evergreen (low water use) shrubs is required along the car wash access drive facing Cameron Avenue to achieve a 3-foot high visual coverage.
15. ~~Based upon the prominent location of the trash enclosure visible from the right-of-way, the enclosure should incorporate a few minor material and color components to blend with the primary structure.~~
16. If the car wash will have solar panels incorporated into the plan, they should be shown as a part of the site plan.

Staff initial finding is that the proposed site plan IS CONSISTENT with the City General Plan. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

Design District: "A" (See Chapter 17.24 For BRP Zoned Sites) [17.30.160]

Maximum Building Height: 50 Feet

Minimum Setbacks:	Building	Landscaping
➤ Front (Building site)	20 Feet**	20 Feet** (only building)
➤ Front (Parking)	25 Feet**	25 Feet**
➤ Side	0 Feet	5 Feet*
➤ Street side on corner lot	25 Feet	25 Feet
➤ Side abutting residential zone	15 Feet	5 Feet
➤ Rear	0 Feet	5 Feet*
➤ Rear abutting residential zone	15 Feet	5 Feet

*(Except where building is on property line)

** (Reduce per Ordinance No. 2010-17, see Mooney Blvd. Corridor project)

Minimum Site Area: 5 acres

Parking: As prescribed in Chapter 17.34

Parking:

1. Provide 7 spaces based on the operational statement and need to provide employee and public parking.
2. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot (Zoning Ordinance Section 17.34.030.I).
3. Provide handicapped space(s) (see Zoning Ordinance Section 17.34.030.H).
4. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.34.040.D & 17.30.130.C). Through the Master Plan, the use of parking lot diamonds for tree wells may be established similar to the existing Packwood Plan area.
5. A planter is required every other row. Said planter shall contain trees on twenty (20) foot centers (Z O Section 17.30.130.C.6). Through the Master Plan, the use of parking lot diamonds for tree wells may be established similar to the existing Packwood Plan area.
6. No repair work or vehicle servicing allowed in a parking area (Z O Section 17.34.030.L).
7. It is highly recommended that bicycle rack(s) be provided on site plan.
8. No parking shall be permitted in a required front/rear/side yard (ZO Section 17.34.030.F).
9. Design/locate parking lot lighting to deflect any glare away from abutting residential areas, calculations to be shown on construction documents (Zoning Ordinance Section 17.34.030.J).
10. Parking lot to be screened from view by a 3-foot tall solid wall or shrubs when located adjacent to a public street.
11. Provide transit facilities on site plan.
12. Provide shared parking/access agreements. Said agreements/ easements to be approved and recorded prior to issuance of building permits (Zoning Ordinance Section 17.34.050).
13. Provide off-street loading facility (Zoning Ordinance Section 17.34.070 & 17.34.080).
14. The project should provide preferential parking spaces for carpools and vanpools to decrease the number of single occupant vehicle work trips. The preferential treatment could include covered parking spaces or close-in parking spaces, or designated free parking, or a guaranteed space for the vehicle.

Fencing and Screening:

1. Provide screening for roof mounted equipment (Zoning Ordinance Section 17.30.130.F).
2. Provide screened trash enclosure with solid screening gates (Z O Section 17.30.130.F).
3. Provide solid screening of all outdoor storage areas. Outdoor storage to be screened from public view with solid material (Zoning Ordinance Section 17.30.130.F).

4. Outdoor retail sales prohibited.
5. All outdoor storage areas are to be identified on the site plan and they are to be shown with screening (fencing). No materials may be stored above the storage area fence heights (Zoning Ordinance Section 17.30.130.F).

Landscaping:

1. The City has adopted the State Water Efficient Landscape Ordinance. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance. **NOTE: Prior to a final for the project, a signed Certificate of Compliance for the MWELO standards is required indicating that the landscaping has been installed to MWELO standards.**
2. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.130.C).
3. All landscape areas to be protected with 6-inch concrete curbs (Z O Section 17.30.130.F).
4. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months. Through the Master Plan, the use of parking lot diamonds for tree wells may be established similar to the existing Packwood Plan area.
5. Provide a detailed landscape and irrigation plan as a part of the building permit package (Zoning Ordinance Section 17.34.040).
6. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Z O
7. Section 17.30.130.C). Through the Master Plan, the use of parking lot diamonds for tree wells may be established similar to the existing Packwood Plan area.
8. Provide a detailed landscape and irrigation plan for review prior to issuance of building permits. Please review Zoning Ordinance section 17.30.130-C for current landscaping and irrigation requirements.
9. Locate existing oak trees on site and provide protection for all oak trees greater than 2" diameter (see Oak Tree Preservation Ordinance).
10. Maintenance of landscaped areas. - A landscaped area provided in compliance with the regulations prescribed in this title or as a condition of a use permit or variance shall be planted with materials suitable for screening or ornamenting the site, whichever is appropriate, and plant materials shall be maintained and replaced as needed, to screen or ornament the site. (Prior code § 7484)

Lighting:

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.
2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

Noise: NOISE ORDINANCE (Municipal Code Chapter 8.36)

The City's Noise Ordinance has standards for maximum noise levels near sensitive land uses. The project, as with all other uses in the City, will be required to meet the standards of the Noise Ordinance during construction of the project and during operation of the use on the site. It is the property owner's responsibility to ensure that the Ordinance is being met. Copies of the Noise Ordinance are available at the Community Development Department front counter or online at www.ci.visalia.ca.us. (Click on Municipal Code and then go to Chapter 8.36.)

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments. The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

Signature _____

A handwritten signature in black ink, written over a horizontal line. The signature is stylized and appears to be a first name followed by a last name.

ITEM NO: 1

DATE: December 16, 2015

SITE PLAN NO:

SPR15127 RESUBMIT

PROJECT TITLE:

SURF THRU CAR WASH- NO MAP

DESCRIPTION:

PROPOSED CAR WASH & VACUUM CANOPIES AND SITE IMPROVEMENTS ON A VACANT DIRT LOT (CR) (X) (A)

APPLICANT:

VERMELTFOORT ROBERT

PROP OWNER:

DBO DEVELOPMENT NO 33

LOCATION:

NWC OF CAMERON AVE & STONEBROOK ST

APN(S):

126-062-095

**City of Visalia
Police Department**

303 S. Johnson St.

Visalia, Ca. 93292

(559) 713-4370

Site Plan Review Comments



No Comment at this time.



Request opportunity to comment or make recommendations as to safety issues as plans are developed.



Public Safety Impact fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001

Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.



Not enough information provided. Please provide additional information pertaining to:



Territorial Reinforcement: Define property lines (private/public space).



Access Controlled / Restricted etc:



Lighting Concerns:



Landscaping Concerns:



Traffic Concerns:



Surveillance Issues:



Line of Sight Issues:



Other Concerns:

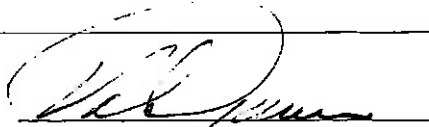
BWINTER L98

City of Visalia
 Building: Site Plan
 Review Comments

ITEM NO: 1 DATE: December 16, 2015
 SITE PLAN NO: SPR15127 RESUBMIT
 PROJECT TITLE: SURF THRU CAR WASH- NO MAP
 DESCRIPTION: PROPOSED CAR WASH & VACUUM CANOPIES AND SITE IMPROVEMENTS ON A VACANT DIRT LOT (CR) (X) (A)
 APPLICANT: VERMELTFOORT ROBERT
 PROP OWNER: DBO DEVELOPMENT NO 33
 LOCATION: NWC OF CAMERON AVE & STONEBROOK ST
 APN(S): 126-062-035

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project
 Please refer to the applicable California Codes & local ordinance for additional requirements.

- Business Tax Certification is required. For information call (559) 713-4326
 - A building permit will be required. For information call (559) 713-4444
 - Submit 4 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)
 - Submit 4 sets of plans prepared by an architect or engineer. Must comply with 2013 California Building Cod Sec. 2308 for conventional light frame construction or submit 2 sets of engineered calculations.
 - Indicate abandoned wells, septic systems and excavations on construction plans.
 - You are responsible to ensure compliance with the following checked items:
 - Meet State and Federal requirements for accessibility for persons with disabilities.
 - A path of travel, parking, common area and public right of way must comply with requirements for access for persons with disabilities.
 - Multi family units shall be accessible or adaptable for persons with disabilities.
 - Maintain sound transmission control between units minimum of 50 STC.
 - Maintain fire-resistive requirements at property lines.
 - A demolition permit & deposit is required. For information call (559) 713-4444
 - Obtain required clearance from San Joaquin Valley Air Pollution Board. Prior to am demolition work
 - For information call (661) 392-5500
 - Location of cashier must provide clear view of gas pump island
 - Plans must be approved by the Tulare County Health Department. For information call (559) 624-7400
 - Project is located in flood zone _____ * Hazardous materials report.
 - Arrange for an on-site inspection. (Fee for inspection \$151.90) For information call (559) 713-4444
 - School Development fees. Commercial \$0.54 per square foot. Residential \$3.48 per square foot.
 - Existing address must be changed to be consistent with city address. For information call (559) 713-4320
 - Acceptable as submitted
 - No comments
 - See previous comments dated: _____
- Special comments: _____



 Signature

Date: 12/14/15



Site Plan Review Comments For:

City of Visalia
Fire Department
707 W Acequia
Visalia, CA 93291
559-713-4261 office
559-713-4808 fax

ITEM NO: 1

DATE: December 16, 2015

SITE PLAN NO:

SPR15127

RESUBMIT

PROJECT TITLE:

SURF THRU CAR WASH- NO MAP

DESCRIPTION:

PROPOSED CAR WASH & VACUUM CANOPIES AND
SITE IMPROVEMENTS ON A VACANT DIRT LOT (CR)
(X) (A)

APPLICANT:

VERMELTFOORT ROBERT

PROP OWNER:

DBO DEVELOPMENT NO 33

LOCATION:

NWC OF CAMERON AVE & STONEBROOK ST

APN(S):

126-062-095

The following comments are applicable when checked:

- The Site Plan Review comments are issued as general overview of your project. With further details, additional requirements will be enforced at the Plan Review stage. Please refer to the 2013 California Fire Code (CFC), 2013 California Building Codes (CBC) and City of Visalia Municipal Codes.
- All fire detection, alarm, and extinguishing systems in existing buildings shall be maintained in an operative condition at all times and shall be replaced or repaired where defective. If building has been vacant for a significant amount of time, the fire detection, alarm, and or extinguishing systems may need to be evaluated by a licensed professional. *2013 CFC 901.6*
- No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire & life safety requirements including fire protection.
- More information is needed before a Site Plan Review can be conducted. Please submit plans with more detail. Please include information on

General:

- Address numbers must be placed on the exterior of the building in such a position as to be clearly and plainly visible from the street. Numbers will be at least four inches (4") high and shall be of a color to contrast with their background. If multiple addresses are served by a common driveway, the range of numbers shall be posted at the roadway/driveway. *2013 CFC 505.1*
- A Knox Box key lock system is required. Where access to or within a structure or an area is restricted because of secured openings (doors and/or gates) or for fire-fighting purposes, a key box is to be installed in an approved location. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Accquia Ave. Please allow adequate time for shipping and installation.) *2013 CFC 506.1*
- All hardware on exit doors shall comply with Chapter 10 of the 2013 California Fire Code. This includes all locks, latches, bolt locks, and panic and fire exit hardware.
- Provide illuminated exit signs and emergency lighting through-out building. *2013 CFC 1011*
- When portion of the building are built upon a property line or in close proximity to another structure the exterior wall shall be constructed as to comply *2013 California Building Code Table 508.4 and Table 602.*

- Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system. *2013 CFC 304.3.3*
- If your business handles hazardous material in amounts that exceed the Maximum Allowable Quantities listed on *Table 5003.1.1(1), 5003.1.1(2), 5003.1.1(3) and 5003.1.1(4) of the 2013 California Fire Code*, you are required to submit an emergency response plan to the Tulare County Health Department. Also you shall indicate the quantities on your building plans and prior to the building final inspection a copy of your emergency response plan and Safety Data Sheets shall be submitted to the Visalia Fire Department.

Water Supply:

- Construction and demolition sites shall have an approved water supply for fire protection, either temporary or permanent, and shall be made available as soon as combustible material arrives on the site. *2013 CFC 3312*
- No additional fire hydrants are required for this project; however, additional fire hydrants may be required for any future development.
- There is/are 2 fire hydrants required for this project. (See marked plans for fire hydrant locations.)
- Fire hydrant spacing shall comply with the following requirements:
The exact location of fire hydrants and final decision as to the number of fire hydrants shall be at the discretion of the fire marshal, fire chief and/or their designee. *Visalia Municipal Code 16.36.120 & 16.36.120(8)*
 - Single-family residential developments shall be provided with fire hydrants every six hundred (600) lineal feet of residential frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Multi-family, zero lot line clearance, mobile home park or condominium developments shall be provided with fire hydrants every four hundred (400) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Multi-family or condominium developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every six (600) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Commercial or industrial developments shall be provided with fire hydrants every three hundred (300) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
 - Commercial or industrial developments with one hundred (100) percent coverage fire sprinkler systems shall be provided with fire hydrants every five hundred (500) lineal feet of frontage. In isolated developments, no less than two (2) fire hydrants shall be provided.
- When any portion of a building is in excess of one hundred fifty (150) feet from a water supply on a public street there shall be provided on site fire hydrants and water mains capable of supplying the required fire flow. *Visalia Municipal Code 16.36.120(6)*

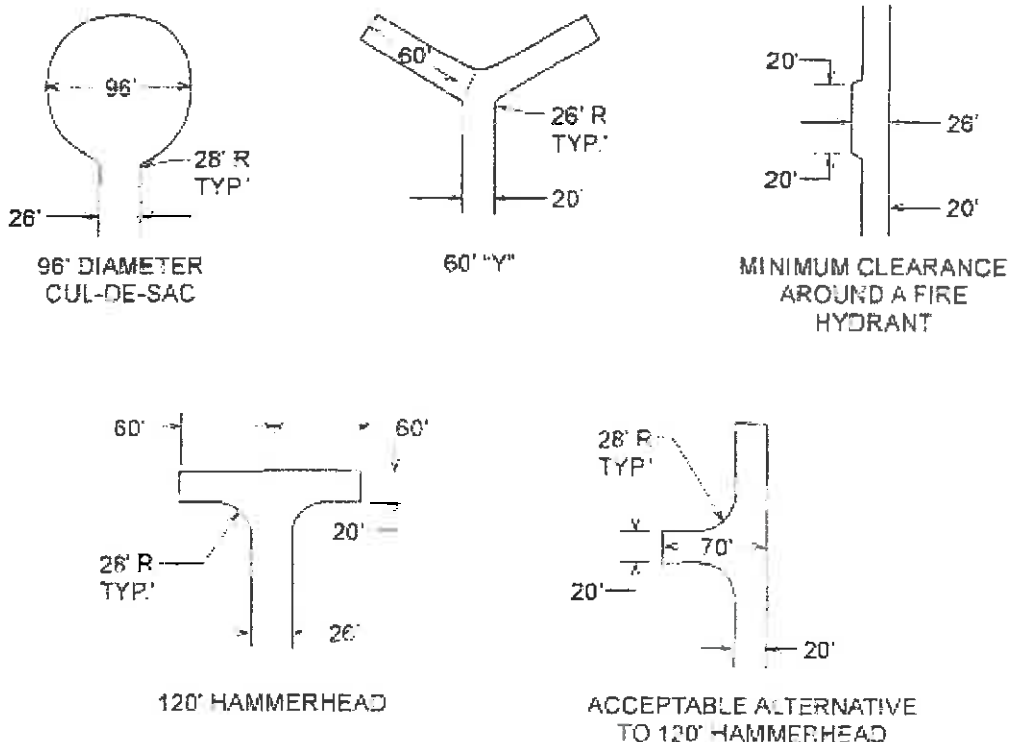
Emergency Access:

- ☒ A construction access road is required and shall be a minimum of 20 feet wide. The road shall be an all-weather driving surface accessible prior to and during construction. The access road shall be capable of holding 75,000 pound piece of fire apparatus, and shall provide access to within 100 feet of temporary or permanent fire department connections. *2013 CFC 3310*

- ☐ Buildings or portions of buildings or facilities with a vertical distance between the grade plans and the highest roof surface exceed 30 feet shall provide an approved fire apparatus access roads capable of accommodating fire department aerial apparatus. Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders. Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building. *2013 CFC D105*

- ☒ A fire apparatus access roads shall be provide and must comply with the CFC and extend to within 150 of all portions of the facility and all portions of the exterior walls of the first story of the building as measured by an approved route around the exterior of the building or facility. Minimum turning radius for emergency fire apparatus shall be 20 feet inside radius and 43 feet outside radius. *2013 CFC 503.1.1*

- ☒ Fire apparatus access roads in excess of 150 feet and dead end shall be provided with a turnaround. Length 151-500 feet shall be a minimum of 20 feet in width and have a 120 foot Hammerhead, 60-foot "Y" or 96-Foot diameter Cul-de-sac in accordance with Figure D103.1 of the 2013 CFC. Length 501-750 feet shall be 26 feet in width and have a 120 foot Hammerhead, 60-foot "Y" or 96-Foot diameter Cul-de-sac in accordance with Figure D103.1 of the 2013 CFC.



*FIGURE D103.1
DEAD-END FIRE APPARATUS ACCESS ROAD TURNAROUND*


- Gates on access roads shall be a minimum width of 20 feet and shall comply with the following:
2013 CFC D103.5
- Typical chain and lock shall be the type that can be cut with a common bolt cutter, or the developer may opt to provide a Knox Box key lock system.
 - Gates shall be of the swinging or sliding type.
 - Gates shall allow manual operation by one person. (power outages)
 - Gates shall be maintained in an operative condition at all times.
 - Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Acequia Ave. Please allow adequate time for shipping and installation.)
- In any and all new One- or two-family dwellings residential developments regardless of the number of units, street width shall be a minimum of 36 feet from curb to curb to allow fire department access and to permit parking on both sides of the street. A minimum of 20 feet shall be provided for developments that don't allow parking on the streets. *2013 CFC D107.2*

Fire Protection Systems:

- An automatic fire sprinkler system will be required for this building. Also a fire hydrant is required within 50 feet of the Fire Department Connection (FDC). *2013 CFC 903 and Visalia Municipal Code 16.36.120(7)*
- Commercial cooking appliances and domestic cooking appliances used for commercial purposes that produces grease laden vapors shall be provided with a Type 1 Hood, in accordance with the California Mechanical Code, and an automatic fire extinguishing system. *2013 CFC 904.11& 609.2*

Special Comments:

- A fire hydrant is required within 50 feet from the fire department connection (FDC).



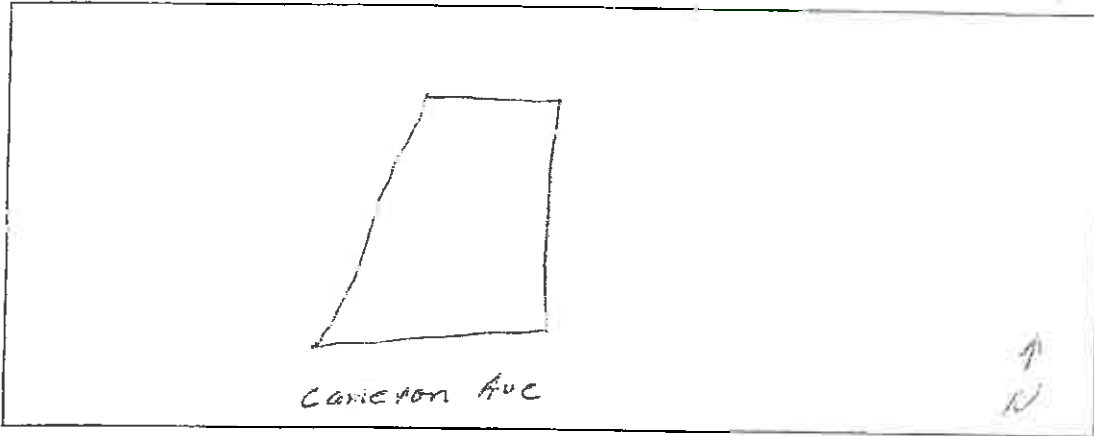
Maribel Vasquez
Fire Inspector

City of Visalia
Parks and Urban Forestry
336 N. Ben Maddox Way
Visalia, CA 93292

Date: 8-11-15

Site Plan Review # 15127

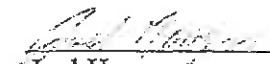
SITE PLAN REVIEW COMMENTS



COMMENTS: See Below None

- Please plot and protect all Valley Oak Trees.
- Landscape along parkway to be planted by developer and maintained by a maintenance district.
- All drainage from curb and gutter along streets to be connected to storm drain system.
- All trees planted in street right-of-way to be approved by the Public Works Superintendent of Parks.
- Tie-ins to existing infrastructure may require a bore. Check with the Public Works Department prior to any street cut.

Other Comments: _____


Joel Hooyer

Parks and Urban Forestry Supervisor
559 713-4295 Fax 559 713-4818

Email: jhooyer@ci.visalia.ca.us

CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4500

COMMERCIAL BIN SERVICE

DATE: NOVEMBER 10, 2012
SITE PLAN NO: SPR15127 RESUBMIT
PROJECT TITLE: SURF THRU CAR WASH- NO MAP
DESCRIPTION: PROPOSED CAR WASH & VACUUM CANOPIES AND SITE IMPROVEMENTS ON A VACANT DIRT LOT (CR) (X) (A)
APPLICANT: VERMELTFOORT ROBERT
PROP OWNER: DBO DEVELOPMENT NO 33
LOCATION: NWC OF CAMERON AVE & STONEBROOK ST
APN(S): 126-062-095

- No comments.
- Same comments as as
- Revisions required prior to submitting final plans. See comments below.
- Resubmittal required. See comments below.
- Customer responsible for all cardboard and other bulky recyclables to be broken down be fore disposing of in recycle containers.
- ALL refuse enclosures must be R-3 or R-4
- Customer must provide combination or keys for access to locked gates/bins
- Type of refuse service not indicated.
- Location of bin enclosure not acceptable. See comments below.
- Bin enclosure not to city standards double.
- Inadequate number of bins to provide sufficient service. See comments below.
- Drive approach too narrow for refuse trucks access. See comments below.
- Area not adequate for allowing refuse truck turning radius of :
Commercial (X) 50 ft. outside 36 ft. inside; Residential () 35 ft. outside, 20 ft. inside.
- Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- Bin enclosure gates are required
- Hammerhead turnaround must be built per city standards.
- Cul - de - sac must be built per city standards.
- Bin enclosures are for city refuse containers only. Grease drums or any other items are not allowed to be stored inside bin enclosures.
- Area in front of refuse enclosure must be marked off indicating no parking
- Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS)
- Customer will be required to roll container out to curb for service.
- Must be a concrete slab in front of enclosure as per city standards

The width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.

Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.

Bin enclosure gates must open 180 degrees and also hinges must be mounted in front of post see page 2 for instructions

PROJECT LOOKS GOOD WITH ANY FUTURE CHANGES SOLID WASTE WILL NEED TO BE NOTIFIED.

Javier Hernandez, Solid Waste Front Load Supervisor 713-4338

**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

Jason Huckleberry 713-4259
 Adrian Rubalcaba 713-4271

ITEM NO: 1 DATE: DECEMBER 16, 2015

SITE PLAN NO.: 15-127 3RD RESUBMITTAL
PROJECT TITLE: SURF THRU CAR WASH
DESCRIPTION: PROPOSED CAR WASH & VACUUM CANOPIES
AND SITE IMPROVEMENTS ON A VACANT DIRT
LOT (CR) (X) (A)
APPLICANT: VERMELTFOORT ROBERT
PROP OWNER: DBO DEVELOPMENT NO 33
LOCATION: NWC OF CAMERON AVE & STONEBROOK ST
APN: 126-062-095

SITE PLAN REVIEW COMMENTS

- REQUIREMENTS (indicated by checked boxes)
- Install curb return with ramp, with 30' radius; **MIN. 30' RADIUS**
- Install curb; gutter **AS NECESSARY ONSITE**
- Drive approach size: 35' MIN Use radius return; **REFER CITY MAJOR COMMERCIAL DETAILS**
- Sidewalk: 6' width; 10' parkway width at **CAMERON**
- Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- Right-of-way dedication required. A title report is required for verification of ownership.
- Deed required prior to issuing building permit; **EASEMENT FOR SIDEWALK**
- City Encroachment Permit Required. **ALL WORK IN PUBLIC RIGHT-OF-WAY**
- Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit. Contact Encroachment Tech. at 713-4414.
- CalTrans Encroachment Permit required. CalTrans comments required prior to issuing building permit. Contacts: David Deel (Planning) 488-4088;
- Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map.
- Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. Prepared by registered civil engineer or project architect. All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) directed to the City's existing storm drainage system; b) directed to a permanent on-site basin; or c) directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: : maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
- Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
- Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)
- Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- Traffic indexes per city standards:

- Install street striping as required by the City Engineer.
- Install landscape curbing (typical at parking lot planters).
- Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- Provide "R" value tests: each at
- Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- Access required on ditch bank, 15' minimum Provide wide riparian dedication from top of bank.
- Show Oak trees with drip lines and adjacent grade elevations. Protect Oak trees during construction in accordance with City requirements.
- A permit is required to remove oak trees. Contact Joel Hooyer at 713-4295 for an Oak tree evaluation or permit to remove. A pre-construction conference is required.
- Relocate existing utility poles and/or facilities.
- Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- Subject to existing Reimbursement Agreement to reimburse prior developer:
- Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- Comply with prior comments. Resubmit with additional information. Redesign required.

Additional Comments:

- 1. Approximately 3 street lights will be required on Cameron, per City collector street light details. A separate electrical plan design shall be submitted to City Engineer for review and approval.**
- 2. The proposed drive approach location on Cameron is acceptable. The existing median will be required to be extended east, at developer's expense, limiting access to a right-in & right-out function at time of further onsite development and construction of main drive approach.**
- 3. A Traffic Impact Study (TIS) is required; refer to City standard "Category II" criteria and further conditions of approval by Traffic Safety. Additional improvements may be required per the TIS.**
- 4. A Grant of Easement is required for the required 6' sidewalk along Cameron behind property line. Refer to City website for document format and procedures.**
- 5. All backflow preventers shall be placed behind sidewalk and out of public right-of-way. All required utility connections within/across Cameron Ave. shall be bored.**
- 6. Landscape and irrigation plans shall comply with State MWELo standards. Street trees shall be installed in parkway along Cameron Ave. Refer to Urban Forestry Street Tree ordinance, planting details, and master list.**
- 7. An accessible route from public sidewalk to building site is required. Revise accordingly.**
- 8. Parking lot shall comply with current City and ADA standards.**
- 9. Per applicant, there are available utility service stubs in Cameron. City records do not indicate existing utilities near the Surf Thru location, further coordinate with City Engineer.**

10. The associated master site layout plan noted a signalized intersection at the two aligning major drive approaches for north and south developments on Cameron. Per City standards, signalization of this intersection will not be entertained; the nearest signalized intersection may be Stonebrook & Cameron, a 1/2 mile increment standard from signalized intersection at Mooney.

11. Refuse enclosure to comply with City 24' enclosure standards with gates and concrete apron.

12. City impact fees apply to new land development and building construction. Refer to Page 4 for fee summary.

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: 15-127 3RD RESUBMITTAL

Date: 12/16/2015

Summary of applicable Development Impact Fees to be collected at the time of building permit:

(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

(Fee Schedule Date:9/4/2015)

(Project type for fee rates:CAR WASH)

Existing uses may qualify for credits on Development Impact Fees.

<u>FEE ITEM</u>	<u>FEE RATE</u>
<input checked="" type="checkbox"/> Groundwater Overdraft Mitigation Fee	\$1,196/AC
<input checked="" type="checkbox"/> Transportation Impact Fee	\$13,481/1000SF
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	\$18,126/EACH, TREATMENT PLANT FEE: \$19,736/EACH
<input type="checkbox"/> Sewer Front Foot Fee	
<input checked="" type="checkbox"/> Storm Drain Acq/Dev Fee	\$6,768/AC
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input checked="" type="checkbox"/> Waterways Acquisition Fee	\$4,968/AC
<input checked="" type="checkbox"/> Public Safety Impact Fee: Police	\$8,019/AC
<input checked="" type="checkbox"/> Public Safety Impact Fee: Fire	\$1,754/AC
<input checked="" type="checkbox"/> Public Facility Impact Fee	\$514/1000SF
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Adrian Rubalcaba



526 15127

**CITY OF VISALIA
NONSIGNIFICANT WASTEWATER
DISCHARGE PERMIT APPLICATION**

Agency Use:
Permit No: _____
Code No: _____
Data Entry By: _____

PLEASE PRINT OR TYPE

APPLICANT BUSINESS NAME: _____ PHONE: _____
BUSINESS ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____
BUSINESS OWNER: _____ PHONE: _____
MAILING ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____
CONTACT PERSON: _____ TITLE: _____
NATURE OF BUSINESS: (auto repair, car wash, machine shop, painting, battery dealer, etc.)

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO
Does your facility conduct automotive servicing operations that involve the exchange or replacement of fluids (e.g. oil, transmission or brake fluid, radiator coolant etc.)? YES NO
Does your facility have any floor drains? YES NO
Does your facility have a steam cleaner? YES NO
Does your facility wash vehicles on site? YES NO

If generated, how do you dispose of the following:

Grease, oil and sand interceptor contents _____

I hereby affirm that all information furnished is true and correct to the best of my knowledge.

Signature

Date

Public Works Department
Quality Assurance Division
7579 Ave. 288
Visalia CA 93277
(559) 713-4487

CITY OF VISALIA
ORDINANCE 13.08

13.08.570 Traps required.

Grease, oil and sand traps shall be provided when, in the opinion of the City, they are necessary for the protection of the sewerage system from liquid wastes containing grease in excessive amounts, or any flammable wastes, sand and other harmful ingredients; except that such traps shall not be required for buildings used solely for residential purposes. Such traps shall be required for example, on discharges from all service stations, automotive repair garages, car washes, restaurants, eating establishments and food preparation establishments, and such other commercial or industrial establishments as the city may designate. (Prior code § 4254)

13.08.580 Construction of traps.

All traps shall be of a type and capacity approved by the city, and shall be so located as to be readily and easily accessible for cleaning and inspection. Restaurant traps shall be gas-tight, of a type approved for restaurant use by the division of building safety. Traps for all other facilities, including service stations and garages, shall be in accordance with the adopted plan of the city for such traps or shall be the approved equal thereof as determined by the director. (Prior code § 4255)

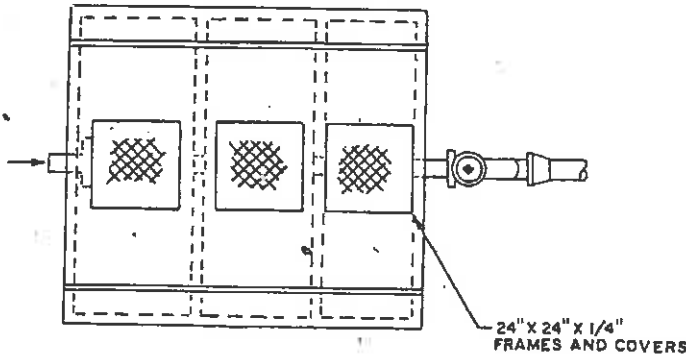
13.08.590 Maintenance of traps.

When installed, all grease, oil and sand traps shall be maintained by the owner, at owner's expense, in continuously efficient operation at all times. (Prior code § 4256)

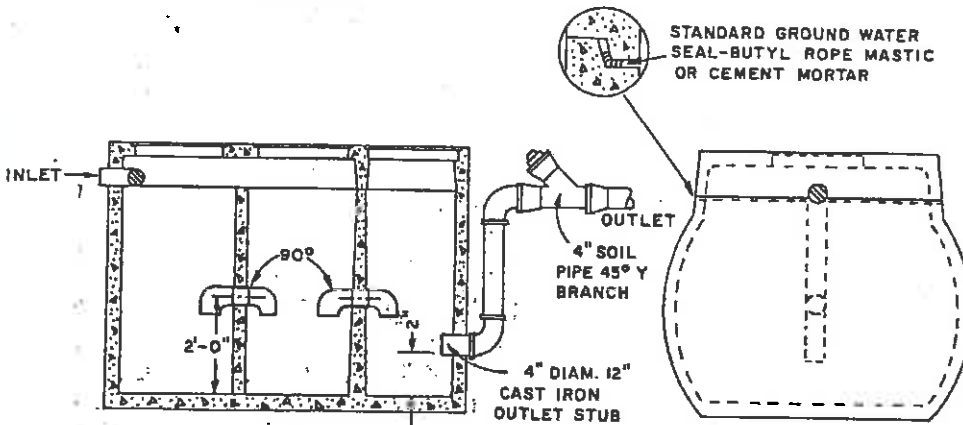
TYPICAL PRECAST 1000 GAL.

NO SCALE

(SEE DETAIL S-9A FOR NOTES)



PLAN VIEW



LONGITUDINAL SECTION

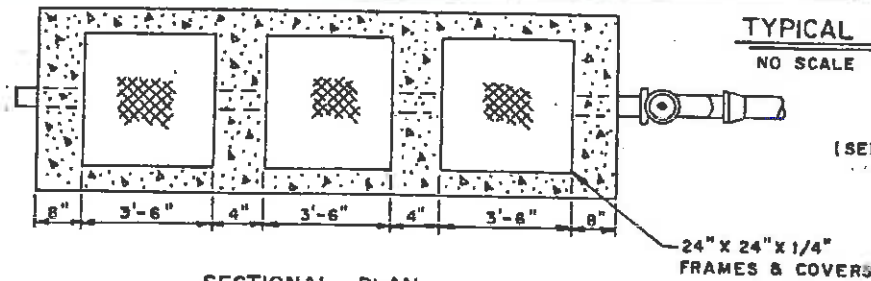
END VIEW

TYPICAL POUR-IN-PLACE

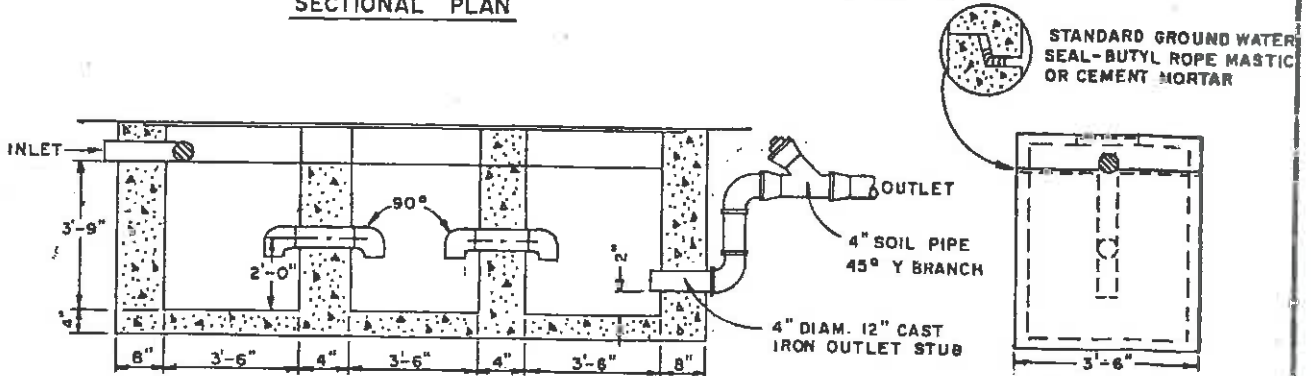
NO SCALE

1000 GAL.

(SEE DETAIL S-9A FOR NOTES)



SECTIONAL PLAN



ELEVATION SECTION

END VIEW

Approved by Visalia City Council on _____ DATE

**City of Visalia
Improvement Standards**

**Standard Outside Industrial Sand,
Grease & Oil Interceptor**

REVISIONS

S-9

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

December 16, 2015

ITEM NO: <u>1</u>	RESUBMITL
SITE PLAN NO:	SPR15127
PROJECT TITLE:	SURF THRU CAR WASH- NO MAP
DESCRIPTION:	PROPOSED CAR WASH & VACUUM CANOPIES AND SITE IMPROVEMENTS ON A VACAN DIRT LOT (CR) (X) (A)
APPLICANT:	VERMELTFOORT ROBERT
PROP. OWNER:	DBO DEVELOPMENT NO 33
LOCATION:	NWC OF CAMERON AVE & STONEBROOK ST
APN(S):	126-062-095

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- No Comments
- See Previous Site Plan Comments
- Install Street Light(s) per City Standards.
- Install Street Name Blades at Locations.
- Install Stop Signs at Locations.
- Construct parking per City Standards PK-1 through PK-4.
- Construct drive approach per City Standards.
- Traffic Impact Analysis required.

Additional Comments:

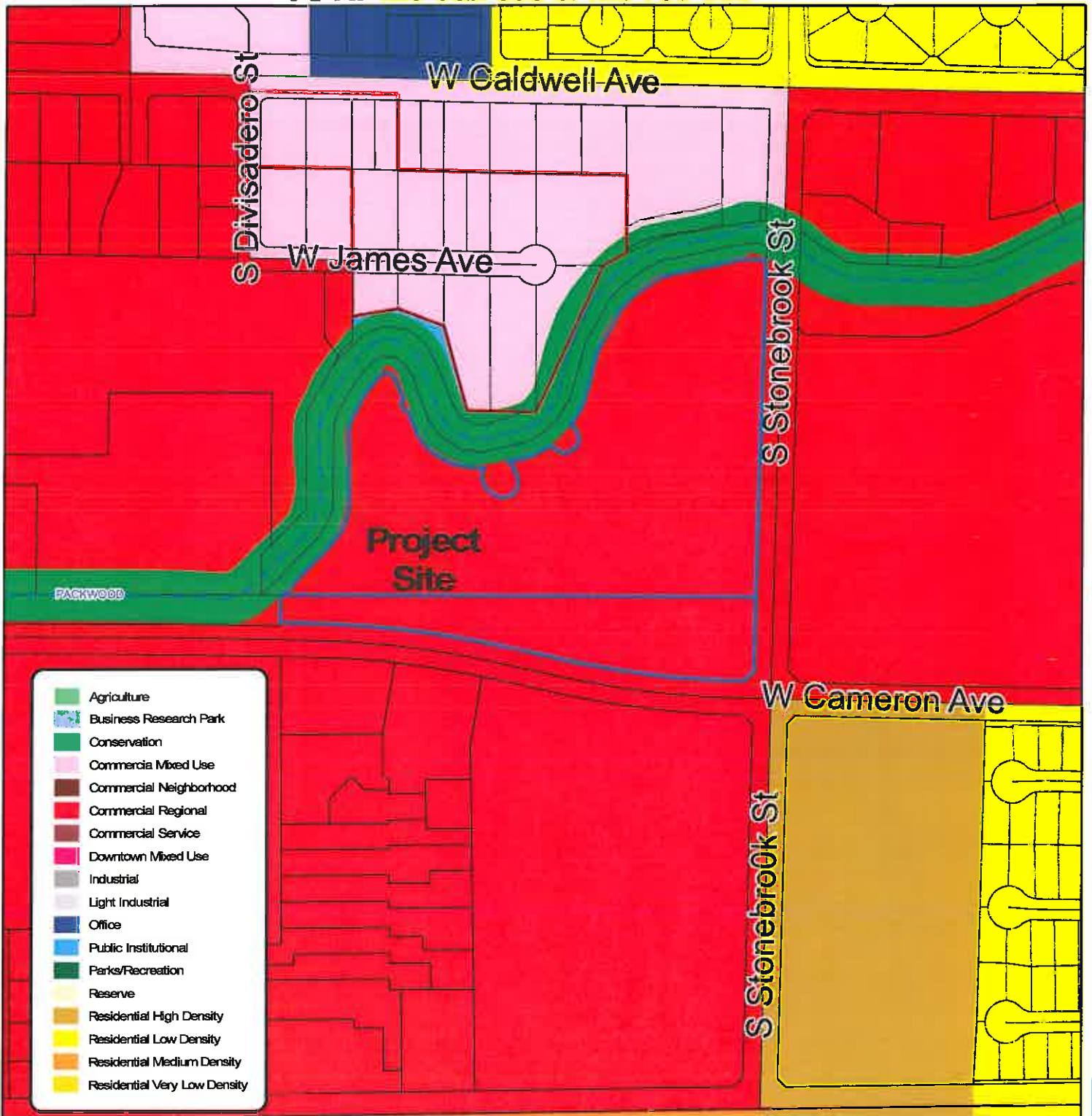
- Driveway shall align with Costco driveway to the south.



Leslie Blair

CUP No. 2016-09 & Tentative Parcel Map No. 2016-06

APN: 126-062-095 & 126-730-039

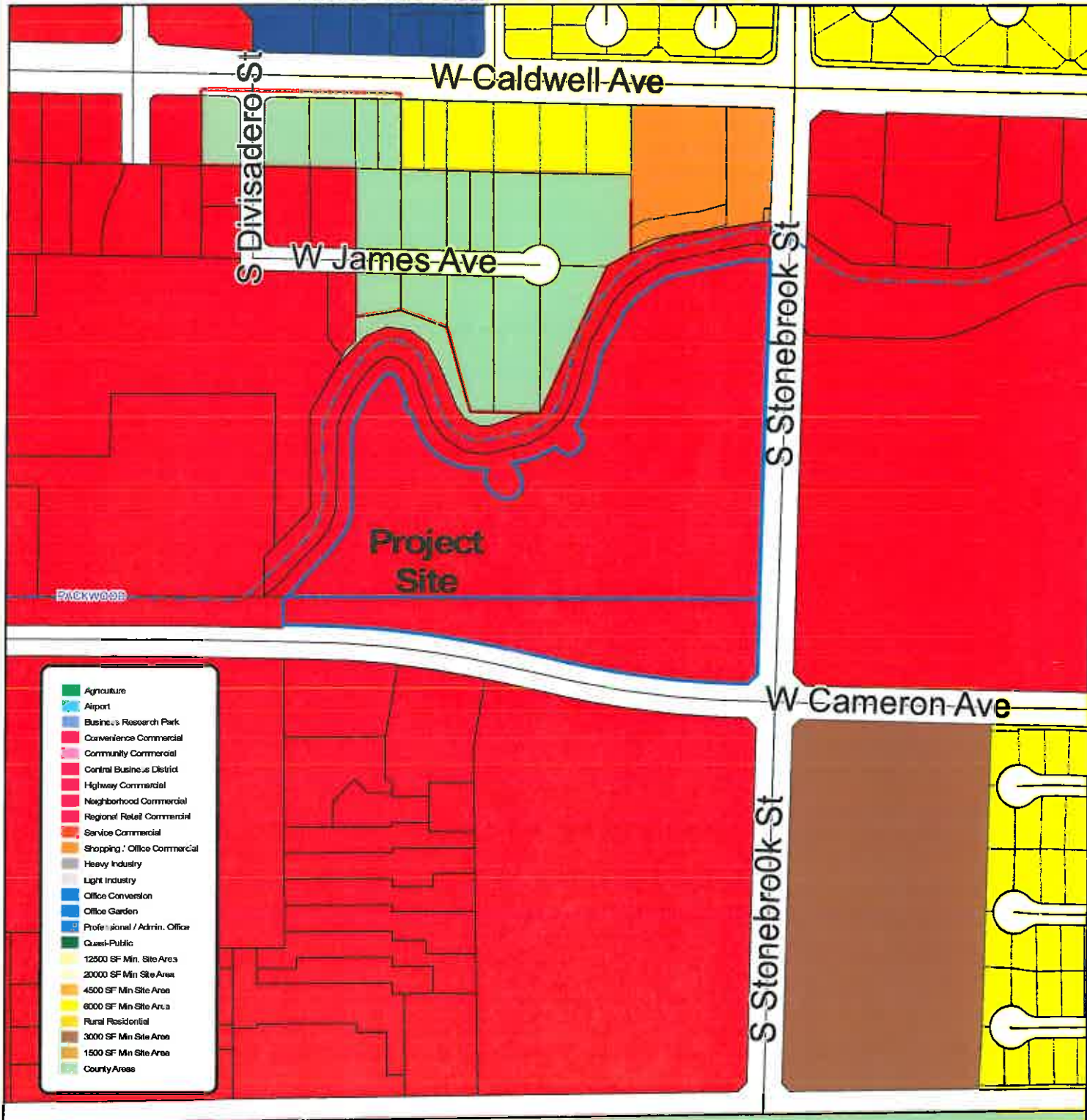


General Plan Land Use Map



CUP No. 2016-09 & Tentative Parcel Map No. 2016-06

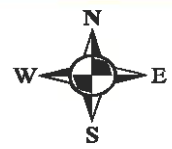
APN: 126-062-095 & 126-730-039



- Agriculture
- Airport
- Business Research Park
- Convenience Commercial
- Community Commercial
- Central Business District
- Highway Commercial
- Neighborhood Commercial
- Regional Retail Commercial
- Service Commercial
- Shopping / Office Commercial
- Heavy Industry
- Light Industry
- Office Conversion
- Office Garden
- Professional / Admin. Office
- Quasi-Public
- 12500 SF Min. Site Area
- 20000 SF Min. Site Area
- 4500 SF Min. Site Area
- 6000 SF Min. Site Area
- Rural Residential
- 3000 SF Min. Site Area
- 1500 SF Min. Site Area
- County Areas



Zoning Map



CUP No. 2016-09 & Tentative Parcel Map No. 2016-06

APN: 126-062-095 & 126-730-039



Aerial Photo



CUP No. 2016-09 & Tentative Parcel Map No. 2016-06

APN: 126-062-095 & 126-730-039



Project Site

W James Ave

S Stonebrook St

W Cameron Ave

S Stonebrook St

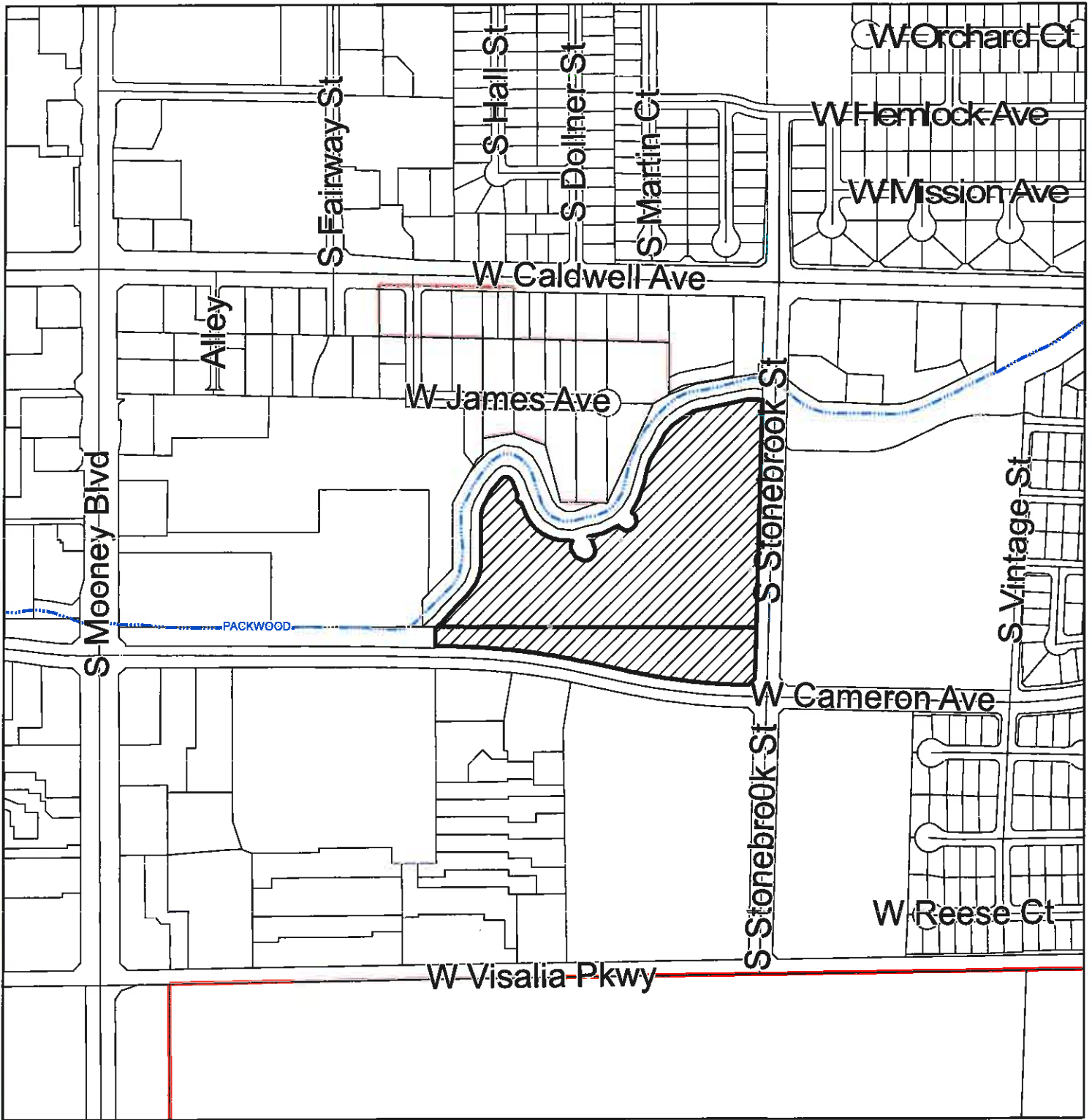


Aerial Photo



CUP No. 2016-09 & Tentative Parcel Map No. 2016-06

APN: 126-062-095 & 126-730-039



Vicinity Map



City of Visalia

Memo



To: Planning Commission
From: Paul Scheibel, AICP, Principal Planner (713-4369) *PS*
Date: June 13, 2016
Re: Request for Continuation of Agenda Item 9, General Plan Amendment 2016-06, 5th Cycle General Plan Housing Element Update (2014-2023)

Recommended Action

Staff recommends that the Planning Commission continue the item to the June 27, 2016, Planning Commission meeting.

Discussion

Staff requests the item be continued to June 27, 2016. Staff is currently revising the draft document to address the formal review comments received from the State Department of Housing and Community Development (HCD) on May 20, 2016. Staff's intent is to complete the responses to the HCD comments and to incorporate them in a final revised Housing Element Update text. Further, given the length of the revised document (approximately 150 pages), staff intends to provide early distribution of the revised document for the Planning Commission's convenience.