

Riverbend Commercial Center
Transportation Impact Analysis
Report in the City of Visalia

Final Report
October 2012

Prepared for
Forester, Weber &
Associates, L.L.C.

Prepared By:



**RIVERBEND COMMERCIAL CENTER
TRANSPORTATION IMPACT ANALYSIS REPORT
IN THE CITY OF VISALIA**

FINAL REPORT

**Prepared For:
FORESTER, WEBER & ASSOCIATES, L.L.C.**

Prepared By

**OMNI-MEANS, LTD.
ENGINEERS & PLANNERS
309 West Main Street, Suite 100
Visalia, California 93291
(559) 734-5895**

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INTRODUCTION

This report has been prepared to present the results of a traffic study prepared by OMNI-MEANS for a proposed retail Phase I development in the City of Visalia. The development, herein called project, is located at the southeast corner of Riggin Avenue/Dinuba Boulevard (also called State Route 63) in the City of Visalia (reference Figure 1). According to the site plan, this proposed project contains 9.25 acres of Phase I space, including restaurants, a gas station, office space, and retail uses.

This study will include a two-phased approach. Phase I is proposed to consist of a Wendy's Restaurant and an ARCO gas station with a convenience market and a car wash. The second phase will provide for additional Phase I development with office space, another restaurant, and other retail uses. This will be analyzed in the 2035 cumulative analysis conditions.

EXISTING ROADWAY SYSTEM

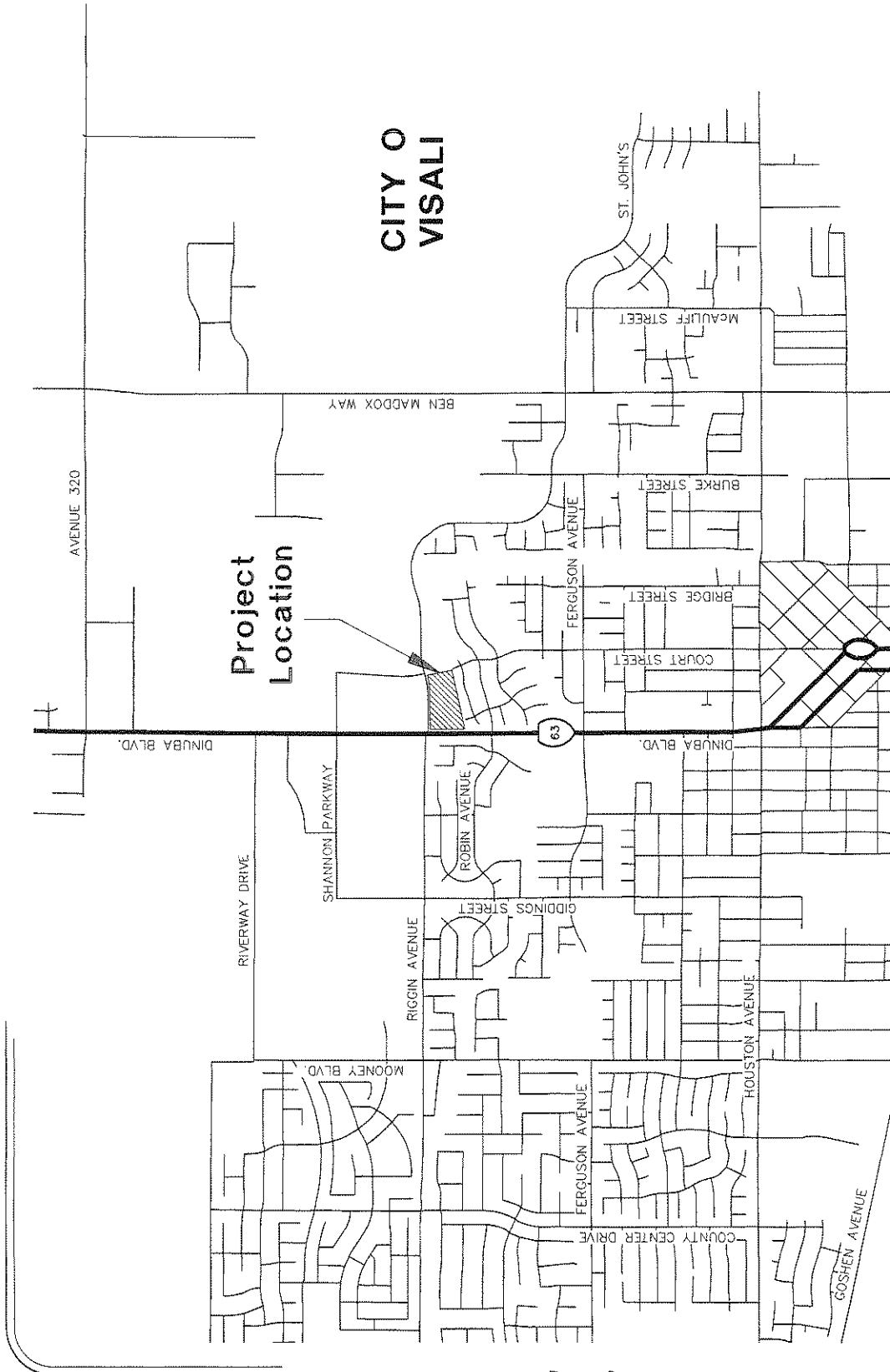
Roadways that provide primary circulation in the vicinity of the project site include Riggin Avenue, Dinuba Boulevard (State Route 63), Court Street, Shannon Parkway, Robin Avenue, Ferguson Avenue, St. John's Parkway, Giddings Street, and Ben Maddox Way.

Riggin Avenue is an east-west arterial in Visalia that extends from the community of Goshen to Thomas Street where it heads south and turns into St. Johns Parkway. This roadway is two-lanes west of Conyer Street and four-lanes east of Conyer Street until it merges into St. Johns Parkway. Riggin Avenue serves industrial uses in northwest Visalia and residential, retail and commercial uses along the corridor.

Dinuba Boulevard (State Route 63) is a major north-south four-lane arterial that extends from Houston Avenue north through the city limits. State Route 63 ultimately begins at State Route 137 in the City of Tulare and ends in Fresno County. State Route 63 is known as Mooney Boulevard in Visalia until it reaches State Route 198, where it is diverted east and picked up again at Court Street north of State Route 198. Dinuba Boulevard becomes State Route 63 north of Houston Avenue and serves residential, commercial and retail uses throughout the project study area.

Court Street is a north-south roadway that runs parallel to and east of Dinuba Boulevard and will be a future driveway for the project. This roadway is planned to be extended through the project area between Dove Avenue and Riggin Avenue. In the study area, this roadway is two-lanes and primarily serves residential land uses.

Shannon Parkway, Sedona Avenue (Future), Robin Avenue, Ferguson Avenue, St. John's Parkway, Giddings Street and Ben Maddox Way are other local facilities that provide circulation for this project and are analyzed in this report at the study intersections.



Riverbend Commercial Center TIAR

Project Vicinity Map

EXISTING TRAFFIC VOLUMES

Based upon OMNI-MEANS' understanding of the project, the following existing intersections were identified as critical intersections for this study.

- Shannon Parkway/State Route 63
- Riggin Avenue/N. Giddings Street
- Riggin Avenue/Dinuba Boulevard (State Route 63)
- Riggin Avenue/Court Street
- Robin Avenue/State Route 63
- Ferguson Avenue/State Route 63
- St. John's Parkway/Ben Maddox Way
- Sedona Avenue/State Route 63 (Future Only)
- Project Driveways

At the study intersections, existing weekday AM and PM peak-hour traffic volume counts were conducted by Metro Traffic Data in April 2011 while schools were in session. The AM peak hour is defined as one-hour of peak traffic flow counted between 7:00 AM and 9:00 AM and the PM peak hour is defined as one-hour of peak traffic flow counted between 4:00 PM and 6:00 PM. Figure 2 shows the existing AM and PM peak hour intersection traffic volumes and Figure 3 identifies existing lane geometrics and control at the study intersections.

LEVEL OF SERVICE METHODOLOGY

Traffic operations have been quantified through the determination of "Level of Service" (LOS). LOS is a qualitative measure of traffic operating conditions, whereby a letter grade "A" through "F" is assigned to an intersection or roadway segment representing progressively worsening traffic conditions. LOS was calculated for different intersection control types using the methods documented in the *Highway Capacity Manual 2000 (HCM 2000)*. LOS definitions for different types of intersection controls are outlined in Table 1.

The City of Visalia General Plan Circulation Element has designated LOS "D" as the minimum acceptable LOS standard on City facilities in general. In this report, a peak-hour of LOS "D" is taken as the threshold for acceptable traffic operations at all study intersections. All intersection turning movement volumes and LOS worksheets are contained in the Appendix.

To determine whether "significance" should be associated with unsignalized intersection LOS, a supplemental traffic signal warrant analysis was also performed. The signal warrant criteria employed for this study are presented in the *Manual on Uniform Traffic Control Devices (MUTCD)*. Specifically, this study utilized the Peak-Hour-Volume Warrant 3 (Urban Areas). Though utilization of this warrant may indicate that signalization would be required, the final decision to provide this improvement should be based on further studies utilizing the additional warrants presented in the MUTCD.

TABLE 1
LEVEL OF SERVICE CRITERIA
FOR INTERSECTIONS

LEVEL OF SERVICE	TYPE OF FLOW	DELAY	MANEUVERABILITY	STOPPED D	
				SIGNALIZED	U
A	Stable Flow	Very slight delay. Progression is very favorable, with most vehicles arriving during the green phase not stopping at all.	Turning movements are easily made, and nearly all drivers find freedom of operation.	≤ 10.0	
B	Stable Flow	Good progression and/or short cycle lengths. More vehicles stop than for LOS A, causing higher levels of average delay.	Vehicle platoons are formed. Many drivers begin to feel somewhat restricted within groups of vehicles.	>10 and ≤ 20.0	>10 and $<$
C	Stable Flow	Higher delays resulting from fair progression and/or longer cycle lengths. Individual cycle failures may begin to appear at this level. The number of vehicles stopping is significant, although many still pass through the intersection without stopping.	Back-ups may develop behind turning vehicles. Most drivers feel somewhat restricted	>20 and ≤ 35.0	>15 and $<$
D	Approaching Unstable Flow	The influence of congestion becomes more noticeable. Longer delays may result from some combination of unfavorable progression, long cycle lengths, or high volume-to-capacity ratios. Many vehicles stop, and the proportion of vehicles not stopping declines. Individual cycle failures are noticeable.	Maneuverability is severely limited during short periods due to temporary back-ups.	>35 and ≤ 55.0	>25 and $<$
E	Unstable Flow	Generally considered to be the limit of acceptable delay. Indicative of poor progression, long cycle lengths, and high volume-to-capacity ratios. Individual cycle failures are frequent occurrences.	There are typically long queues of vehicles waiting upstream of the intersection.	>55 and ≤ 80.0	>35 and $<$
F	Forced Flow	Generally considered to be unacceptable to most drivers. Often occurs with over saturation. May also occur at high volume-to-capacity ratios. There are many individual cycle failures. Poor progression and long cycle lengths may also be major contributing factors.	Jammed conditions. Back-ups from other locations restrict or prevent movement. Volumes may vary widely, depending principally on the downstream back-up conditions.	> 80.0	> 50.0 and > 50.0

References: *Highway Capacity Manual 2000*

This traffic study generally provides a “planning level” evaluation of traffic operating conditions, which is considered sufficient for California Environmental Quality Act/National Environmental Policy Act (CEQA/NEPA) purposes. This planning level evaluation has, however, incorporated actual heavy-vehicle adjustment factors, peak hour factors, and signal lost-time factors and reports the resulting intersection delays and LOS as estimated using HCM-2000 methodologies. In this study, a general Peak Hour Factor (PHF) of 0.92 has been applied to the analysis of all study intersections under all analysis scenarios.

The HCM-recommended suburban traffic signal default cycle length of 100 seconds has been used for analysis of future signalized intersections, with 4 seconds of “lost time” per critical signal phase. The *Traffic 8.0* integrated computer software program has been utilized to implement the HCM-2000 analysis methodologies.

EXISTING TRAFFIC OPERATIONS

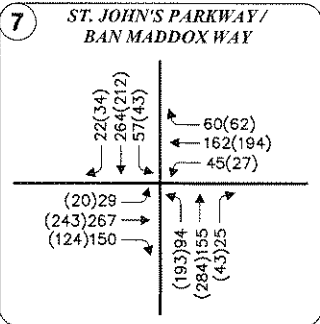
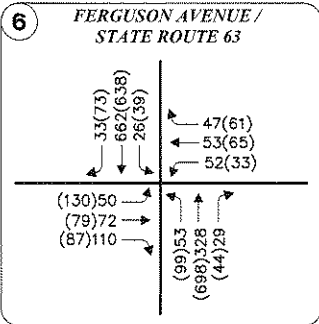
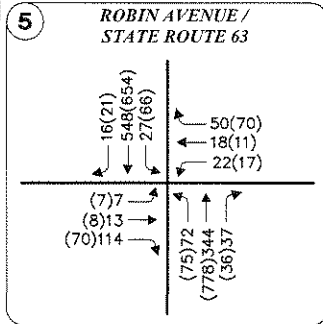
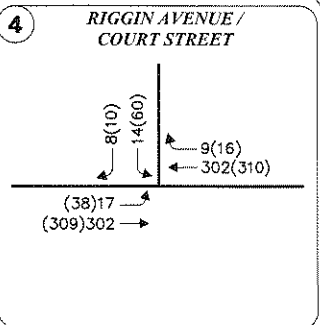
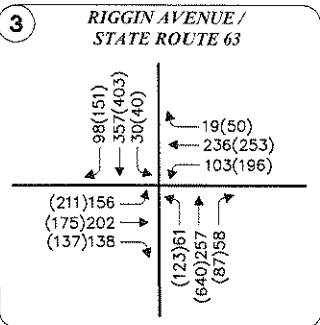
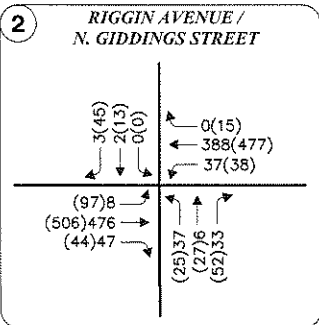
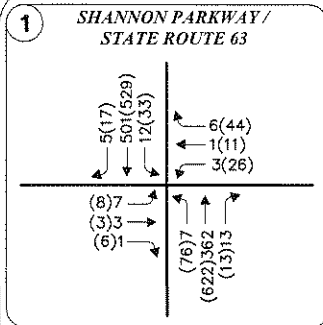
“Existing” peak-hour intersection traffic operations were quantified applying existing traffic volumes (shown on Figure 2) and existing intersection lane geometrics and control (shown on Figure 3). Table 2 presents the “Existing” peak hour intersection LOS.

**TABLE 2
EXISTING CONDITIONS:
INTERSECTION LEVELS-OF-SERVICE**

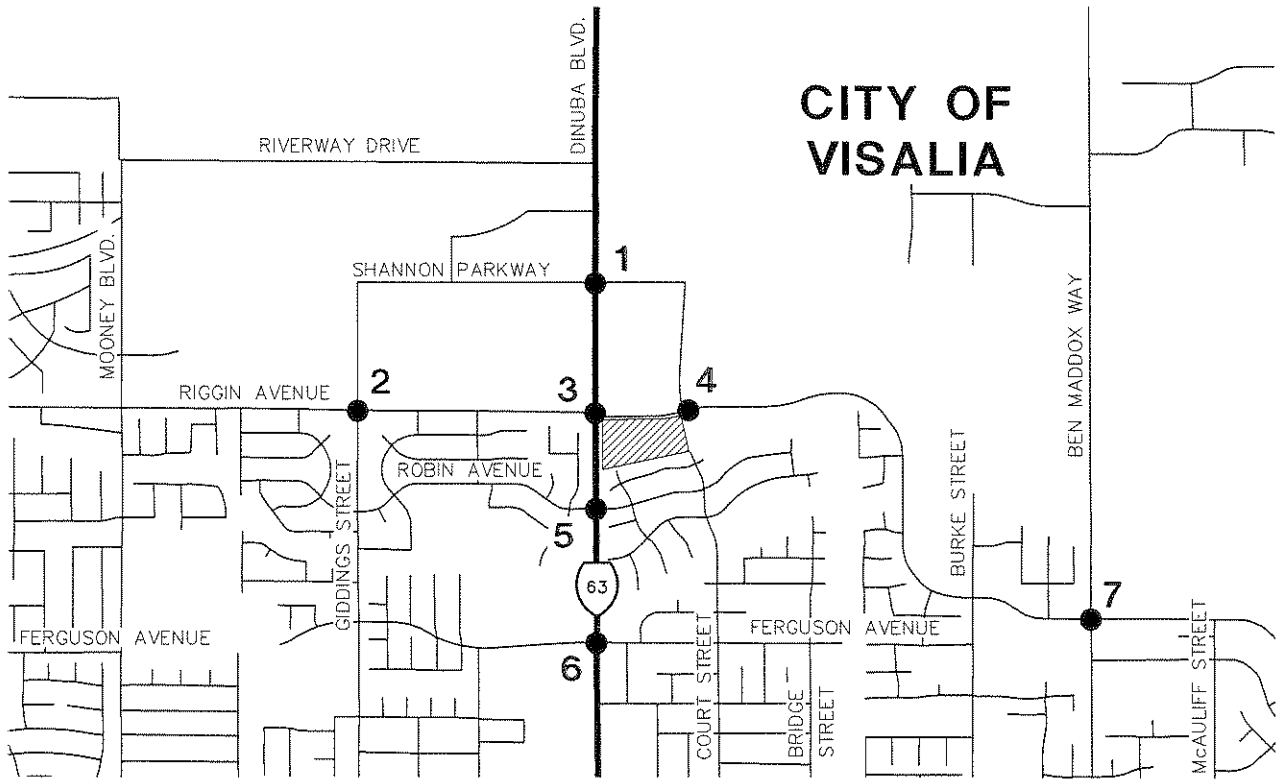
No	Intersection	Control Type	AM Peak Hour			PM Peak Hour		
			Delay (sec/veh)	LOS	Warrant Met?	Delay (sec/veh)	LOS	Warrant Met?
1	Shannon Parkway/State Route 63	Signal	8.0	A	–	15.7	B	–
2	Riggin Avenue/N. Giddings Street	TWSC	24.4	C	No	63.6	F	No
3	Riggin Avenue/State Route 63	Signal	19.9	B	–	26.5	C	–
4	Riggin Avenue/Court Street	TWSC	11.6	B	No	14.3	B	No
5	Robin Avenue/State Route 63	TWSC	27.2	D	No	83.0	F	No
6	Ferguson Avenue/State Route 63	Signal	15.6	B	–	22.2	C	–
7	St. John’s Parkway/Ben Maddox Way	Signal	28.5	C	–	28.4	C	–

*Legend: TWSC = Two-Way-Stop Control. AWSC = All-Way Stop Control.
Average Delay = Average Intersection Delay for Signalized Intersections.
Average Delay = Worst-Case Intersection Movement Delay for TWSC Intersections.
LOS = Average Intersection Level-of-Service for Signalized Intersections.
LOS = Worst-Case Movement’s Level-of-Service for TWSC Intersections.
Warrant = MUTCD Peak-Hour Warrant-3.*

As indicated in Table 2, the intersections at Riggin Avenue/N. Giddings Street and Robin Avenue/State Route 63 are projected to operate at a LOS “F” and “E” conditions, respectively, during the PM peak hour period under “Existing” conditions scenario. In addition, none of the unsignalized intersections meet the MUTCD Peak Hour Warrant 3 under “Existing” AM and PM peak hour conditions.



LEGEND:
 XX - AM PEAK HOUR TRAFFIC VOLUMES
 (XX) - PM PEAK HOUR TRAFFIC VOLUMES
 [Hatched Box] - PROJECT LOCATION

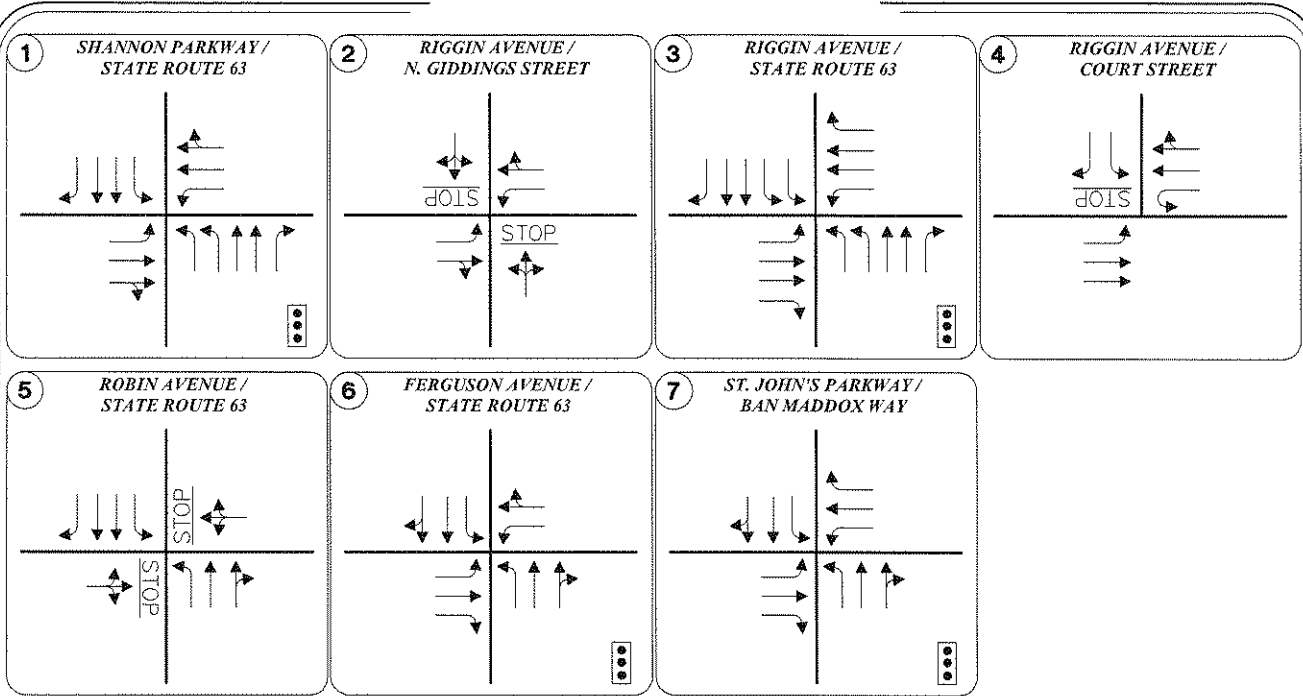


Riverbend Commercial Center TIAR

Figure 2

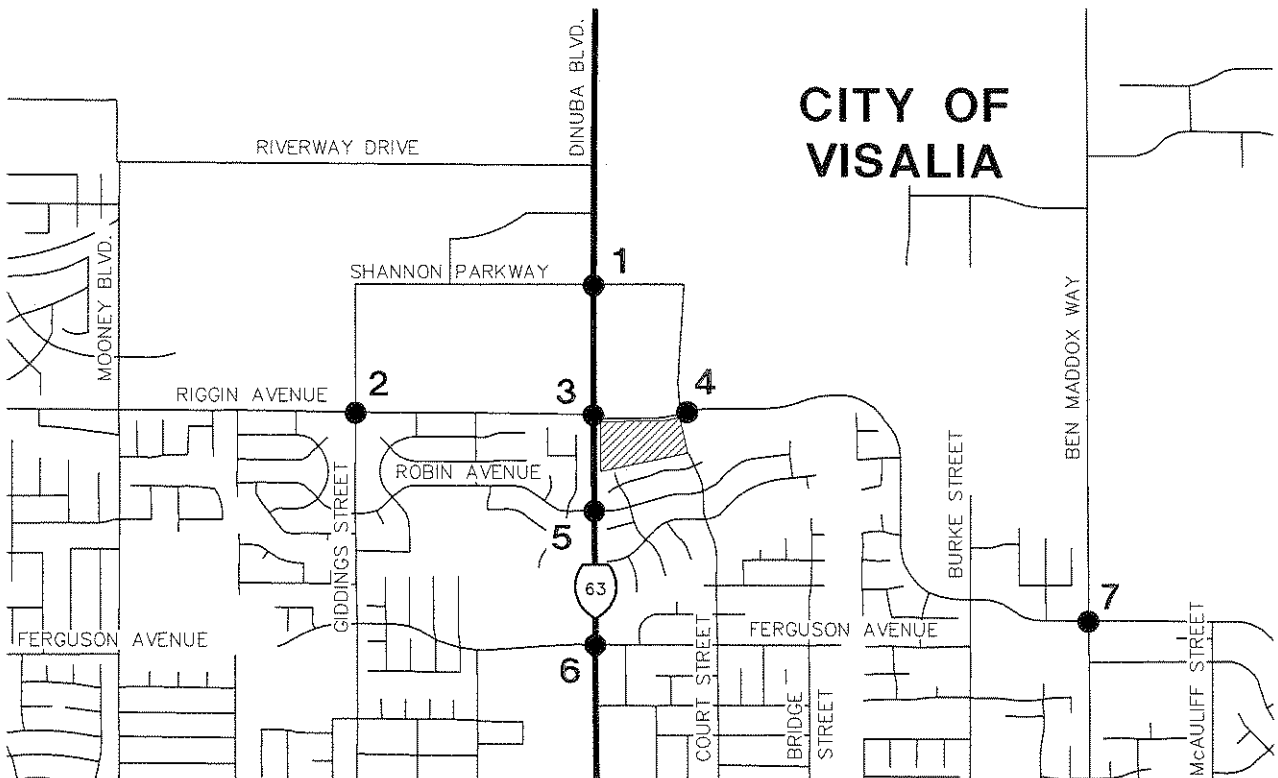
Existing Traffic Volumes





LEGEND:

- PROJECT LOCATION



Riverbend Commercial Center TIAR

Figure 3

Existing Lane Geometrics and Control



Vehicle queues by approach and by approach movement were also evaluated at each of the study intersections with the results shown in Table 3. As indicated in Table 3, the available storage currently accommodates the 95th percentile queue storage requirements for all intersection movements during the AM and PM peak hours.

**TABLE 3
EXISTING CONDITIONS:
INTERSECTION QUEUING ANALYSIS**

Int. #	Queue Segment - Direction	# Lanes	Total Storage (Ft.)	AM Peak Hour 95th % Queue	PM Peak Hour 95th % Queue
1	<i>Shannon Parkway/State Route 63</i>				
	Eastbound Left	1	300	20	25
	Westbound Left	1	240	15	45
	Northbound Left	2	340	15	50
	Northbound Right	1	260	15	15
	Southbound Left	1	300	30	55
	Southbound Right	1	260	15	20
3	<i>Riggin Avenue/State Route 63</i>				
	Eastbound Left	1	270	240	230
	Eastbound Right	1	300	45	45
	Westbound Left	1	280	140	235
	Westbound Right	1	300	15	20
	Northbound Left	2	740	45	45
	Northbound Right	1	370	30	40
	Southbound Left	2	750	25	30
Southbound Right	1	325	40	50	
5	<i>Robin Avenue/State Route 63</i>				
	Eastbound Left/Thru/Right	1	230	35	50
	Westbound Left/Thru/Right	1	330	45	115
	Northbound Left	1	140	15	15
Southbound Left	1	320	15	15	
6	<i>Ferguson Avenue/State Route 63</i>				
	Eastbound Left	1	80	70	75
	Eastbound Right	1	80	40	35
	Westbound Left	1	125	70	50
	Northbound Left	1	200	70	135
Southbound Left	1	100	45	55	

APPROVED/PENDING PROJECTS DESCRIPTION

Within the vicinity of the project, several projects have either been approved or are pending approval to be developed. These projects are single and multi-family residential projects that are all located in north Visalia. The residential projects are identified in Table 4, which outlines number of planned units and number of units within the vicinity of the project.

**TABLE 4
APPROVED/PENDING PROJECTS TRIP GENERATION**

Land Use Category	Unit	Daily Trip Rate/Unit	Weekday AM Peak Hour Rate/Unit			Weekday PM Peak Hour Rate/Unit		
			Total	In %	Out %	Total	In %	Out %
Single Family DU [ITE Code: 210]	Per DU	9.57	0.75	25%	75%	1.01	63%	37%
Apartment [ITE Code: 220]	Per DU	6.65	0.51	20%	80%	0.62	65%	35%
Description	Quantity	Daily Trips	Weekday AM Peak Hour Trips			Weekday PM Peak Hour Trips		
			Total	In	Out	Total	In	Out
Four Creeks Estates (ITE Code: 210)	86 units	823	65	16	49	87	55	32
Orchard Walk (ITE Code: 210)	104 units	995	78	20	58	105	66	39
Riverbend Village (ITE Code: 210)	128 units	1,225	96	24	72	129	81	48
Rockwood Estates (ITE Code: 210)	30 units	287	23	6	17	30	19	11
Shannon Ranch (ITE Code: 210)	471 units	4,507	353	88	268	476	300	176
Highland Park Estates (ITE Code: 210)	175 units	1,675	131	33	98	177	112	65
Highland Park Estates (ITE Code: 220)	173 units	1,150	88	18	70	107	70	37
Vineyard Villas (ITE Code: 220)	66 units	439	34	7	27	41	27	14
Total Approved/Pending Project Trips	1,233 units	11,101	868	212	659	1,152	730	422

Note: Errors due to rounding may occur.

APPROVED/PENDING PROJECTS TRIP GENERATION

Table 4 provides project trip generation for Approved/Pending land-uses based upon data presented in *ITE Trip Generation* (8th Edition). As indicated in Table 4, these seven developments, which include 1,233 new residential units, are estimated to generate 11,101 daily trips, including 868 AM peak hour trips and 1,152 PM peak hour trips.

APPROVED/PENDING PROJECTS TRIP NATURE, DISTRIBUTION, AND ASSIGNMENT

The approved/pending projects are expected to “generate” trips within the City or from other locations within the County. Directional trip distribution for approved/pending projects generated trips was estimated based upon existing traffic flow patterns, geographic location of the project sites, and location of other similar destinations. Trip path assignments were developed based upon origin and destination of trips, location of intersections and driveways, access restrictions at the study intersections and driveways, and on-site circulation patterns.

EXISTING PLUS APPROVED/PENDING PROJECTS CONDITIONS

“Existing plus Approved/Pending Projects” peak-hour intersection operations were quantified under existing lane geometrics and control identified in Figure 3. Applying *Traffix 8.0* computer software, “Existing plus Approved/Pending Projects” peak hour traffic conditions were simulated by superimposing new trips generated by the “Approved/Pending Projects”, as identified in Table 4, over “Existing” base traffic volumes at the study intersections. No improvements to the roadway system were assumed. The resulting “Existing plus Approved/Pending Projects” peak hour intersection traffic volumes are shown on Figure 4. Table 5 presents the resulting peak hour intersection LOS.

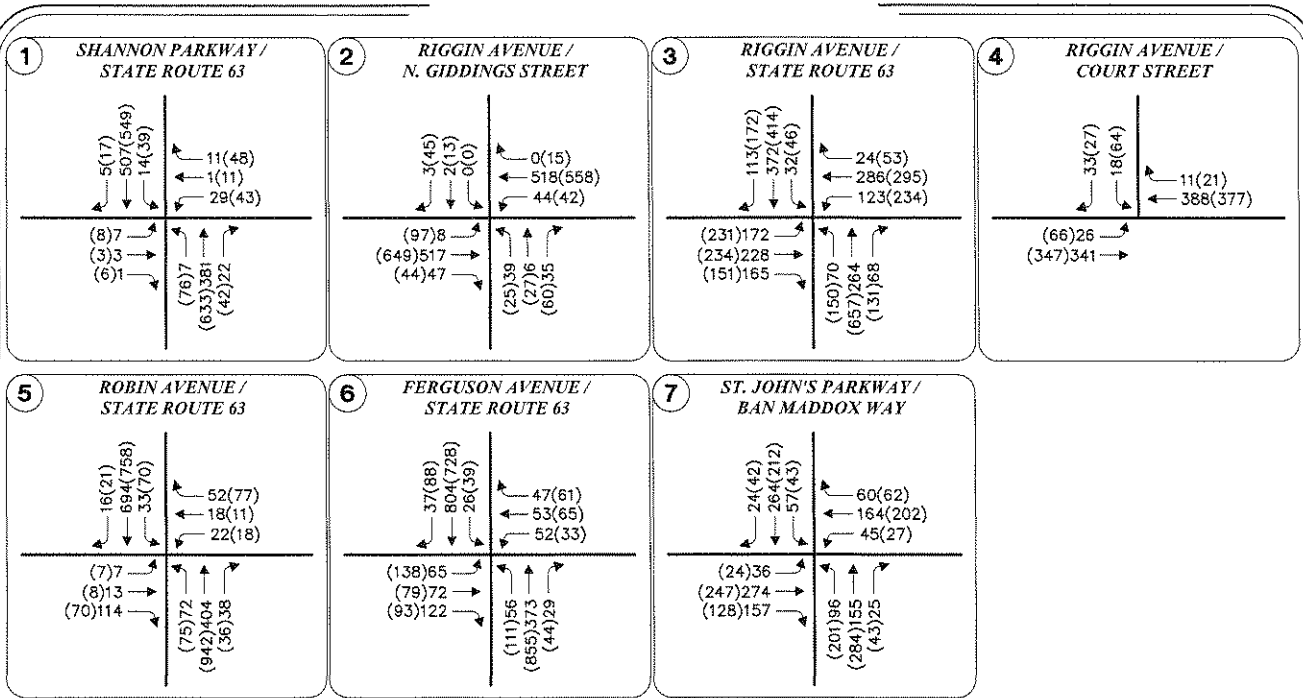
**TABLE 5
EXISTING PLUS APPROVED/PENDING PROJECTS CONDITIONS:
INTERSECTION LEVELS-OF-SERVICE**

No	Intersection	Control Type	AM Peak Hour			PM Peak Hour		
			Delay (sec/veh)	LOS	Warrant Met?	Delay (sec/veh)	LOS	Warrant Met?
1	Shannon Parkway/State Route 63	Signal	18.8	B	--	11.8	B	--
2	Riggin Avenue/N. Giddings Street	TWSC	34.1	D	No	OVRFL	F	Yes
3	Riggin Avenue/State Route 63	Signal	21.5	C	--	35.8	D	--
4	Riggin Avenue/Court Street	TWSC	11.6	B	No	16.3	C	No
5	Robin Avenue/State Route 63	TWSC	41.4	E	No	OVRFL	F	No
6	Ferguson Avenue/State Route 63	Signal	30.9	C	--	22.9	C	--
7	St. John's Parkway/Ben Maddox Way	Signal	28.7	C	--	28.6	C	--

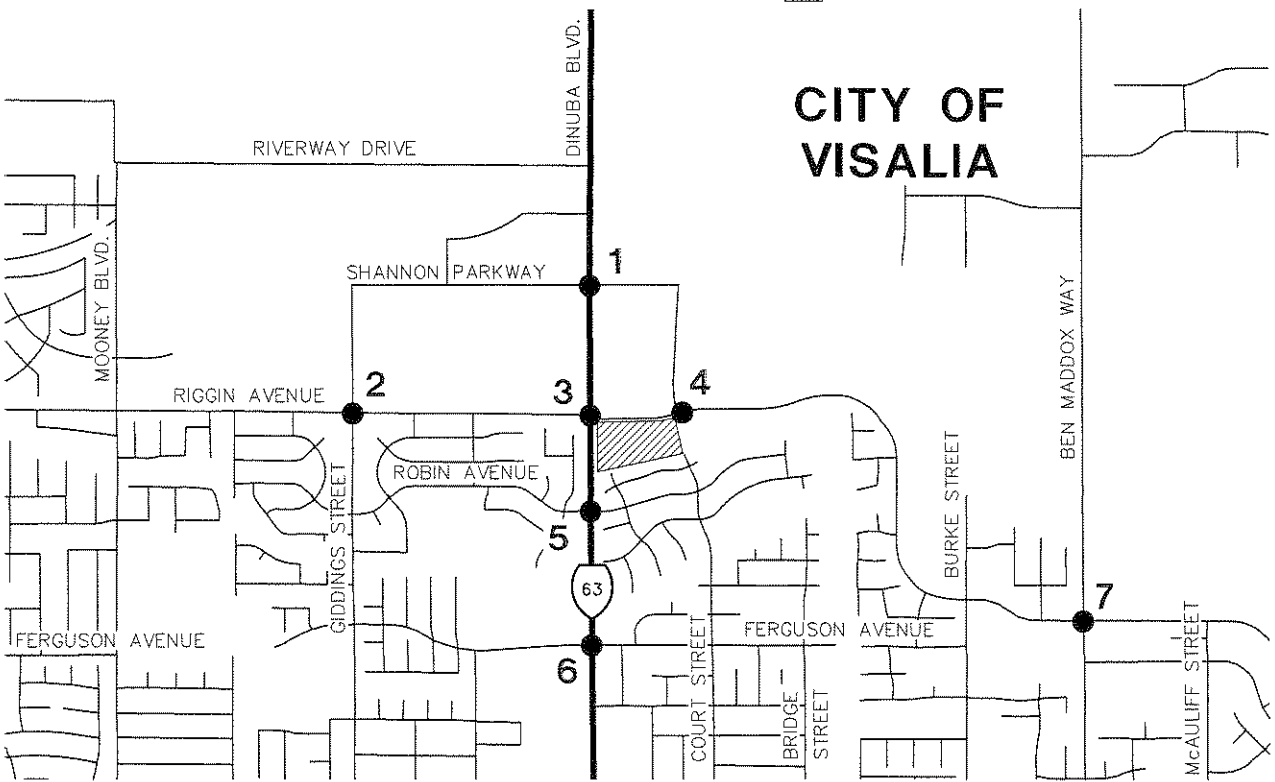
*Legend: TWSC = Two-Way-Stop Control. AWSC = All-Way-Stop Control.
Average Delay = Average Intersection Delay for Signalized Intersections.
Average Delay = Worst-Case Intersection Movement Delay for TWSC Intersections.
LOS = Average Intersection Level-of-Service for Signalized Intersections.
LOS = Worst-Case Movement's Level-of-Service for TWSC Intersections.
Warrant = MUTCD Peak-Hour Warrant-3.
OVRFL = Overflow conditions (> 100 seconds delay).*

As shown in Table 5, the intersections at Riggin Avenue/N. Giddings Street and Robin Avenue/State Route 63 are projected to operate at a LOS "E" or "F" conditions during AM and/or PM peak hour periods under "Existing plus Approved/Pending Projects" conditions scenario. In addition, the intersection at Riggin Avenue/N. Giddings Street meets the MUTCD Peak Hour Warrant 3 under "Existing plus Approved/Pending Projects" AM and PM peak hour conditions.

All mitigation measures are discussed in a subsequent section of this report.



LEGEND:
 XX - AM PEAK HOUR TRAFFIC VOLUMES
 (XX) - PM PEAK HOUR TRAFFIC VOLUMES
 [Hatched Box] - PROJECT LOCATION



Riverbend Commercial Center TIAR

Figure 4

Existing plus Approved/Pending Projects Traffic Volumes



PHASE I ACCESS

As identified in the introduction, the retail shopping development is located at the southeast corner of Riggin Avenue/Dinuba Boulevard in the City of Visalia. According to the site plan, this proposed project contains approximately 3.25 acres to be constructed under Phase I. This project proposes to have two “right turn only” driveways, one on State Route 63 south of Riggin Avenue and one on Riggin Avenue east of State Route 63. These driveways will be stop controlled (exiting the site) as said locations. Project driveways shall be constructed to city and state standards.

PHASE I TRIP GENERATION

Table 6 identifies the estimated trip generation of the project’s land-uses based upon data presented in *ITE Trip Generation* (8th Edition). For this project, trip generation rates for ITE land use codes 934 (fast food with drive-thru), 946 (gas station with mini-mart and car wash) and 710 (general office building) were applied to obtain the project trips contained in Table 6. It should be noted the independent variable used to determine the number of project trips is determined solely by the maximum number of vehicles that can be fueled simultaneously. As indicated in Table 6, this project is estimated to generate 1,965 daily trips, including 170 AM peak hour trips and 165 PM peak hour trips for the Phase I scenario.

**TABLE 6
PHASE I TRIP GENERATION**

Land Use Category	Unit	Daily Trip Rate/ Unit	Weekday AM Peak Hour Rate/Unit			Weekday PM Peak Hour Rate/Unit		
			Total	In %	Out %	Total	In %	Out %
Gas Station w/ mini-mart and car wash [ITE Code 946]	Fueling Stations	152.84	11.93	51%	49%	13.94	51%	49%
Fast Food with Drive-Thru [ITE Code 934]	Per 1,000 sq. ft.	496.12	49.35	51%	49%	33.84	52%	48%
General Office Building [ITE Code 710]	Per 1,000 sq. ft.	11.01	1.55	88%	12%	1.49	17%	83%
Description	Quantity (Units)	Daily Trips	Weekday AM Peak Hour Trips			Weekday PM Peak Hour Trips		
			Total	In	Out	Total	In	Out
Gas Station with mini-mart and car wash [ITE Code 946]	16 Fueling Stations	2,445	191	97	94	223	114	109
Fast Food with Drive-Thru [ITE Code 934]	3,202 sq. ft.	1,589	158	81	77	108	56	52
General Office Building [ITE Code 710]	1,750 sq. ft.	19	3	2	1	3	1	2
49% Pass-by Reduction (Gas Station w/ Mini-Mart)		(1,198)	(94)	(48)	(46)	(109)	(56)	(53)
56% Pass-by Reduction (Fast Food w/ Drive-Thru)		(890)	(88)	(45)	(43)	(60)	(31)	(29)
Total Trips		1,965	170	87	83	165	84	81

Note: Errors due to rounding may occur.

INTERNAL CAPTURE RATES

A basic premise behind the data presented in the *Trip Generation Manual* is that they were collected at single-use, free-standing sites. However, the development of mixed-use or multi-use sites is increasingly popular. While the trip generation rates for individual uses on such sites may be the same or similar to what they are for free standing sites, there is potential for interaction among those uses within the multi-use site, particularly where the trip can be made by walking. A common example of this internal trip-making occurs at a multi-use development containing two or more ITE use classifications between which trips can be made without using the off-site road system. As outlined in the *Trip Generation Handbook*, an internal capture rate can generally

be defined as a percentage reduction that can be applied to the trip generation estimates for individual land uses to account for trips internal to the site. All internal capture rates utilized in this technical memorandum were taken from the *ITE Trip Generation Handbook*.

PASS-BY TRIPS

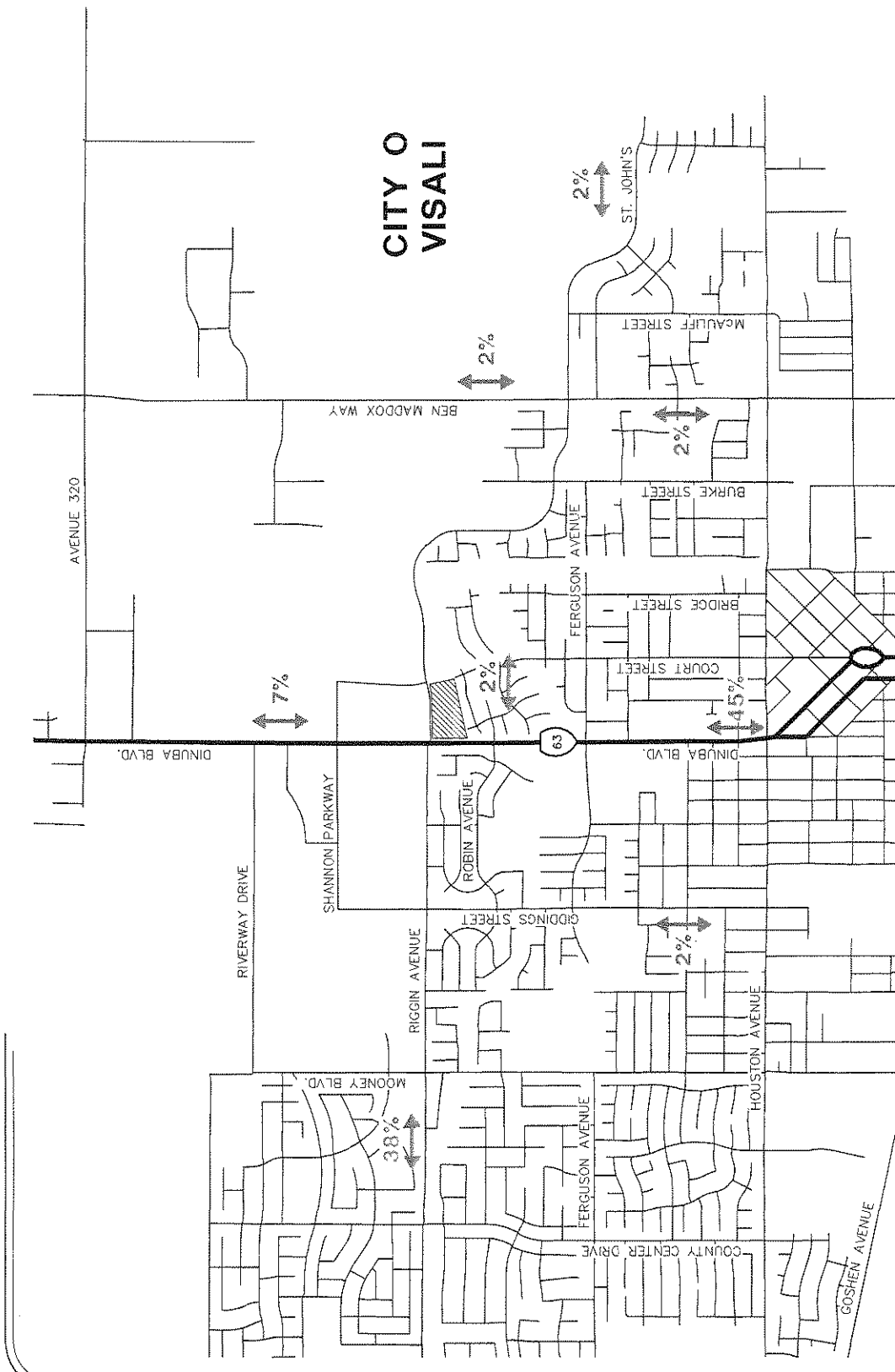
According to the *Trip Generation Handbook*, a pass-by trip is a “trip made as an intermediate stop on the way from an origin to a primary trip destination without a route diversion. Pass-by trips are attracted from traffic passing the site on an adjacent street or roadway that offers direct access to the generator.” Generally, pass-by trips are only generated at retail oriented and commercial developments adjacent to a busy street. It is important to note that the pass-by reduction is not subtracted from the trip generation at the project driveways, but is applied to the adjacent roadway network.

For this project a pass-by trip reduction factor of 49% is assumed for trips utilizing Dinuba Boulevard (State Route 63) for the gas station and 56% for fast food land uses based upon studies identified in the *ITE Trip Generation Handbook*. These pass by trips are still accounted for the main project driveway and are analyzed in the report. It should be noted that the pass-by trips were only applied at intersection #9 (Project Driveway #2/State Route 63). Additional documentation regarding pass-by trips is included in the Appendix and reference Tables 5.9 and 5.13 in the *ITE Trip Generation Handbook* (March 2001). In addition, Figures 5.9 and 5.10 are also included in the Appendix.

PHASE I TRIP NATURE, DISTRIBUTION, AND ASSIGNMENT

Phase I of the project is expected to “generate” and “attract” trips throughout the City and from other locations throughout the area. Directional trip distribution for project generated trips was estimated based upon existing traffic flow patterns, geographic location of the project sites, and location of other similar destinations. The result in trip distribution of Phase I trips throughout the study area are shown on Figure 5 and are identified below:

- 7% to/from Dinuba Boulevard north of Riggin Avenue;
- 45% to/from Dinuba Boulevard south of Riggin Avenue;
- 38% to/from Riggin Avenue west of State Route 63;
- 2% to/from Giddings Street south of Riggin Avenue;
- 2% to/from Robin Street/Court Street east of State Route 63;
- 2% to/from St. John’s Parkway east of Ben Maddox Way;
- 2% to/from northbound Ben Maddox Way via St. John’s Parkway; and
- 2% to/from southbound Ben Maddox Way via St. John’s Parkway.



Riverbend Commercial Center TIAR

Phase I Trip Distribution

EXISTING PLUS APPROVED/PENDING PROJECTS PLUS PHASE I CONDITIONS

“Existing plus Approved/Pending Projects plus Phase I” peak-hour intersection operations were quantified utilizing the proposed lane geometrics and control identified in Figure 7. Under this scenario, it has also been assumed that northbound State Route 63 will construct a right turn only driveway between Robin Avenue and Riggan Avenue (intersection # 9) and that Riggan Avenue will also construct a right turn only driveway between State Route 63 and Court Street (intersection # 8) along this corridor.

Applying *Traffic 8.0* computer software, “Existing plus Approved/Pending Projects plus Phase I” peak hour conditions were simulated by superimposing new trips generated over “Existing plus Approved/Pending Projects” traffic at the study intersections. The resulting “Existing plus Approved/Pending Projects plus Phase I” peak hour intersection traffic volumes are shown on Figure 6. Table 7 presents the resulting peak hour intersection LOS.

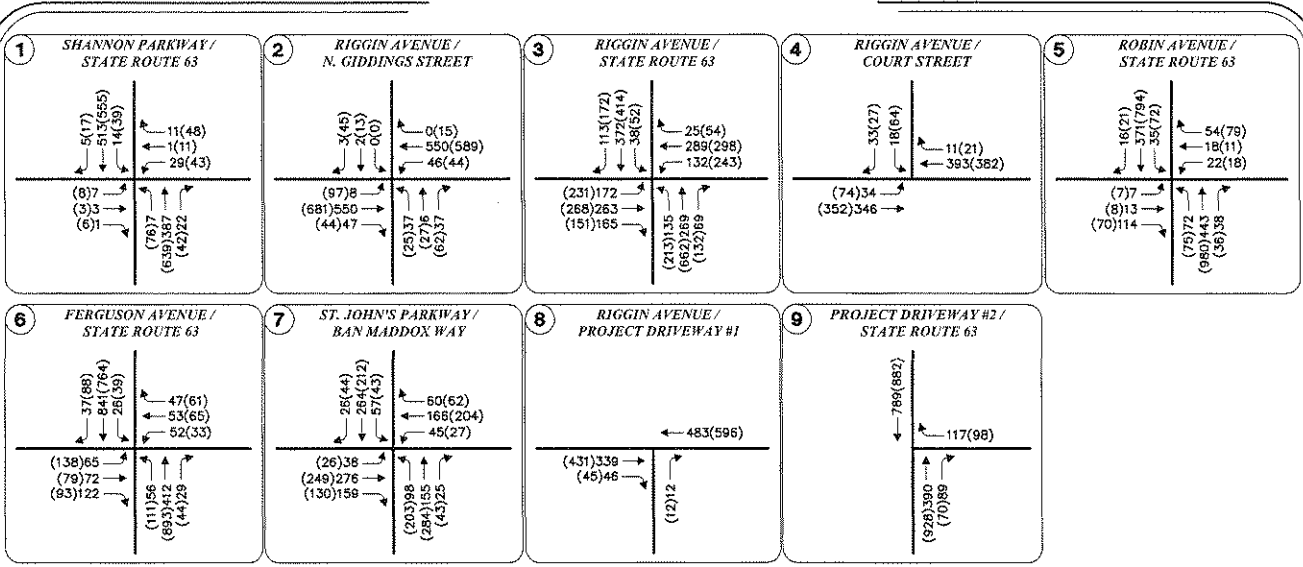
**TABLE 7
EXISTING PLUS APPROVED/PENDING PROJECTS PLUS PHASE I CONDITIONS:
INTERSECTION LEVELS-OF-SERVICE**

No	Intersection	Control Type	AM Peak Hour			PM Peak Hour		
			Delay (sec/veh)	LOS	Warrant Met?	Delay (sec/veh)	LOS	Warrant Met?
1	Shannon Parkway/State Route 63	Signal	18.7	B	--	11.8	B	--
2	Riggan Avenue/N. Giddings Street	TWSC	39.7	E	No	OVRFL	F	Yes
3	Riggan Avenue/State Route 63	Signal	23.0	C	--	39.5	D	--
4	Riggan Avenue/Court Street	TWSC	11.8	B	No	16.8	C	No
5	Robin Avenue/State Route 63	TWSC	50.7	F	Yes	OVRFL	F	No
6	Ferguson Avenue/State Route 63	Signal	29.9	C	--	23.1	C	--
7	St. John's Parkway/Ben Maddox Way	Signal	28.8	C	--	28.7	C	--
8	Riggan Avenue/Project Driveway #1	TWSC	9.6	A	No	10.0	A	No
9	Project Driveway #2/State Route 63	TWSC	10.5	B	No	14.0	C	No

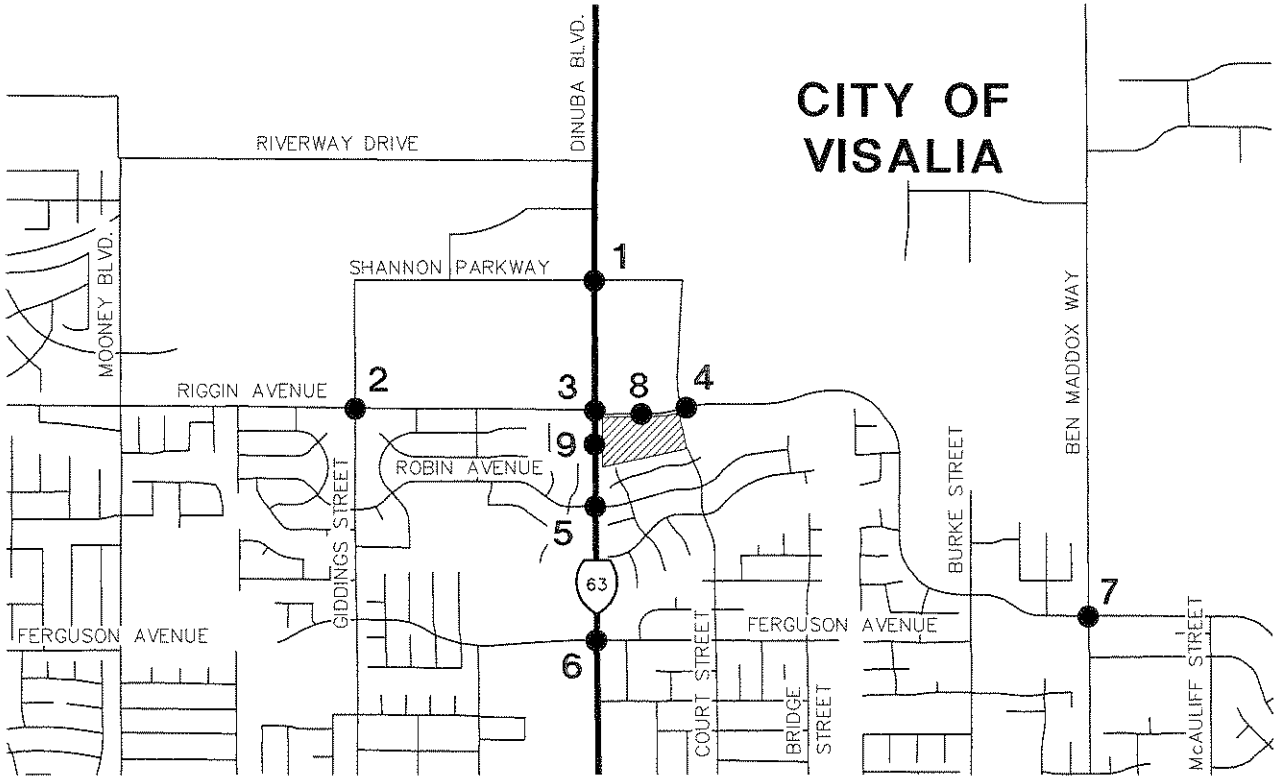
*Legend: TWSC = Two-Way-Stop Control. AWSC = All-Way Stop Control.
Average Delay = Average Intersection Delay for Signalized Intersections.
Average Delay = Worst-Case Intersection Movement Delay for TWSC Intersections.
LOS = Average Intersection Level-of-Service for Signalized Intersections.
LOS = Worst-Case Movement's Level-of-Service for TWSC Intersections.
Warrant = MUTCD Peak-Hour Warrant-3.
OVRFL = Overflow conditions (> 100 seconds delay).*

As shown in Table 7, the intersections at Riggan Avenue/N. Giddings Street and Robin Avenue/State Route 63 are projected to operate at a LOS “F” conditions during AM and PM peak hour periods under “Existing plus Approved/Pending Projects plus Phase I” conditions scenario. In addition, the intersections at Riggan Avenue/N. Giddings Street and Robin Avenue/State Route 63 meet the MUTCD Peak Hour Warrant 3 under “Existing plus Approved/Pending Projects plus Phase I” AM and/or PM peak hour conditions.

All mitigation measures are discussed in a subsequent section of this report.



LEGEND:
 XX - AM PEAK HOUR TRAFFIC VOLUMES
 (XX) - PM PEAK HOUR TRAFFIC VOLUMES
 [Hatched Box] - PROJECT LOCATION

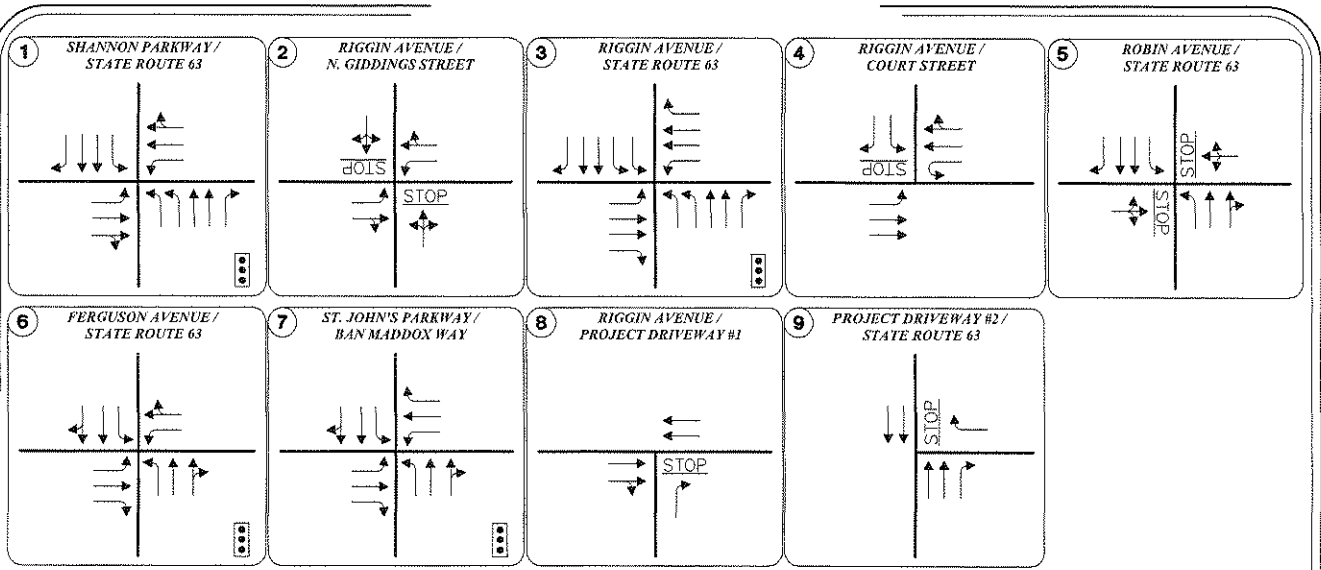


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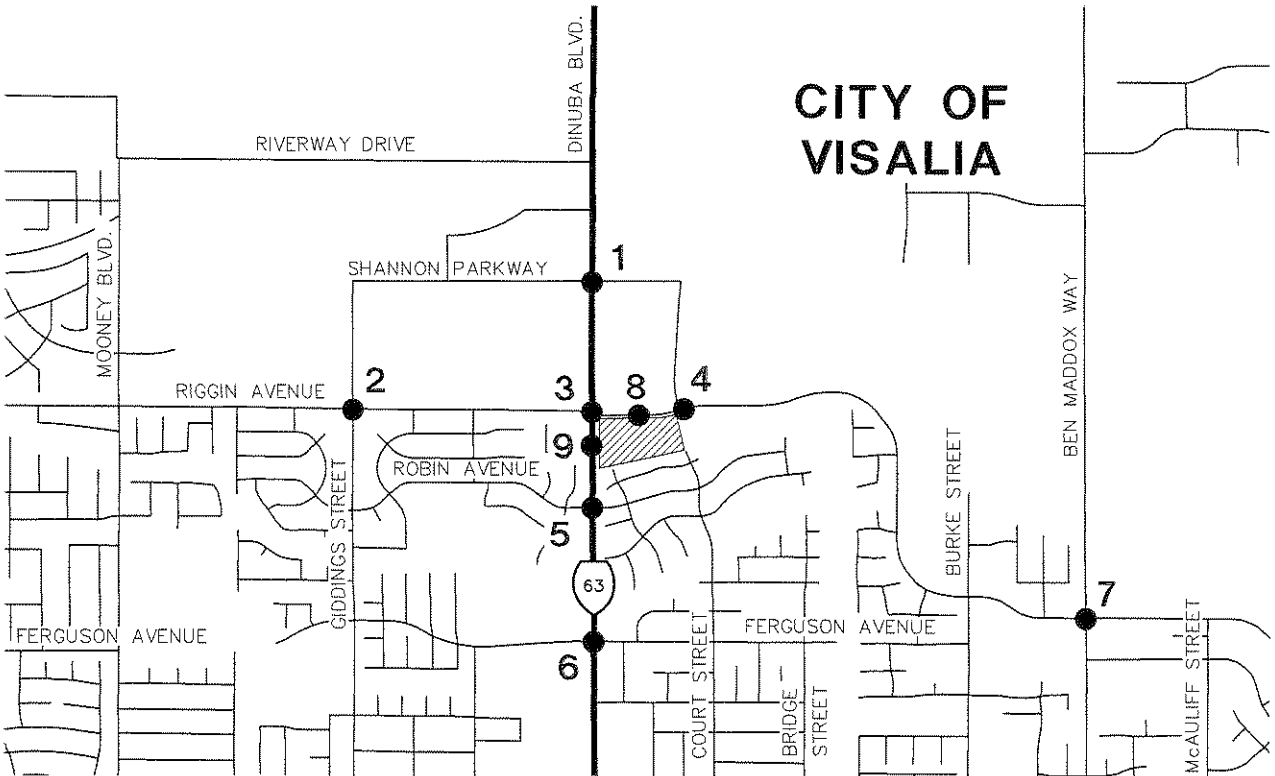
Figure 6

Existing plus Approved/Pending
 Projects plus Phase I Traffic Volumes





LEGEND:
 - PROJECT LOCATION



Riverbend Commercial Center TIAR

Figure 7

Existing plus Approved/Pending Projects
 plus Phase I Lane Geometrics and Control



Vehicle queues by approach and by approach movement were also evaluated at each of the study intersections with the results shown in Table 8. As indicated in Table 8, the available storage currently accommodates the 95th percentile queue storage requirements for all intersection movements except for the westbound left turn at Riggan Avenue/State Route 63 and the eastbound left at Ferguson Avenue/State Route 63 during the AM & PM peak hours.

**TABLE 8
EXISTING PLUS APPROVED/PENDING PROJECTS PLUS PHASE I CONDITIONS:
INTERSECTION QUEUING ANALYSIS**

Int. #	Queue Segment - Direction	# Lanes	Total Storage (Ft.)	AM Peak Hour 95th % Queue	PM Peak Hour 95th % Queue
1	<i>Shannon Parkway/State Route 63</i>				
	Eastbound Left	1	300	20	25
	Westbound Left	1	240	50	70
	Northbound Left	2	340	15	55
	Northbound Right	1	260	20	25
	Southbound Left	1	300	30	65
	Southbound Right	1	260	15	20
3	<i>Riggan Avenue/State Route 63</i>				
	Eastbound Left	1	270	255	270
	Eastbound Right	1	300	45	45
	Westbound Left	1	280	210	390
	Westbound Right	1	300	20	25
	Northbound Left	2	740	80	145
	Northbound Right	1	370	35	45
	Southbound Left	2	750	30	40
Southbound Right	1	325	45	50	
5	<i>Robin Avenue/State Route 63</i>				
	Eastbound Left/Thru/Right	1	230	60	105
	Westbound Thru/Left	1	330	80	225
	Northbound Left	1	140	15	15
Southbound Left	1	320	15	15	
6	<i>Ferguson Avenue/State Route 63</i>				
	Eastbound Left	1	80	295	190
	Eastbound Right	1	80	40	35
	Westbound Left	1	125	70	50
	Northbound Left	1	200	35	155
	Southbound Left	1	100	15	55

FUTURE CONDITIONS

GENERAL

Under Year 2035 conditions, two scenarios were analyzed. Under the first scenario, herein called the “Year 2035 Base” scenario, it is assumed that the City will continue to develop but the project development will not occur. The second scenario, identified as “Year 2035 Base plus Project,” assumes that development of the entire Project will occur. Both scenarios assume that no improvements have been made to the study intersections or roadways; therefore, “Existing” lane geometrics and control are used in the analysis under conditions with and without project. This enables the City and Caltrans to identify future project impacts to the study intersections.

Year 2035 AM and PM daily traffic forecasts were provided by the TCAG. OMNI-MEANS worked with TCAG staff to develop future year (2035) traffic volumes utilizing the TCAG Regional Travel Demand Forecast Model, which uses *Cube* software. Although TCAG has a peak hour model, it was not utilized for the future analysis. OMNI-MEANS used the daily directional traffic counts at each leg of the intersection to balance the turning movement counts. The turning movement counts were computed using techniques provided in National Cooperative Highway Research Program (NCHRP 255) through the use of TurnsW32 computer application. Based upon future trip “ins” and “outs” for each leg of the intersection, TurnsW32 runs several iterations to calculate future daily traffic volumes by turning movement. Following this process, OMNI-MEANS checked the forecasted turning movements for reasonableness and made adjustments where necessary.

YEAR 2035 BASE CONDITIONS

“Year 2035 Base” peak-hour intersection traffic operations were quantified applying “Year 2035 Base” traffic volumes shown on Figure 8 and “Year 2035 Base” intersection lane geometrics and control identified on Figure 9. Table 9 presents the “Year 2035 Base” peak hour intersection LOS.

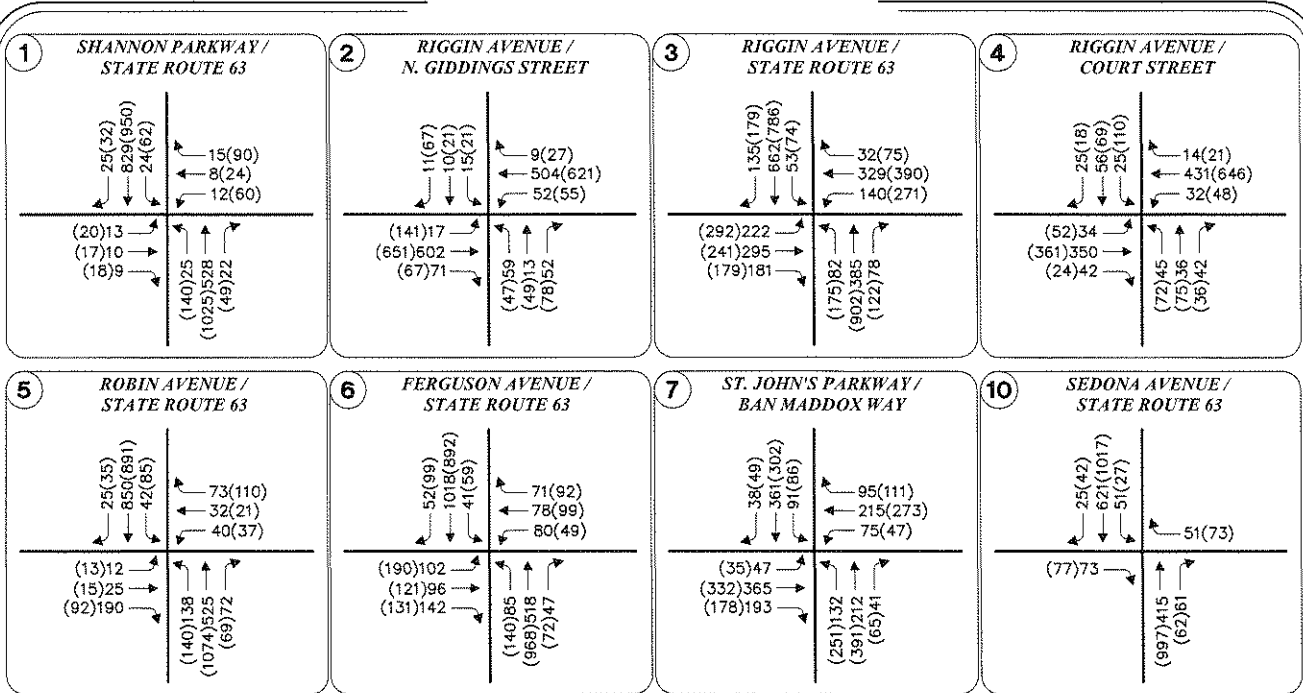
**TABLE 9
YEAR 2035 BASE CONDITIONS:
INTERSECTION LEVELS-OF-SERVICE**

No	Intersection	Control Type	AM Peak Hour			PM Peak Hour		
			Delay (sec/veh)	LOS	Warrant Met?	Delay (sec/veh)	LOS	Warrant Met?
1	Shannon Parkway/State Route 63	Signal	9.5	A	--	18.6	B	--
2	Riggin Avenue/N. Giddings Street	TWSC	OVRFL	F	No	OVRFL	F	Yes
3	Riggin Avenue/State Route 63	Signal	26.4	C	--	36.5	C	--
4	Riggin Avenue/Court Street	TWSC	24.8	C	No	OVRFL	F	No
5	Robin Avenue/State Route 63	TWSC	OVRFL	F	Yes	OVRFL	F	Yes
6	Ferguson Avenue/State Route 63	Signal	21.4	C	--	33.3	C	--
7	St. John's Parkway/Ben Maddox Way	Signal	30.9	C	--	31.2	C	--
10	Sedona Avenue/State Route 63	TWSC	10.3	B	No	9.5	B	No

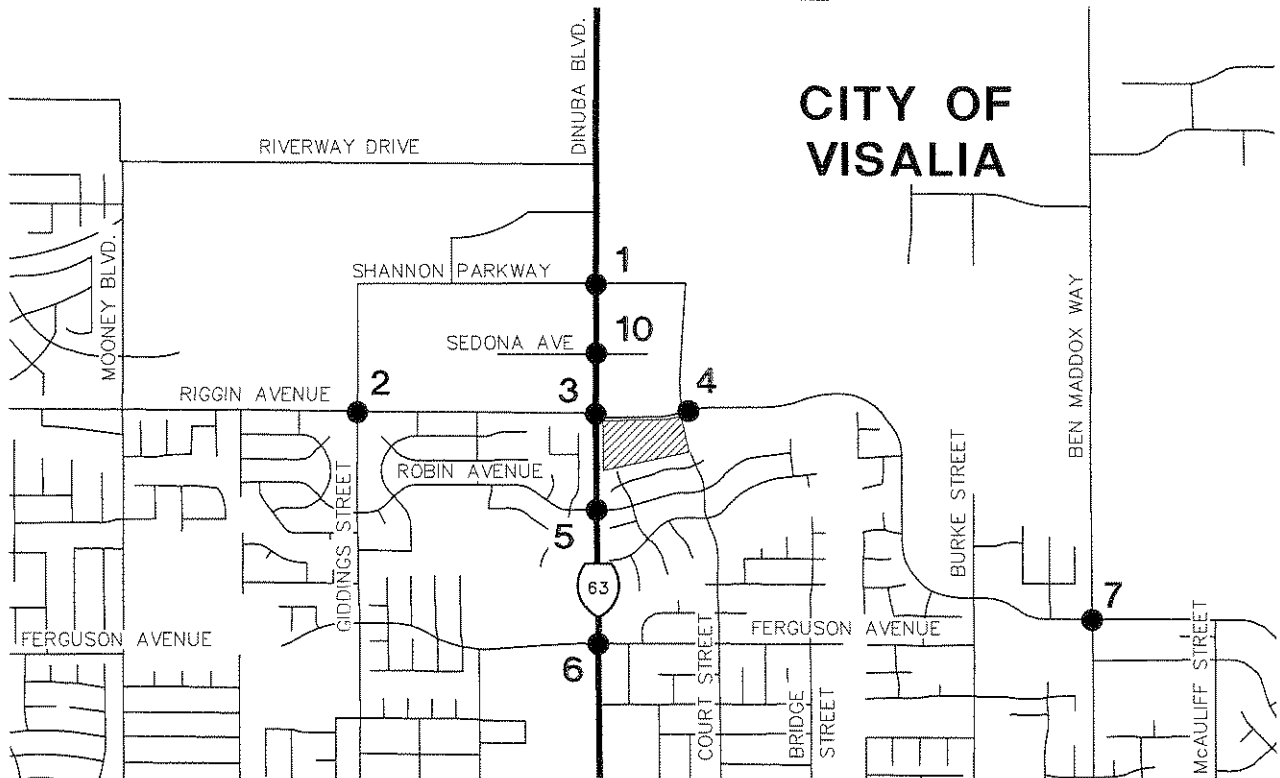
*Legend: TWSC = Two-Way-Stop Control. AWSC = All-Way Stop Control.
Average Delay = Average Intersection Delay for Signalized Intersections.
Average Delay = Worst-Case Intersection Movement Delay for TWSC Intersections.
LOS = Average Intersection Level-of-Service for Signalized Intersections.
LOS = Worst-Case Movement's Level-of-Service for TWSC Intersections.
Warrant = MUTCD Peak-Hour Warrant-3.
OVRFL = Overflow conditions (> 100 seconds delay).*

As shown in Table 9, the intersections at Riggin Avenue/N. Giddings Street, Riggin Avenue/Court Street and Robin Avenue/State Route 63 are projected to operate at peak hour LOS "F" conditions under AM and/or PM peak hour periods for "Year 2035 Base" conditions. In addition, the unsignalized intersections of Riggin Avenue/N. Giddings Street and Robin Avenue/State Route 63 are projected to meet the MUTCD Peak Hour Warrant 3 under "Year 2035 Base" AM and/or PM peak hour volume conditions.

All mitigation measures are discussed in a subsequent section of this report.



LEGEND:
 XX - AM PEAK HOUR TRAFFIC VOLUMES
 (XX) - PM PEAK HOUR TRAFFIC VOLUMES
 [Hatched Box] - PROJECT LOCATION

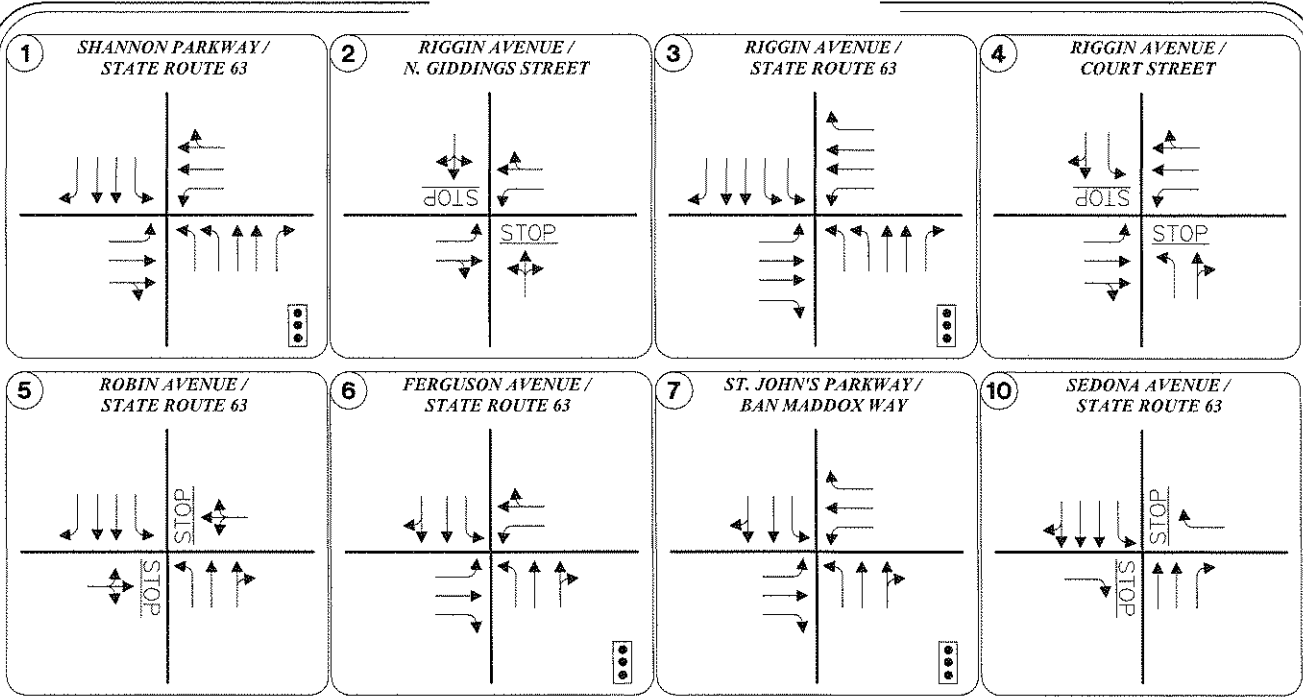


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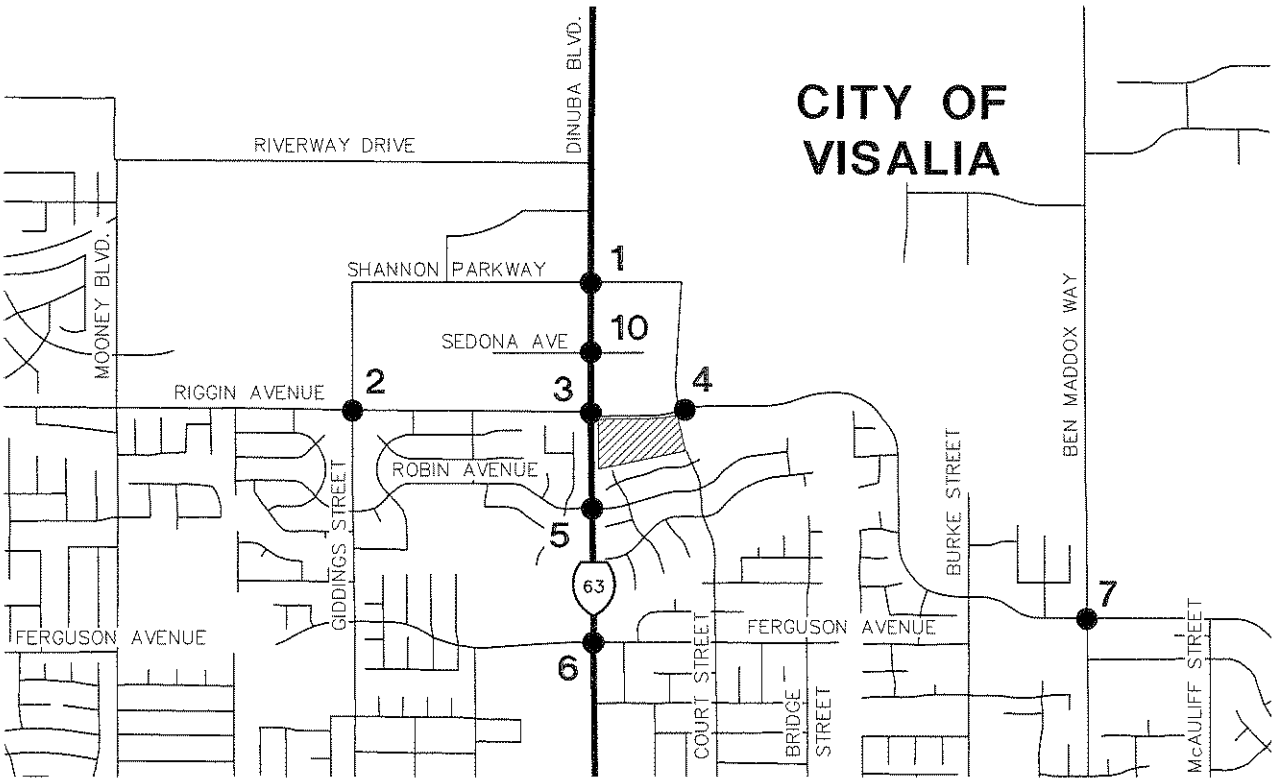
Figure 8

Year 2035 Base Traffic Volumes





LEGEND:
 - PROJECT LOCATION



Riverbend Commercial Center TIAR

Figure 9

Year 2035 Base Lane Geometrics and Control



Vehicle queues by approach and by approach movement were also evaluated at each of the study intersections with the results shown in Table 10. As indicated in Table 10, the available storage currently accommodates the 95th percentile queue storage requirements for all intersection movements except for the westbound left turn at Riggins Avenue/State Route 63 and the eastbound left at Ferguson Avenue/State Route 63 during the AM & PM peak hours.

**TABLE 10
YEAR 2035 BASE CONDITIONS:
INTERSECTION QUEUING ANALYSIS**

Int. #	Queue Segment - Direction	# Lanes	Total Storage (Ft.)	AM Peak Hour 95th % Queue	PM Peak Hour 95th % Queue
<i>1</i>	<i>Shannon Parkway/State Route 63</i>				
	Eastbound Left	1	300	30	45
	Westbound Left	1	240	30	100
	Northbound Left	2	340	25	95
	Northbound Right	1	260	20	35
	Southbound Left	1	300	50	105
	Southbound Right	1	260	20	25
<i>3</i>	<i>Riggins Avenue/State Route 63</i>				
	Eastbound Left	1	270	360	410
	Eastbound Right	1	300	50	55
	Westbound Left	1	280	220	380
	Westbound Right	1	300	25	35
	Northbound Left	2	740	55	125
	Northbound Right	1	370	35	50
	Southbound Left	2	750	40	55
Southbound Right	1	325	50	55	
<i>5</i>	<i>Robin Avenue/State Route 63</i>				
	Eastbound Left/Thru/Right	1	230	375	500+
	Westbound Left/Thru/Right	1	330	400	500+
	Northbound Left	1	140	15	15
	Southbound Left	1	320	15	15
<i>6</i>	<i>Ferguson Avenue/State Route 63</i>				
	Eastbound Left	1	80	135	265
	Eastbound Right	1	80	45	45
	Westbound Left	1	125	95	70
	Northbound Left	1	200	105	200
	Southbound Left	1	100	55	80
<i>10</i>	<i>Sedona Avenue/State Route 63</i>				
	Eastbound Right	1	150	15	15
	Westbound Right	1	150	15	15
	Northbound Right	1	350	15	15
	Southbound Left	1	350	15	15

YEAR 2035 TRIP GENERATION

Table 11 identifies the estimated trip generation of the project's land-uses based upon data presented in *ITE Trip Generation* (8th Edition). For this project, trip generation rates for ITE land use codes 934 (fast food with drive-thru), 946 (gas station with mini-mart and car wash) and 820 (shopping center), that is proposed to include retail, restaurant/coffee kiosk and office uses were applied to obtain the project trips contained in Table 11. It should be noted the independent variable used to determine the number of project trips is determined solely by the maximum number of vehicles that can be fueled simultaneously. As indicated in Table 11, this project is estimated to generate 3,940 daily trips, including 216 AM peak hour trips and 337 PM peak hour trips for the "Year 2035 Base plus Project".

**TABLE 11
YEAR 2035 PROJECT TRIP GENERATION**

Land Use Category	Unit	Daily Trip Rate/Unit	Weekday AM Peak Hour Rate/Unit			Weekday PM Peak Hour Rate/Unit		
			Total	In %	Out %	Total	In %	Out %
Gas Station w/ mini-mart and car wash [ITE Code 946]	Fueling Stations	152.84	11.93	51%	49%	13.94	51%	49%
Fast Food with Drive-Thru [ITE Code 934]	Per 1,000 sq. ft.	496.12	49.35	51%	49%	33.84	52%	48%
General Office Building [ITE Code 710]	Per 1,000 sq. ft.	11.01	1.55	88%	12%	1.49	17%	83%
Shopping Center [ITE Code 820]	Per 1,000 ft ²	42.94	1.00	61%	39%	3.73	49%	51%
Description	Quantity (Units)	Daily Trips	Weekday AM Peak Hour Trips			Weekday PM Peak Hour Trips		
Gas Station with mini-mart and car wash [ITE Code 946]	16 Fueling Stations	2,445	Total	In	Out	Total	In	Out
Fast Food with Drive-Thru [ITE Code 934]	3,202 sq. ft.	1,589	191	97	94	223	114	109
General Office Building [ITE Code 710]	1,750 sq. ft.	19	158	81	77	108	56	52
Shopping Center [ITE Code 820]	46,000 sq. ft.	1,975	3	2	1	3	1	2
49% Pass-by Reduction (Gas Station w/ Mini-Mart)		(1,198)	(94)	(48)	(46)	(109)	(56)	(53)
56% Pass-by Reduction (Fast Food w/ Drive-Thru)		(890)	(88)	(45)	(43)	(60)	(31)	(29)
Total Trips		3,940	216	127	89	337	113	224

YEAR 2035 BASE PLUS PROJECT CONDITIONS

"Year 2035 Base plus Project" peak-hour intersection traffic operations were quantified applying "Year 2035 Base plus Project" traffic volumes shown on Figure 10 and "Year 2035 Base plus Project" intersection lane geometrics and control shown on Figure 11. Table 12 presents the "Year 2035 Base plus Project" peak hour intersection LOS.

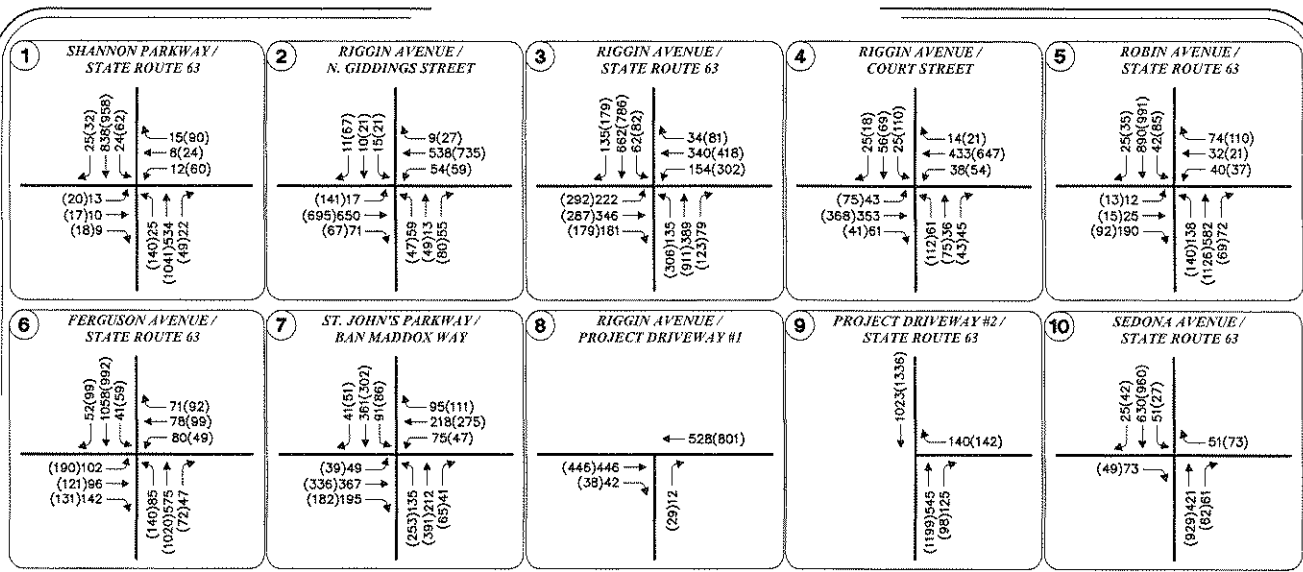
**TABLE 12
YEAR 2035 BASE PLUS PROJECT CONDITIONS:
INTERSECTION LEVELS-OF-SERVICE**

No	Intersection	Control Type	AM Peak Hour			PM Peak Hour		
			Delay (sec/veh)	LOS	Warrant Met?	Delay (sec/veh)	LOS	Warrant Met?
1	Shannon Parkway/State Route 63	Signal	9.5	A	--	18.5	B	--
2	Riggin Avenue/N. Giddings Street	TWSC	OVRFL	F	No	OVRFL	F	Yes
3	Riggin Avenue/State Route 63	Signal	29.7	C	--	42.7	D	--
4	Riggin Avenue/Court Street	TWSC	27.8	D	No	OVRFL	F	Yes
5	Robin Avenue/State Route 63	TWSC	OVRFL	F	Yes	OVRFL	F	Yes
6	Ferguson Avenue/State Route 63	Signal	21.6	C	--	35.5	D	--
7	St. John's Parkway/Ben Maddox Way	Signal	31.0	C	--	31.3	C	--
8	Riggin Avenue/Project Driveway #1	TWSC	10.0	B	No	10.1	B	No
9	Project Driveway #2/State Route 63	TWSC	11.6	B	No	19.2	C	No
10	Sedona Avenue/State Route 63	TWSC	10.4	B	No	9.5	A	No

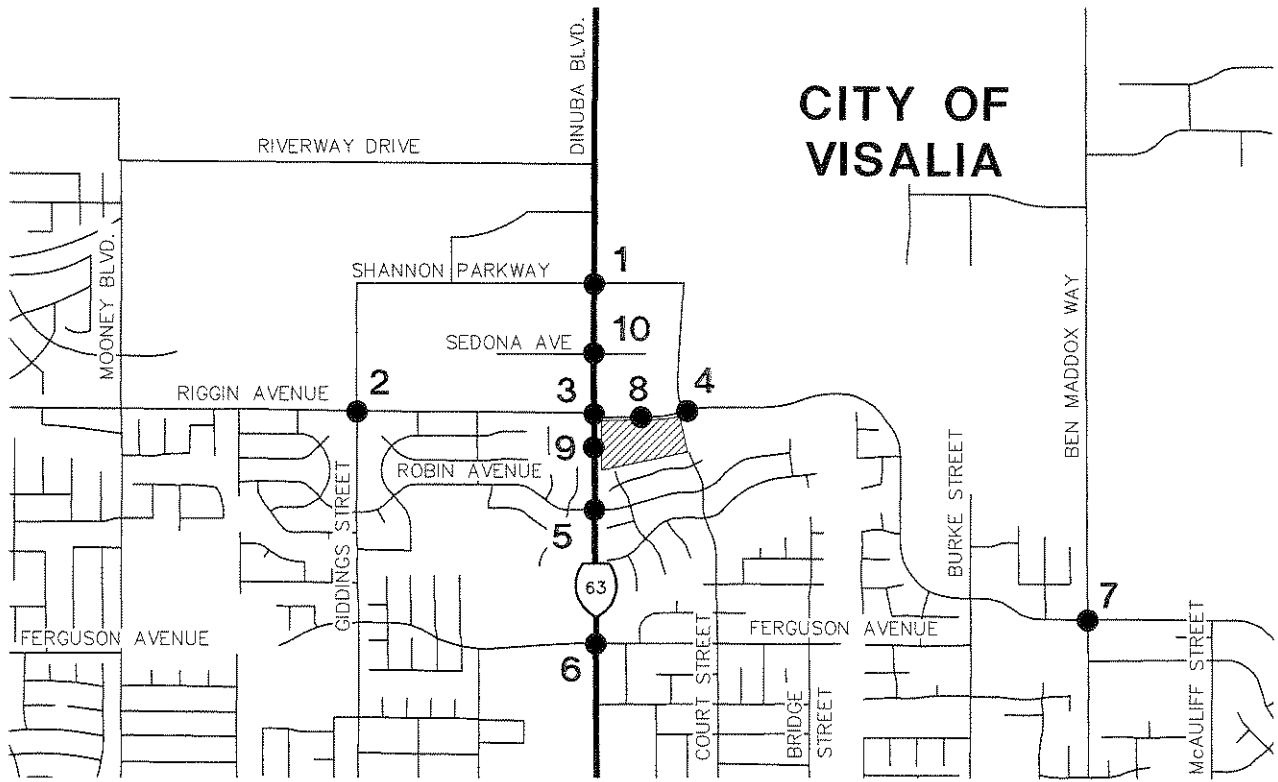
*Legend: TWSC = Two-Way-Stop Control. AWSC = All-Way Stop Control.
Average Delay = Average Intersection Delay for Signalized Intersections.
Average Delay = Worst-Case Intersection Movement Delay for TWSC Intersections.
LOS = Average Intersection Level-of-Service for Signalized Intersections.
LOS = Worst-Case Movement's Level-of-Service for TWSC Intersections.
Warrant = MUTCD Peak-Hour Warrant-3.
OVRFL = Overflow conditions (> 100 seconds delay).*

As shown in Table 12, the intersections at Riggin Avenue/N. Giddings Street, Riggin Avenue/Court Street and Robin Avenue/State Route 63 are projected to operate at LOS "F" conditions under AM and/or PM peak hour periods for "Year 2035 Base plus Project" conditions. In addition, the unsignalized intersections of Riggin Avenue/N. Giddings Street, Riggin Avenue/Court Street and Robin Avenue/State Route 63 are projected to meet the MUTCD Peak Hour Warrant 3 under projected "Year 2035 Base plus Project" AM and PM peak hour volume conditions.

All mitigation measures are discussed in the following section of this report.



LEGEND:
 XX - AM PEAK HOUR TRAFFIC VOLUMES
 (XX) - PM PEAK HOUR TRAFFIC VOLUMES
 [Hatched Box] - PROJECT LOCATION

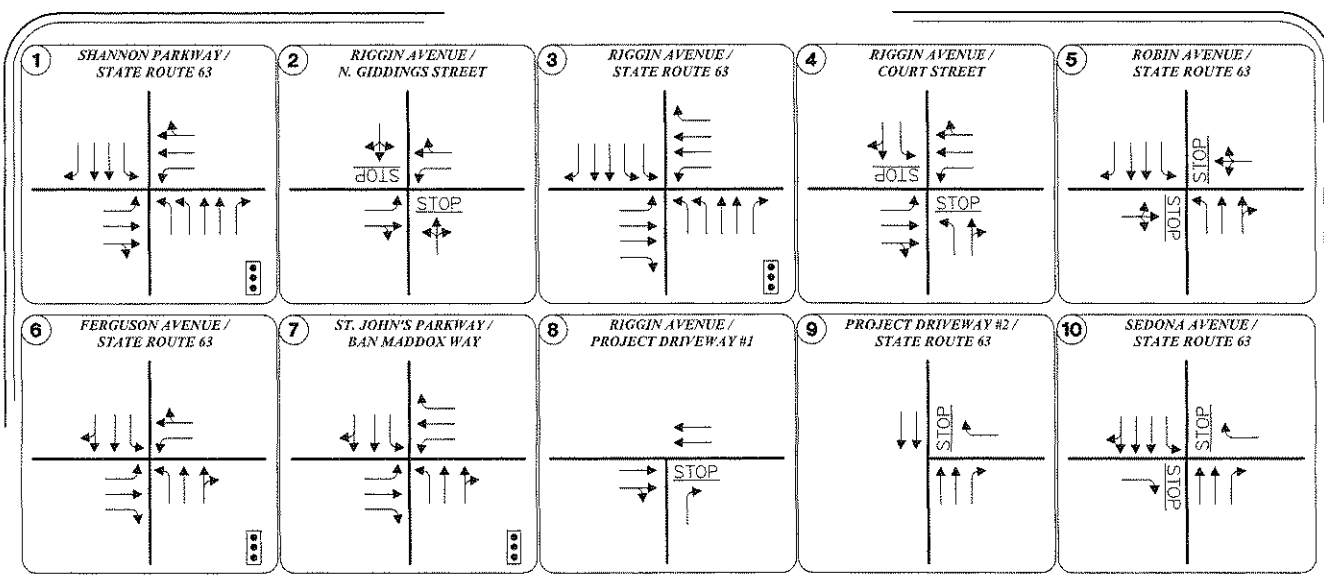


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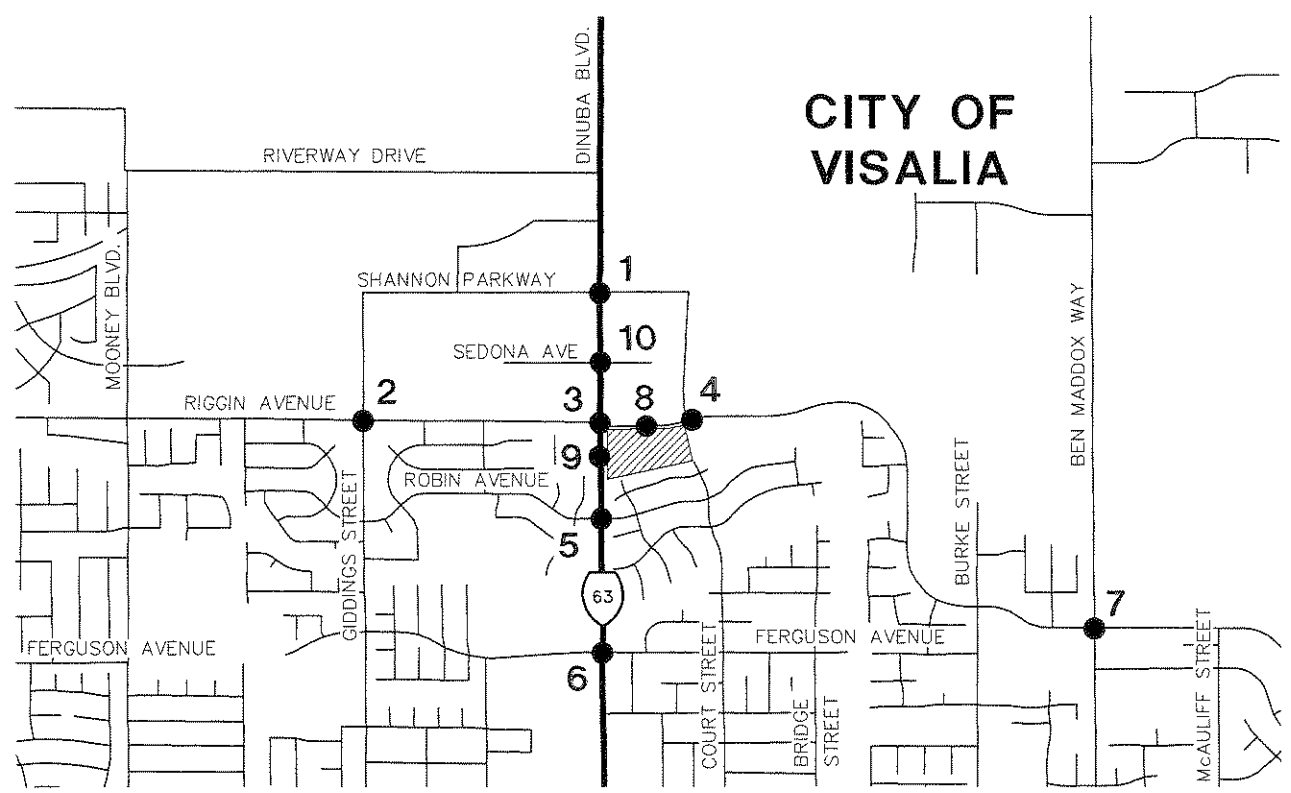
Figure 10

Year 2035 Base plus Project Traffic Volumes





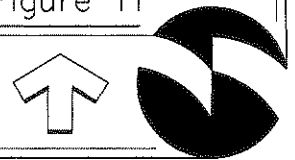
LEGEND:
 - PROJECT LOCATION



Riverbend Commercial Center TIAR

Figure 11

Year 2035 Base plus Project Lane Geometrics and Control



Vehicle queues by approach and by approach movement were also evaluated at each of the study intersections with the results shown in Table 13. As indicated in Table 13, the available storage currently accommodates the 95th percentile queue storage requirements for all intersection movements except for the eastbound/westbound left turn lanes at Riggin Avenue/State Route 63, eastbound/westbound approaches at Robin Avenue/State Route 63, and the northbound and southbound left turn lanes at Ferguson Avenue/State Route 63 during the “Year 2035 Base plus Project” AM & PM peak hours.

**TABLE 13
YEAR 2035 BASE PLUS PROJECT CONDITIONS:
INTERSECTION QUEUING ANALYSIS**

Int. #	Queue Segment - Direction	# Lanes	Total Storage (Ft.)	AM Peak Hour 95th % Queue	PM Peak Hour 95th % Queue
1	<i>Shannon Parkway/State Route 63</i>				
	Eastbound Left	1	300	30	45
	Westbound Left	1	240	30	100
	Northbound Left	2	340	25	95
	Northbound Right	1	260	25	35
	Southbound Left	1	300	50	105
	Southbound Right	1	260	20	25
3	<i>Riggin Avenue/State Route 63</i>				
	Eastbound Left	1	270	360	450
	Eastbound Right	1	300	50	55
	Westbound Left	1	280	235	450
	Westbound Right	1	300	25	40
	Northbound Left	2	740	80	235
	Northbound Right	1	370	35	45
	Southbound Left	2	750	45	60
Southbound Right	1	325	45	55	
5	<i>Robin Avenue/State Route 63</i>				
	Eastbound Left/Thru/Right	1	230	475	500+
	Westbound Thru/Left	1	330	500+	500+
	Northbound Left	1	140	25	25
Southbound Left	1	320	25	25	
6	<i>Ferguson Avenue/State Route 63</i>				
	Eastbound Left	1	80	135	270
	Eastbound Right	1	80	45	45
	Westbound Left	1	125	95	70
	Northbound Left	1	200	105	195
Southbound Left	1	100	55	80	
10	<i>Sedona Avenue/State Route 63</i>				
	Eastbound Right	1	150	15	15
	Westbound Right	1	150	15	15
	Northbound Right	1	350	15	15
Southbound Left	1	350	15	15	

RECOMMENDED MITIGATION MEASURES

This section presents a list of recommended mitigation measures at the study intersections and roadways based upon the results of the analysis presented in this report. All of the study intersections are projected to operate at acceptable LOS “D” conditions or better through 2035 with implementation of the recommended mitigation measures identified below.

At the end of this section, Figure 12 identifies mitigated lane geometrics and control to achieve acceptable operating conditions at the study intersections. Because the mitigation measures are recommended for buildout in Year 2035 and generally do not provide an implementation year, the study intersections requiring mitigation to achieve acceptable LOS should be monitored on a regular basis by the City of Visalia and Caltrans. TCAG has an annual traffic monitoring program that could be used to collect LOS at these intersections.

EXISTING CONDITIONS

Under “Existing” the following mitigation measures are recommended:

Riggin Avenue/N. Giddings Street: Install a traffic signal and provide for northbound and southbound left turn channelization. This intersection currently operates at LOS “F” conditions during the PM peak hour. Implementation of these mitigation measures will result in LOS “B” operating conditions.

Robin Avenue/State Route 63: This intersection currently operates at LOS “E” conditions during the PM peak hour; however, this intersection does not meet the peak hour warrant because the minor street (Robin Avenue) approaches do not carry enough traffic volume to justify signalization. Therefore, it is recommended that the state and City of Visalia monitor this intersection in the future. A right turn only intersection with a raised median may be recommended for this intersection to achieve adequate levels of service. The state and City of Visalia should include this intersection in TCAG’s Annual Traffic Monitoring Program.

EXISTING PLUS APPROVED/PENDING PROJECTS CONDITIONS

Under “Existing plus Approved/Pending Project” conditions, it is assumed that mitigation measures recommended under “Existing” conditions have been implemented. Under “Existing plus Approved/Pending Projects, no additional mitigation measures are recommended.

EXISTING PLUS APPROVED/PENDING PROJECTS PLUS PHASE I CONDITIONS

Under “Existing plus Approved/Pending Projects plus Phase I” conditions, it is assumed that mitigation measures recommended under “Existing” conditions have been implemented. Under “Existing plus Approved/Pending Projects plus Phase I” conditions, the following mitigation measures have been identified:

Robin Avenue/State Route 63: This intersection is forecasted to exceed the queuing capacity for the westbound left turning movement. It is recommended that the project stripe the westbound approach to accommodate a left-thru lane and a dedicated right turn lane with a minimum of 300’.

Ferguson Avenue/State Route 63: This intersection is forecasted to exceed the queuing capacity for the eastbound left turning movements. It is recommended that the project restripe this movement to allow for additional eastbound left turning movements to be 300’.

Project Driveways: Based upon the site plan, it is assumed that Riggin Avenue/Intersection # 9 and State Route 63/Intersection # 8 provide for right turn only access into/out of the project site. The “right turn only”

driveways are recommended as a result of the close proximity to the adjacent intersection. This includes the major project driveway intersection on State Route 63 250' south of Riggan Avenue and a throat depth on the major project driveway of 80' per Caltrans July 17, 2011, letter to the city.

It is recommended that a raised median on State Route 63 be installed along the center median of the property line adjacent to and west of the proposed project that comply with city and state standards. The median is recommended to be constructed along State Route 63 approximately 450' in length from Riggan Avenue to the south as indicated in site plan as part of Phase I of the project.

YEAR 2035 BASE CONDITIONS

Under "Year 2035 Base" conditions, it is assumed that mitigation measures recommended under "Existing plus Approved/Pending Projects plus Phase I" conditions have been implemented. Under "Year 2035 Base" conditions, the following mitigation measures have been identified:

Riggan Avenue/Court Street: Install stop signs on the eastbound and westbound approaches along Riggan Avenue to have an all way stop controlled intersection. Implementation of these mitigation measures will result in acceptable LOS "C" operating conditions.

Robin Avenue/State Route 63: This intersection is forecasted to operate at unacceptable LOS "F" conditions and meets the Peak Hour Warrant during the AM and PM peak hours. Therefore, it is recommended that a raised median be installed on State Route 63 that would restrict access on the minor approaches. Based upon the analysis of future traffic data, a right turn only intersection is recommended for this intersection to achieve adequate levels of service. Because this is not a result of the proposed project, the City of Visalia and Caltrans are recommended to resolve this projected deficiency. Implementation of these mitigation measures will result in acceptable LOS "C" operating conditions.

YEAR 2035 BASE PLUS PROJECT CONDITIONS

Under "Year 2035 Base plus Project" conditions, it is assumed that mitigation measures recommended under "Year 2035 Base" conditions have been implemented. Under "Year 2035 Base plus Project" the following mitigation measures are recommended:

Court Street between Dove Avenue and Riggan Avenue: Extend this roadway during Phase II construction. This street shall be constructed to city standards.

PRO RATA SHARE CALCULATIONS

Table 14 identifies the pro rata share calculations as documented in the Caltrans *Guide for the Preparation of Traffic Impact Studies* (December 2002). The method for calculating equitable mitigation measures is as follows:

$$P=T/(T_B - T_E)$$

Where:

P = The equitable share for the proposed project's traffic impact.

T = The vehicle trips generated by the project during the peak hour of adjacent State highway facility in vehicles per hour (vph).

T_B = The forecasted traffic volume on a impacted State highway facility at the time of general plan build-out (e.g., 20 year model or the furthest future model date feasible), vph.

T_E = The traffic volume existing on the impacted State highway facility plus other approved projects that will generate traffic that has yet to be constructed/opened, vph.

**TABLE 14
PRO RATA SHARE CALCULATIONS**

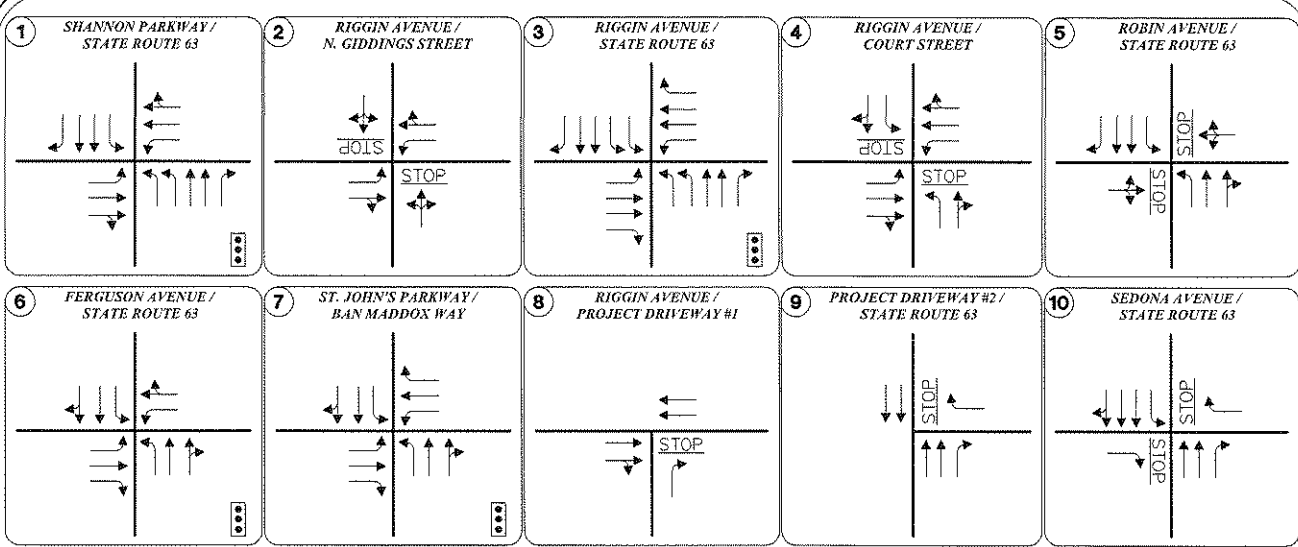
Intersection	Existing + Approved/Pending	2035 General Plan Buildout	Project Trips	Pro Rata %
Riggin Avenue/Giddings Street	1,037	2,576	260	16.9%
Riggin Avenue/Court Street	652	1,973	148	11.2%
Robin Avenue/State Route 63	1,268	3,264	296	14.8%

As shown in Table 14, the proposed project will generate a portion of PM peak hour trips that will contribute to the deficiencies identified above.


According to the methodology described in the Caltrans *Guide for the Preparation of Traffic Impact Studies* (December 2002), Table 10 is neither intended as, nor does it establish a legal standard for determining equitable responsibility and cost of the project's traffic impact; the intent is to provide:

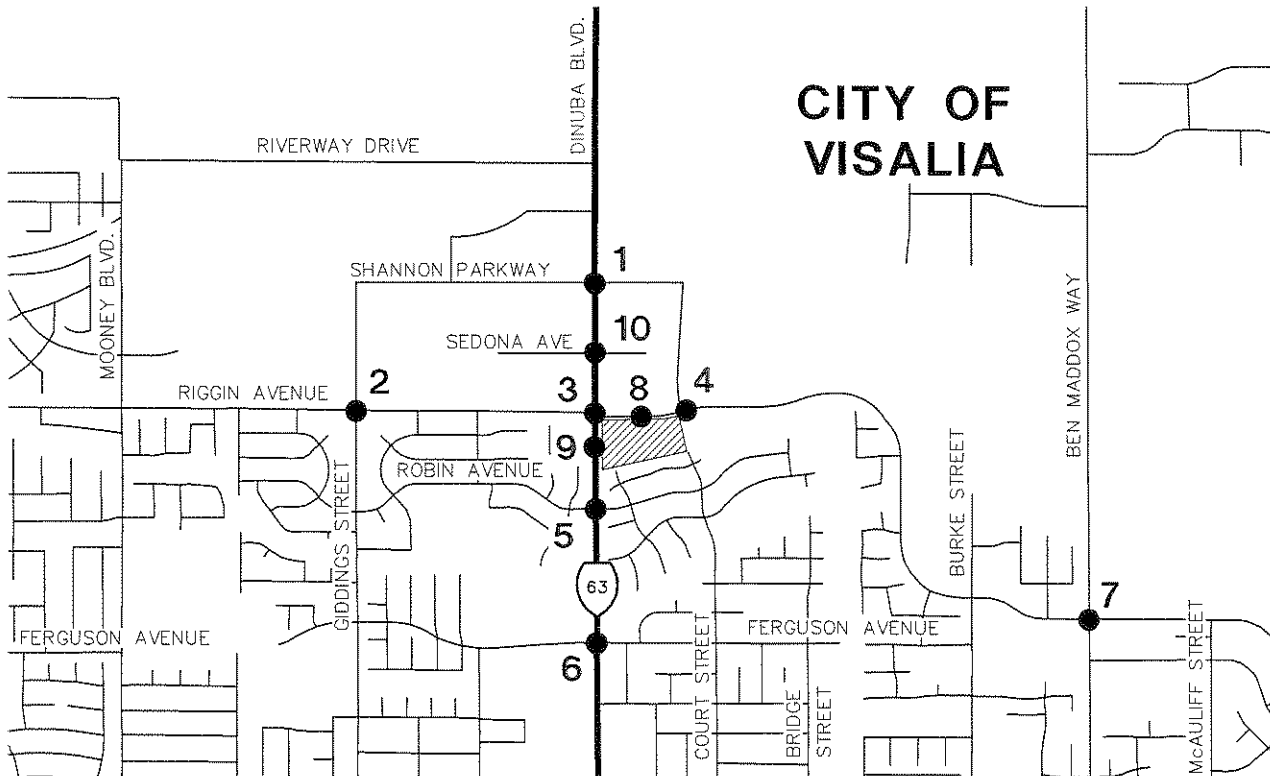
1. A starting point for early discussions to address traffic mitigation equitably;
2. A means for calculating the equitable share for mitigating traffic impacts; and
3. A means for establishing rough proportionality [Dolan vs. City of Tigard, 1994, 512 U.S. 374 (114 S. Ct. 2309)].

According to the Caltrans' *Guide for Preparation of Traffic Impact Studies* (December 2002), the method for calculating equitable mitigation measures "... is not intended for circumstances where a project proponent will be receiving a substantial benefit from the identified mitigation measure. In these cases, the project should take full responsibility toward providing the necessary infrastructure." Therefore, the beneficiary of the project driveway intersections on Riggin Avenue and State Route 63 shall provide for necessary improvements in order to accommodate access to their development.



LEGEND:

 - PROJECT LOCATION



Riverbend Commercial Center TIAR

Figure 12

Year 2035 Base plus Project Mitigated Lane Geometrics and Control



ACOUSTICAL ANALYSIS
PROPOSED WENDY'S AT RIVERBEND CENTER
DINUBA BOULEVARD AND RIGGIN AVENUE
VISALIA, CALIFORNIA

BBA Report No. 11-021

PREPARED FOR
BRIDGECOURT HOMES, L.P.
P.O. BOX 336
GLENORA, CALIFORNIA 91740

PREPARED BY
BROWN-BUNTIN ASSOCIATES, INC.
VISALIA, CALIFORNIA

JULY 25, 2011



INTRODUCTION

The project is a proposed Wendy's drive-through restaurant to be located near the southeast corner of Dinuba Boulevard (State Route 63) and Riggins Avenue within the City of Visalia, California. The drive-through restaurant would be part of the Riverbend Commercial Center that would include a gas station, mini-mart and car wash. The City of Visalia has required an acoustical analysis for the project to determine if noise levels produced by the drive-through operation would exceed city noise standards and recommend noise mitigation measures if required.

This analysis, prepared by Brown-Buntin Associates, Inc. (BBA), is based upon the project site plan dated June 8, 2011, measured noise level data obtained at a similar restaurant and project operations information provided by the project developer. Revisions to the site plan or other project-related information available to BBA at the time the analysis was prepared may require a reevaluation of the findings and/or recommendations of the report.

Appendix A provides definitions of the acoustical terminology used in this report. Unless otherwise stated, all sound levels reported in this analysis are A-weighted sound pressure levels in decibels (dB). A-weighting de-emphasizes the very low and very high frequencies of sound in a manner similar to the human ear. Most community noise standards utilize A-weighted sound levels, as they correlate well with public reaction to noise.

CRITERIA FOR ACCEPTABLE NOISE EXPOSURE

The City of Visalia Noise Element of the General Plan (noise element) establishes noise level criteria in terms of the Day-Night Average Level (DNL) metric. The DNL is the time-weighted energy average noise level for a 24-hour day, with a 10 dB penalty added to noise levels occurring during the nighttime hours (10:00 p.m.-7:00 a.m.). The DNL represents cumulative exposure to noise over an extended period of time and is therefore calculated based upon *annual average* conditions.

The exterior noise compatibility criterion of the noise element is 65 dB DNL within outdoor activity areas of residential uses. Outdoor activity areas generally include backyards of single-family residences and individual patios or decks and common outdoor activity areas of multi-family developments. The intent of the exterior noise level requirement is to provide an acceptable noise environment for outdoor activities and recreation.

The noise element also requires that interior noise levels attributable to exterior noise sources not exceed 45 dB DNL. The intent of the interior noise level standard is to provide an acceptable noise environment for indoor communication and sleep.

Ordinance No. 90-03 of the Visalia Ordinance Code (noise ordinance) applies to noise sources that are not pre-empted from local control by existing state or federal regulations. Pre-empted noise sources include traffic on public roadways, railroad operations and aircraft operations. The

proposed drive-through restaurant is not a pre-empted noise source and is therefore subject to the provisions of the noise ordinance.

The noise ordinance addresses the statistical distribution of noise over time and allows for progressively shorter periods of exposure to levels of increasing loudness. Table I summarizes the exterior noise level standards of the ordinance. Note that the ordinance is to be applied during any one-hour time period of the day, and that the standards are more restrictive during the nighttime hours, defined by the ordinance as between 7:00 p.m. and 6:00 a.m. There is a 5 dB penalty applied if the noise source of concern consists primarily of speech or music. The standards of the noise ordinance may be adjusted if existing noise levels not related to the source of concern already exceed the standards of the ordinance.

TABLE I EXTERIOR NOISE LEVEL STANDARDS, DBA CITY OF VISALIA NOISE ORDINANCE			
Category	Cumulative # Min/Hr. (L_n) ¹	Daytime (6:00 a.m.-7:00 p.m.)	Nighttime (7:00 p.m.-6:00 a.m.)
1	30 (L_{50})	50/45 ²	45/40 ²
2	15 (L_{25})	55/50 ²	50/45 ²
3	5 ($L_{8.3}$)	60/55 ²	55/50 ²
4	1 ($L_{1.7}$)	65/60 ²	60/55 ²
5	0 (L_{max})	70/65 ²	65/60 ²

¹The L_n is an abbreviation for the percentage of time that a certain noise level is exceeded during a one-hour period.

²The standards have been adjusted by 5 dB since the noise source of concern consists primarily of speech.

EXISTING PROJECT SITE NOISE EXPOSURE

The primary existing source of noise affecting the project site and surrounding area is traffic on Dinuba Boulevard (State Route 63) and Riggin Avenue. Ambient noise level measurements were conducted on July 12, 2011 near the southern boundary of the project site at a distance of approximately 200 feet from the center of Dinuba Boulevard. The noise monitoring site is representative of the closest existing residential properties to the proposed drive-through restaurant. Figure 1 shows the project site and ambient noise monitoring site.

Noise monitoring equipment consisted of a Larson-Davis Laboratories Model LDL 820 sound level analyzer equipped with a B&K Type 4176 1/2" microphone. The microphone was located on a tripod at about five feet above the ground. The noise monitoring equipment complies with specifications of the American National Standards Institute (ANSI) for Type I (Precision) sound level meters and was calibrated prior to use with a B&K Type 4230 acoustic calibrator to ensure the accuracy of the measurements. Ambient noise monitoring data are summarized in Table II.

Figure 1: Project Site and Ambient Noise Monitoring Location

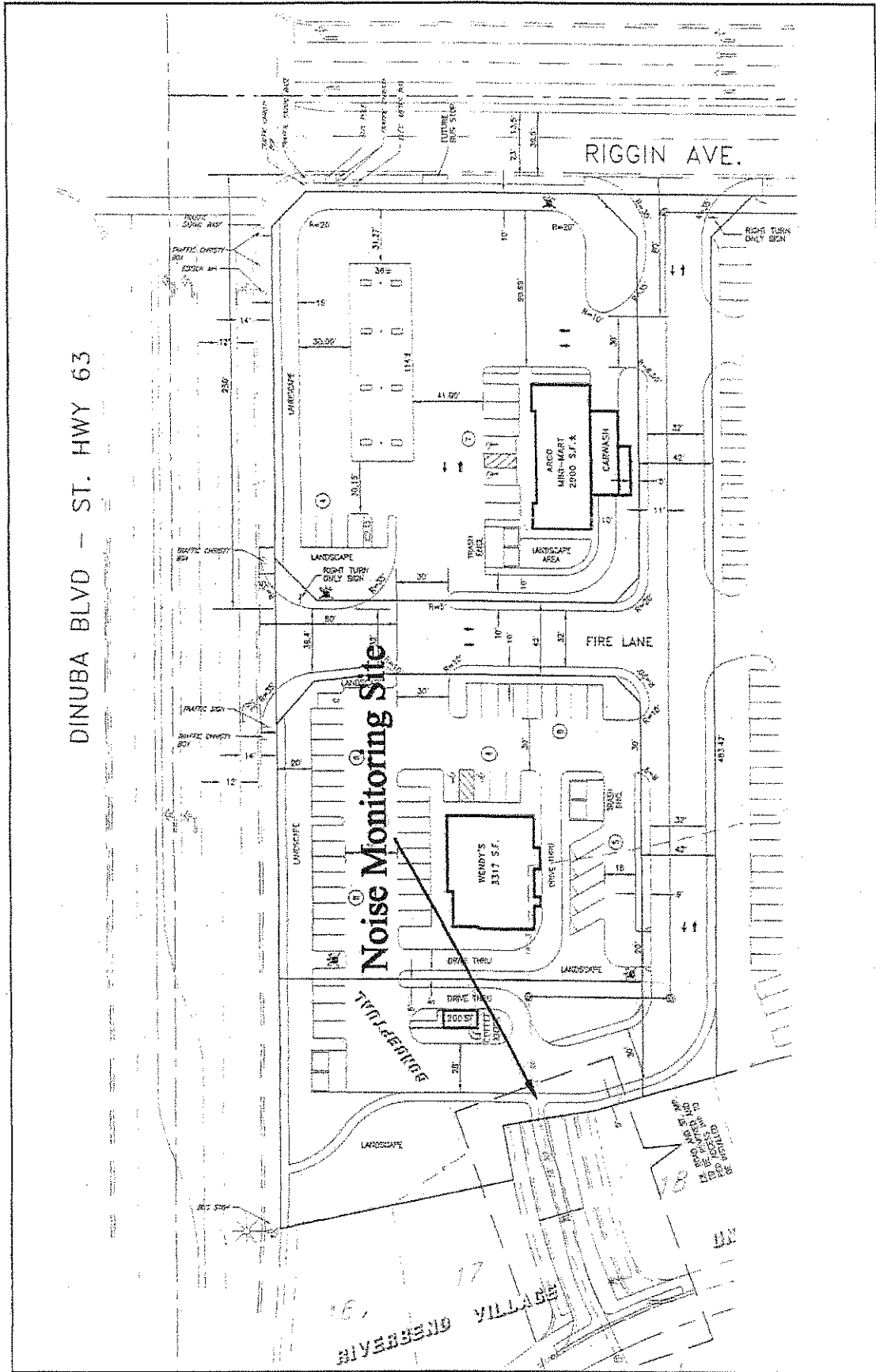


Table II indicates that existing ambient noise levels in the project vicinity are in the range of 42-63 dBA with an energy average level (L_{eq}) of 52.7 dBA. The predominant noise source at the time of the ambient noise level measurements was traffic on Dinuba Boulevard. The estimated DNL within the residential area in the vicinity of the project site is 50-65 dB, depending upon distance from Dinuba Boulevard. This is a typical noise exposure for many Visalia neighborhoods.

TABLE II
SUMMARY OF AMBIENT NOISE LEVEL MEASUREMENTS
JULY 12, 2011

Time	Location	A-weighted Decibels, dBA						
		L_{eq}	L_{min}	L_{max}	L_{50}	L_{25}	$L_{8.3}$	$L_{1.7}$
9:00-9:15 a.m.	Project site @ 200' from SR63 CL	52.7	41.7	62.9	50.8	54.2	56.9	59.0

Source: Brown-Buntin Associates, Inc.

PROJECT-RELATED NOISE LEVELS

In order to obtain reference noise level data for the project, BBA measured noise levels at an existing Wendy's drive-through restaurant located on South Mooney Boulevard in Visalia. Measurements were conducted during the early afternoon of July 11, 2011 between 12:45 p.m. and 1:45 p.m. using the previously-described noise monitoring equipment. According to the project developer, the configuration of the drive-through lane and loudspeaker equipment at the tested Wendy's restaurant is similar to that proposed for the project.

The microphone used by customers to order food and the loudspeaker used by employees to confirm orders are both integrated into a menu board that is located a few feet from the drive-through lane at the approximate height of a typical car window. With reference to the proposed project, the menu board containing the microphone/loudspeaker system would be located near the southwest corner of the restaurant building with the loudspeaker facing south. Vehicles would enter the drive-through lane from the west and then turn to the north along the east side of the restaurant.

Reference noise measurements were obtained at a distance of approximately 40 feet from the menu board containing the microphone/loudspeaker system at an angle of about 45° toward the rear of the vehicle being served. This provided a worst-case exposure to sound from the loudspeaker system since the vehicle was not located directly between the loudspeaker and measurement location. Cars were lined up in the access lane during the noise measurement period indicating that the drive-through lane was operating at or near a peak level of activity.

Each ordering cycle was observed to take approximately 60 seconds including vehicle movements. A typical ordering cycle included 5-10 seconds of loudspeaker use with typical maximum noise levels in the range of 60-62 dBA at the 40 foot-reference location. Vehicles

moving through the drive-through lane produced noise levels in the range of 55-60 dBA at the same distance. Vehicles parked at the ordering position (between the menu board and measurement site) were observed to provide significant acoustic shielding during the ordering sequence. The effects of such shielding are reflected by the noise measurement data.

The proposed hours of restaurant operation were not known to BBA at the time this analysis was prepared. However, it is reasonable to assume that the drive-through operation would extend into the nighttime hours as defined by the city's noise ordinance (7:00 p.m.-6:00 a.m.). This means that the nighttime standards of the city's noise ordinance are applicable to the project.

As noted above, a typical drive-through ordering cycle was observed to last about 60 seconds during peak levels of activity. This translates into about 60 cycles per hour. Assuming that loudspeaker use would average 5-10 seconds per cycle, also noted above, loudspeaker use would total 5-10 minutes per hour (or 8.3-16.7% of the time) during peak levels of activity. This corresponds to the $L_{8,3}$ and L_{25} statistical categories of the city's noise ordinance. The nighttime $L_{8,3}$ and L_{25} standards for sources consisting primarily of speech or music are 50 and 45 dBA, respectively. Noise from idling vehicles is assumed to occur at least 50% of the time during peak levels of activity. This corresponds to a nighttime L_{50} standard of 40 dBA.

The closest noise-sensitive receptors to the project are existing residential properties located to the south off of North Encina Court. The closest residential property line would be located about 125 feet from the menu board containing the microphone/loudspeaker system. Potential project-related noise exposure at that location was calculated based upon the above-described reference noise measurement data and the normal rate of sound attenuation over distance for a "point" noise source (6 dB/doubling of distance).

Table III summarizes project-related noise levels at the closest residential property line, and compares the levels to applicable noise ordinance standards. The project would be expected to comply with the city's noise ordinance in all categories during the daytime hours. However, the project could exceed the city's noise ordinance in Categories 1 and 2 during the nighttime hours by up to 5 dB.

Compliance with the city's *noise element* is determined using the DNL descriptor. The DNL may be calculated using the L_{eq} measured during typical source operations and the assumed hours of operation. If it is assumed that the project could operate continuously at peak levels of activity between the hours of 7:00 a.m. and 10:00 p.m. (a worst-case scenario), the calculated DNL due to the project at the closest noise-sensitive property line would be in the range of 50-55 dB. This complies with the city's exterior land use compatibility criterion of 65 dB DNL. Assuming normal residential construction, the project would also comply with the city's interior noise level standard of 45 dB DNL.

NOISE MITIGATION

Noise mitigation is not required for compliance with the interior or exterior standards of the city's *noise element* at the closest noise-sensitive properties. Table III shows that the project would also be expected to comply with all statistical categories of the *noise ordinance* for

assumed peak levels of activity during the daytime hours (6:00 a.m. to 7:00 p.m.). If peak levels of activity were to occur during the nighttime hours (7:00 p.m. to 6:00 a.m.), the project has the potential to exceed Categories 1 and 2 of the noise ordinance by up to 5 dB.

TABLE III WORST-CASE NOISE EXPOSURE COMPARED TO NOISE ORDINANCE STANDARDS WENDY'S DRIVE-THROUGH AT RIVERBEND CENTER							
Category	Cumulative Min/Hr. (L _n)	Daytime (6:00 a.m.-7:00 p.m.)			Nighttime (7:00 p.m.-6:00 a.m.)		
		Noise Standard ¹	Project Noise	Compliance	Noise Standard ¹	Project Noise	Compliance
1	30 (L ₅₀)	45	45	Yes	40	45	? ²
2	15 (L ₂₅)	50	50	Yes	45	50	? ²
3	5 (L _{8.3})	55	50	Yes	50	50	Yes
4	1 (L _{1.7})	60	52	Yes	55	52	Yes
5	0 (L _{max})	65	53	Yes	60	53	Yes

¹Standards adjusted (made 5 dB more restrictive) for noise sources consisting primarily of speech or music.
²Project would comply within these categories if noise ordinance standards are adjusted for existing noise levels from traffic on Dinuba Boulevard during likely periods of peak project activities.

Source: Brown-Buntin Associates, Inc.

As noted above, the city's noise ordinance standards may be adjusted if noise levels not related to the project already exceed the standards of the ordinance. During the site inspection and ambient noise measurements on July 12, 2011, individual cars and pickups on Dinuba Boulevard were observed to produce noise levels in the range of 50-57 dBA near the closest noise-sensitive properties to the project. Individual trucks and buses produced noise levels in the range of 60-63 dBA. The measured L₅₀ during the ambient noise monitoring period was 50.8 dBA.

It is unlikely that the proposed drive-through restaurant would operate at peak levels during times of the day or night when traffic volumes on Dinuba Boulevard are low. If it may be assumed that traffic noise associated with Dinuba Boulevard Avenue already exceeds 50 dBA for more than 30 minutes per hour during periods when peak project activity would be expected, it may be concluded that the project would not exceed the city's noise ordinance standards. Noise mitigation would therefore not be required for compliance with the noise ordinance.

CONCLUSIONS

The proposed Wendy's drive-through restaurant at Riverbend Center will comply with applicable City of Visalia exterior and interior noise level requirements without additional mitigation.

The conclusions and recommendations of this acoustical analysis are based upon the best information known to Brown-Buntin Associates, Inc. (BBA) at the time the analysis was

prepared concerning the proposed site plan, noise levels produced by similar drive-through loudspeaker systems and hours of peak project operations. Any significant changes in these factors will require a reevaluation of the findings of this report. Additionally, any significant future changes in drive-through sound system technology, noise regulations or other factors beyond BBA's control may result in long-term noise results different from those described by this analysis.

Respectfully submitted,



Robert E. Brown
President

REB:dm

APPENDIX A

ACOUSTICAL TERMINOLOGY

AMBIENT NOISE LEVEL: The composite of noise from all sources near and far. In this context, the ambient noise level constitutes the normal or existing level of environmental noise at a given location.

CNEL: Community Noise Equivalent Level. The average equivalent sound level during a 24-hour day, obtained after addition of approximately five decibels to sound levels in the evening from 7:00 p.m. to 10:00 p.m. and ten decibels to sound levels in the night before 7:00 a.m. and after 10:00 p.m.

DECIBEL, dB: A unit for describing the amplitude of sound, equal to 20 times the logarithm to the base 10 of the ratio of the pressure of the sound measured to the reference pressure, which is 20 micropascals (20 micronewtons per square meter).

DNL/ L_{dn} : Day/Night Average Sound Level. The average equivalent sound level during a 24-hour day, obtained after addition of ten decibels to sound levels in the night after 10:00 p.m. and before 7:00 a.m.

L_{eq} : Equivalent Sound Level. The sound level containing the same total energy as a time varying signal over a given sample period. L_{eq} is typically computed over 1, 8 and 24-hour sample periods.

NOTE: The CNEL and DNL represent daily levels of noise exposure averaged on an annual basis, while L_{eq} represents the average noise exposure for a shorter time period, typically one hour.

L_{max} : The maximum noise level recorded during a noise event.

L_n : The sound level exceeded "n" percent of the time during a sample interval (L_{90} , L_{50} , L_{10} , etc.). For example, L_{10} equals the level exceeded 10 percent of the time.

ACOUSTICAL TERMINOLOGY

**NOISE EXPOSURE
CONTOURS:**

Lines drawn about a noise source indicating constant levels of noise exposure. CNEL and DNL contours are frequently utilized to describe community exposure to noise.

**NOISE LEVEL
REDUCTION (NLR):**

The noise reduction between indoor and outdoor environments or between two rooms that is the numerical difference, in decibels, of the average sound pressure levels in those areas or rooms. A measurement of "noise level reduction" combines the effect of the transmission loss performance of the structure plus the effect of acoustic absorption present in the receiving room.

SEL or SENEL:

Sound Exposure Level or Single Event Noise Exposure Level. The level of noise accumulated during a single noise event, such as an aircraft overflight, with reference to a duration of one second. More specifically, it is the time-integrated A-weighted squared sound pressure for a stated time interval or event, based on a reference pressure of 20 micropascals and a reference duration of one second.

SOUND LEVEL:

The sound pressure level in decibels as measured on a sound level meter using the A-weighting filter network. The A-weighting filter de-emphasizes the very low and very high frequency components of the sound in a manner similar to the response of the human ear and gives good correlation with subjective reactions to noise.

**SOUND TRANSMISSION
CLASS (STC):**

The single-number rating of sound transmission loss for a construction element (window, door, etc.) over a frequency range where speech intelligibility largely occurs.



MEETING DATE 05/29/2013
SITE PLAN NO. 13-091
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

RESUBMIT Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

- During site plan design/policy concerns were identified, schedule a meeting with
- Planning Engineering prior to resubmittal plans for Site Plan Review.
- Solid Waste Parks and Recreation Fire Dept.

REVISE AND PROCEED (see below)

A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

Submit plans for a building permit between the hours of 8:30 a.m. and 4:30 p.m., Monday through Friday.

Your plans must be reviewed by:

- CITY COUNCIL REDEVELOPMENT
 PLANNING COMMISSION PARK/RECREATION
 HISTORIC PRESERVATION OTHER _____

ADDITIONAL COMMENTS Conditional Use Permit.

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

Site Plan Review Committee



RECYCLED PAPER

MEETING DATE MAY 29, 2013

SITE PLAN NO. 13-091

PARCEL MAP NO.

SUBDIVISION

LOT LINE ADJUSTMENT NO.

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Your plans must be reviewed by:

CITY COUNCIL

REDEVELOPMENT

PLANNING COMMISSION CUP

PARK/RECREATION

HISTORIC PRESERVATION

OTHER _____

ADDITIONAL COMMENTS _____

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

Site Plan Review Committee

City of Visalia
Building: Site Plan
Review Comments

ITEM NO: 3 DATE: May 29, 2013
SITE PLAN NO: SPR13091
PROJECT TITLE: COMMERCIAL CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SOUTHEAST CORNER OF DINUBA & RIGGIN
APN(S): 091-010-040

NOTE: These are general comments and DO NOT constitute a complete plan check for your specific project
Please refer to the applicable California Code & local ordinance for additional requirements.

- A building permit will be required. *For information call (559) 713-4444*
- Submit 5 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)
- Submit 5 sets of plans prepared by an architect or engineer. Must comply with 2010 California Building Cod Sec. 2308 for conventional light-frame construction or submit 2 sets of engineered calculations.
- Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:
- Meet State and Federal requirements for accessibility for persons with disabilities.
- A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- All accessible units required to be adaptable for persons with disabilities.
- Maintain sound transmission control between units minimum of 50 STC.
- Maintain fire-resistive requirements at property lines.
- A demolition permit & deposit is required. *For information call (559) 713-4444*
- Obtain required permits from San Joaquin Valley Air Pollution Board. *For information call (559) 230-6000*
- Plans must be approved by the Tulare County Health Department. *For information call (559) 624-8011*
- Project is located in flood zone _____ * Hazardous materials report.
- Arrange for an on-site inspection. (Fee for inspection \$146.40) *For information call (559) 713-4444*
- School Development fees. Commercial \$0.47 per square foot. Residential \$2.97 per square foot.
- Park Development fee \$ _____, per unit collected with building permits.
- Existing address must be changed to be consistent with city address. *For information call (559) 713-4320*
- Acceptable as submitted
- No comments at this time

Additional comments: _____

GENE FERRERO 5-28-13
Signature

- A fire lane is required for this project.
- A Knox Box key lock system is required. Applications are available at the Fire Department Administrative Office. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Acequia Ave. Please allow adequate time for shipping and installation.)
- The security gates, if to be locked, shall be locked with a typical chain and lock that can be cut with a common bolt cutter, or the developer may opt to provide a Knox Box key lock system. Applications are available at the Fire Department Administrative Office.
- That portion of the building that is built upon a property line shall be constructed as to comply with Section 503.4 and Table 5-A of the California Building Code.
- Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system.
- If you handle hazardous material in amounts that exceed the exempt amounts listed on Table 3-D of the California Building Code, you are required to submit an emergency response plan to the Tulare County Health Department. Prior to the building final inspection, we will want a copy of the plan and any Material Safety Data Sheets.
- An automatic fire sprinkler system will be required for this building. A fire hydrant is required within 50 feet of the fire department connection. The fire hydrant, fire department connection and the PIV valve should be located together and minimum 25' from the building, if possible. The caps on the FDC shall be Knox locking caps.
- All hardware on exit doors shall comply with Chapter 10 of the California Fire Code. This includes all locks, latches, dolt locks, and panic and fire exit hardware.
- Provide Illuminated exit signs and emergency lighting though-out building.
- All Fire and Life Safety systems located within the building shall be maintained.
- An automatic fire extinguishing system for protection of the kitchen grease hood and ducts is required.
- Special comments: Fire sprinklers may be required depending on the occupany of the buildings and their occupancy loads.


Kurtis Brown, Assistant Fire Marshal

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA NEIGHBORHOOD PRESERVATION/CODE ENFORCEMENT DIVISION

May 29, 2013

ITEM NO: 3
SITE PLAN NO: SPR13091
PROJECT TITLE: COMMERCIAL CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SOUTHEAST CORNER OF DINUBA & RIGGIN
APN(S): 091-010-040

- No Comments
- See Previous Site Plan Comments
- Provide Shopping Cart Retrieval Plan For any Retail That Will Require Shopping Carts

Additional Comments:


Tracy Robertshaw

CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4500

COMMERCIAL BIN SERVICE

ITEM NO: 2 DATE: 08/22/2017
SITE PLAN NO: SPR13091
PROJECT TITLE: COMMERCIAL CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SOUTHEAST CORNER OF DINUBA & RIGGIN
APN(S): 091-010-040

- No comments.
- Same comments as as
- Revisions required prior to submitting final plans. See comments below.
- Resubmittal required. See comments below.
- Customer responsible for all cardboard and other bulky recyclables to be broken down before disposing of in recycle containers.
- ALL refuse enclosures must be R-3 or R-4
- Customer must provide combination or keys for access to locked gates/bins
- Type of refuse service not indicated.
- Location of bin enclosure not acceptable. See comments below.
- Bin enclosure not to city standards double.
- Inadequate number of bins to provide sufficient service. See comments below.
- Drive approach too narrow for refuse trucks access. See comments below.
- Area not adequate for allowing refuse truck turning radius of :
Commercial (X) 50 ft. outside 36 ft. inside; Residential () 35 ft. outside, 20 ft. inside.
- Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- Bin enclosure gates are required
- Hammerhead turnaround must be built per city standards.
- Cul - de - sac must be built per city standards.
- Bin enclosures are for city refuse containers only. Grease drums or any other items are not allowed to be stored inside bin enclosures.
- Area in front of refuse enclosure must be marked off indicating no parking
- Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS)
- Customer will be required to roll container out to curb for service.
- Must be a concrete slab in front of enclosure as per city standards

The width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.

Roll off compactor's t have a clearance of 3 feet from any wall both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.

Bin enclosure gates must open 180 degrees and also hinges must be mounted in front of post see page 2 for instructions

NO COMMENTS, IT LOOKS GOOD TO GO FROM SOLID WASTE.

Javier Hernandez, Solid Waste Front Load Supervisor 713-4338

QUALITY ASSURANCE DIVISION

SITE PLAN REVIEW COMMENTS

ITEM NO: 3 DATE: May 29, 2013

SITE PLAN NO: SPR13091

PROJECT TITLE: COMMERCIAL CENTER

DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES
(R16 ZONED)

APPLICANT: BRIDGECOURT HOMES

PROP OWNER: BRIDGECOURT DEVELOPMENT LLC

LOCATION: SOUTHEAST CORNER OF DINUBA & RIGGIN

APN(S): 091-010-040

YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS:

- WASTEWATER DISCHARGE PERMIT APPLICATION for each 8000 SQ FT RESTAURANT
WENDYS
Car Wash
- SAND AND GREASE INTERCEPTOR – 3 COMPARTMENT for CAR WASH
- GREASE INTERCEPTOR min. 1000 GAL for each 8000 SQ FT RESTAURANT
WENDYS
- GARBAGE GRINDER – ¾ HP. MAXIMUM _____
- SUBMISSION OF A DRY PROCESS DECLARATION _____
- NO SINGLE PASS COOLING WATER IS PERMITTED _____
- OTHER _____
- SITE PLAN REVIEWED – NO COMMENTS

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA
PUBLIC WORKS DEPARTMENT
QUALITY ASSURANCE DIVISION
7579 AVENUE 288
VISALIA, CA 93277

Dr. n. Knight

AUTHORIZED SIGNATURE

5-24-13

DATE



CITY OF VISALIA
NONSIGNIFICANT WASTEWATER
DISCHARGE PERMIT APPLICATION

Agency Use:
Permit No: _____
Code No: _____
Data Entry By: _____

PLEASE PRINT OR TYPE

APPLICANT BUSINESS NAME: _____ PHONE: _____

BUSINESS ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

BUSINESS OWNER: _____ PHONE: _____

MAILING ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

CONTACT PERSON: _____ TITLE: _____

NATURE OF BUSINESS: (restaurant, market, convenience store, yogurt shop, walk-up, etc.)

NO. OF EMPLOYEES: _____ HOURS OPEN: _____ AM/PM TO _____ AM/PM

DAYS PER WEEK BUSINESS OPEN: MON TUES WED THUR FRI SAT SUN

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

If yes, Name of trap: _____ Size of trap: _____ How often is trap cleaned: _____

I hereby affirm that all information furnished is true and correct
to the best of my knowledge.

Signature Date

Public Works Department
Quality Assurance Division
7579 Ave 288
Visalia CA 93277
(559) 713-4487



CITY OF VISALIA NONSIGNIFICANT WASTEWATER DISCHARGE PERMIT APPLICATION

Agency Use:
Permit No: _____
Code No: _____
Data Entry By: _____

PLEASE PRINT OR TYPE

APPLICANT BUSINESS NAME: _____ PHONE: _____

BUSINESS ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

BUSINESS OWNER: _____ PHONE: _____

MAILING ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

CONTACT PERSON: _____ TITLE: _____

NATURE OF BUSINESS: (restaurant, market, convenience store, yogurt shop, walk-up, etc.)

NO. OF EMPLOYEES: _____ HOURS OPEN: _____ AM/PM TO _____ AM/PM

DAYS PER WEEK BUSINESS OPEN: MON TUES WED THUR FRI SAT SUN

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

If yes, Name of trap: _____ Size of trap: _____ How often is trap cleaned: _____

I hereby affirm that all information furnished is true and correct
to the best of my knowledge.

Signature Date

Public Works Department
Quality Assurance Division
7579 Ave 288
Visalia CA 93277
(559) 713-4487



CITY OF VISALIA NONSIGNIFICANT WASTEWATER DISCHARGE PERMIT APPLICATION

Agency Use:
Permit No: _____
Code No: _____
Data Entry By: _____

PLEASE PRINT OR TYPE

APPLICANT BUSINESS NAME: _____ PHONE: _____

BUSINESS ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

BUSINESS OWNER: _____ PHONE: _____

MAILING ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

CONTACT PERSON: _____ TITLE: _____

NATURE OF BUSINESS: (auto repair, car wash, machine shop, painting, battery dealer, etc.)

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

Does your facility conduct automotive servicing operations that involve the exchange or replacement of fluids (e.g. oil, transmission or brake fluid, radiator coolant etc.)? YES NO

Does your facility have any floor drains? YES NO

Does your facility have a steam cleaner? YES NO

Does your facility wash vehicles on site? YES NO

If generated, how do you dispose of the following:

Grease, oil and sand interceptor contents _____

I hereby affirm that all information furnished is true and correct to the best of my knowledge.

Signature Date

Public Works Department
Quality Assurance Division
7579 Ave. 288
Visalia CA 93277
(559) 713-4487

CITY OF VISALIA
ORDINANCE 13.08

13.08.570 Traps required.

Grease, oil and sand traps shall be provided when, in the opinion of the City, they are necessary for the protection of the sewerage system from liquid wastes containing grease in excessive amounts, or any flammable wastes, sand and other harmful ingredients; except that such traps shall not be required for buildings used solely for residential purposes. Such traps shall be required for example, on discharges from all service stations, automotive repair garages, car washes, restaurants, eating establishments and food preparation establishments, and such other commercial or industrial establishments as the city may designate. (Prior code § 4254)

13.08.580 Construction of traps.

All traps shall be of a type and capacity approved by the city, and shall be so located as to be readily and easily accessible for cleaning and inspection. Restaurant traps shall be gas-tight, of a type approved for restaurant use by the division of building safety. Traps for all other facilities, including service stations and garages, shall be in accordance with the adopted plan of the city for such traps or shall be the approved equal thereof as determined by the director. (Prior code § 4255)

13.08.590 Maintenance of traps.

When installed, all grease, oil and sand traps shall be maintained by the owner, at owner's expense, in continuously efficient operation at all times. (Prior code § 4256)

**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

- Jason Huckleberry 713-4259
- Ken McSheehy 713-4447
- Adrian Rubalcaba 713-4271

ITEM NO: 3 DATE: MAY 29, 2013

SITE PLAN NO.: 13-091
PROJECT TITLE: COMMERCIAL CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP OWNER: BRIDGECOURT DEVELOPMENT INC
LOCATION: SOUTHEAST CORNER OF DINUBA & RIGGIN
APN: 091-010-040

SITE PLAN REVIEW COMMENTS

- REQUIREMENTS (indicated by checked boxes)
- Install curb return with ramp, with 35' radius; **COURT & RIGGIN INTERSECTION**
- Install curb; gutter **COURT ST.**
- Drive approach size: **36' MIN.** Use radius return; **REFER TO CALTRANS & CITY DETAILS**
- Sidewalk: **6'** width; **10'** parkway width at **RIGGIN, DINUBA, & COURT**
- Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- Right-of-way dedication required. A title report is required for verification of ownership. **COURT ST.**
- Deed required prior to issuing building permit; **6' EASEMENT FOR SIDEWALK**
- City Encroachment Permit Required.
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit.
- CalTrans Encroachment Permit required. CalTrans comments required prior to issuing building permit.
Contacts: David Deel (planning) 488-4088;
- Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map. Contact Doug Damko, 713-4268, 315 E. Acequia Ave.
- Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. Prepared by registered civil engineer or project architect. All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) directed to the City's existing storm drainage system; b) directed to a permanent on-site basin; or c) directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: : maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
- Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
- Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = .020%, V-gutter = 0.25%)
- Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- Traffic indexes per city standards:

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: 13-091

Date: 5/29/2013

**Summary of applicable Development Impact Fees to be collected at the time of building permit:
(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)**

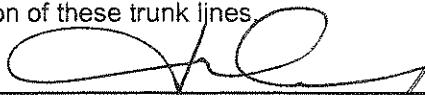
(Fee Schedule Date:5/3/2013)
(Project type for fee rates:MIXED USE)

Existing uses may qualify for credits on Development Impact Fees.

<u>FEE ITEM</u>	<u>FEE RATE</u>
<input checked="" type="checkbox"/> Groundwater Overdraft Mitigation Fee	
<input checked="" type="checkbox"/> Transportation Impact Fee	
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	
<input checked="" type="checkbox"/> Sewer Front Foot Fee	
<input checked="" type="checkbox"/> Storm Drain Acq/Dev Fee	
<input type="checkbox"/> Park Acq/Dev Fee	
<input checked="" type="checkbox"/> Northeast Specific Plan Fees	
<input checked="" type="checkbox"/> Waterways Acquisition Fee	
<input checked="" type="checkbox"/> Public Safety Impact Fee: Police	
<input checked="" type="checkbox"/> Public Safety Impact Fee: Fire	
<input checked="" type="checkbox"/> Public Facility Impact Fee	
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Jason Huckleberry

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

May 29, 2013

ITEM NO: 3
SITE PLAN NO: SPR13091
PROJECT TITLE: COMMERCIAL CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES (R18 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SOUTHEAST CORNER OF DINUBA & RIGGIN
APN(S): 091-010-040

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- No Comments
- See Previous Site Plan Comments
- Install Street Light(s) per City Standards.
- Install Street Name Blades at Locations.
- Install Stop Signs at Locations.
- Construct parking per City Standards PK-1 through PK-4.
- Construct drive approach per City Standards.
- Traffic Impact Study required.

Additional Comments:

- The vegetation within the public right of way and on-site shall be placed in the appropriate locations to not create a sight distance issue at any entrance/exit location. The designer shall check each location for the proper light of sight compliance.
- The intersection of Court Street and St. Johns Parkway will require an Engineering Study prior to it being converted to an all-way stop. This will be subject to the City review prior to any new traffic control measures are installed.


Eric Bons

SITE PLAN REVIEW COMMENTS

Brandon Smith, Planning Division (559) 713-4636

Date: May 29, 2013

SITE PLAN NO: 13-091
PROJECT TITLE: COMMERCIAL CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER ON 9.25 ACRES (R16 ZONED)
APPLICANT TITLE: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT HOMES
LOCATION TITLE: SOUTHEAST CORNER OF DINUBA & RIGGIN
APN TITLE: 091-010-040

General Plan: RLD – Residential Low Density
Existing Zoning: R-1-6 – SFR 6,000 sq. ft. min. site area

Planning Division Recommendation:

- Revise and Proceed
 Resubmit

Project Requirements

- General Plan Amendment
- Change of Zone
- Conditional Use Permit – for conditionally-allowed uses in commercial zones (restaurants with drive-through service, service (gas) station, convenience store), and for parcels without access to public right-of-way
- Tentative Parcel Map (see SPR No. 2013-032 comments)

PROJECT SPECIFIC INFORMATION: 05/22/2013

1. These comments refer to the master plan submittal entitled “Riverbend Village Design Guidelines and Engineering Standards”. See also comments for the previous Site Plan Review Submittals pertaining to the development plan (SPR 10-135 & 2013-031) and the parcel map (SPR 10-136 & 2013-032).
2. Provide copies of the master plan document with the submittal of the Conditional Use Permit. The CUP request will be for the adoption of the document to serve as the design guidelines and engineering standards.
3. The master plan shall identify a Design District and minimum setbacks for the center.
4. The master plan shall indicate that Phase One construction will include street trees along the arterial frontages and foliage along the entire south CMU wall.
5. Per Engineering / Traffic Division’s request, the master plan shall not indicate a 4-way stop at Riggins & Court.
6. No trash enclosures shall be placed within 15 feet from residential-zoned properties.
7. Indicate whether a new monument sign is proposed at the southwest corner of Riggins & Court.
8. Specify the number & location of proposed monument signs, including ARCO sign.
9. Indicate a second pedestrian entry on the driveway facing Court Street.
10. Call out river rock column bases, accent banding, etc. in pictures of convenience store and car wash.
11. The master plan text should mention that the restaurant architecture will match the theme of the Riverbend Village.
12. No signs will be allowed above the roofline of a building, as illustrated in the bottom left picture.

CITY GENERAL PLAN CONSISTENCY

Staff's initial finding is that the proposed project MAY NOT BE CONSISTENT with the City General Plan. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

Design Standards: The property will be assigned a Design District upon approval of a Change of Zone. Staff recommends that Design District "B" standards be considered for this property since it currently applies to other commercial designations on Dinuba Blvd. between Houston Avenue and Robin Avenue. The design standards for this district are as follows:

Design District: "B" [Section 17.30.170 of Visalia Municipal Code]

Maximum Building Height: 50 Feet

Minimum Setbacks:

	Building	Landscaping
➤ Front	15 Feet	15 Feet
➤ Side	0 Feet	5 Feet*
➤ Street side on corner lot	10 Feet	10 Feet
➤ Side abutting residential zone	15 Feet	5 Feet
➤ Rear	0 Feet	5 Feet*
➤ Rear abutting residential zone	20 Feet	5 Feet

*(Except where building is on property line)

Minimum Site Area: 5 acres

Parking:

1. Parking spaces are required in accordance with Zoning Ordinance Section 17.34.020 based on the types of land uses in the center.
 - For restaurants, provide 1 space per 100 square feet of gross floor area.
 - For retail, provide 1 space per 300 square feet of gross floor area.
 - For non-medical offices, provide 1 space per 250 square feet of gross floor area.
2. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot (Zoning Ordinance Section 17.34.030.I).
3. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.34.040.D & 17.30.130.C).
4. A planter is required every other row. Said planter shall be 6 feet in width and shall contain trees on twenty (20) foot centers (Zoning Ordinance Section 17.30.130.C.6).
5. It is highly recommended that bicycle rack(s) be provided on site plan.
6. No parking shall be permitted in a required front/rear/side yard (Zoning Ordinance Section 17.34.030.F).
7. The parking lot shall be screened from view by a 3-foot tall solid wall or shrubs when located adjacent to a public street.
8. The project should provide preferential parking spaces for carpools and vanpools to decrease the number of single occupant vehicle work trips. The preferential treatment could include covered parking spaces or close-in parking spaces, or designated free parking, or a guaranteed space for the vehicle.

Fencing and Screening:

1. Provide screening for roof mounted equipment (Zoning Ordinance Section 17.30.130.F).

2. Provide second-story screening for all windows that may intrude into adjacent residential properties. Details and cross-sections will be required to be reviewed and approved prior to issuance of building permits (Zoning Ordinance Section 17.30.130.F).
3. Provide screened trash enclosure with solid screening gates (Zoning Ordinance Section 17.30.130.F).
4. Provide solid screening of all outdoor storage areas. Outdoor storage to be screened from public view with solid material (Zoning Ordinance Section 17.30.130.F).
5. Outdoor retail sales prohibited.
6. Cross Sections need to be provided for site Plan Review if there is greater than an 18-inch difference between the elevation of the subject site and the adjacent properties, and the sections would be required for the public hearing process also.
7. All outdoor storage areas are to be identified on the site plan and they are to be shown with screening (fencing). No materials may be stored above the storage area fence heights (Zoning Ordinance Section 17.30.130.F).
8. If there is an anticipated grade difference of more than 12-inches between this site and the adjacent sites, a cross section of the difference and the walls must be provided as a part of the Subdivision and/or CUP application package.
9. NOTE: The maximum height of block walls and fences is 7-feet in the appropriate areas; this height is measured on the tallest side of the fence. If the height difference is such that the fence on the inside of the project site is not of sufficient height, the fence height should be discussed with Planning Staff prior to the filing of applications to determine if an Exception to fence/wall height should also be submitted.

Landscaping:

1. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.130.C).
2. All landscape areas to be protected with 6-inch concrete curbs (Zoning Ordinance Section 17.30.130.F).
3. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months.
4. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.30.130.C).
5. Provide a detailed landscape and irrigation plan for review prior to issuance of building permits. Please review Zoning Ordinance section 17.30.130-C for current landscaping and irrigation requirements.
6. Locate existing oak trees on site and provide protection for all oak trees greater than 2" diameter (see Oak Tree Preservation Ordinance).
7. On September 30, 2009, the State Model Water Efficient Landscape Ordinance (MWELo) was finalized by the State Department of Water Resources to comply with AB 1881. AB 1881 along with the MWELo became effective on January 1, 2010. As of January 1, 2010, the State Model Water Efficient Landscape Ordinance became effective by adoption of a City urgency ordinance on December 21, 2009. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance.

Lighting:

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.

2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

Noise: Must meet City noise standards as prescribed by the Noise Element.

Signage: All signs require a separate building permit.

The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments.

Signature _____



MEETING DATE 2/13/13
SITE PLAN NO. 13-031
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

RESUBMIT Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

- During site plan design/policy concerns were identified, schedule a meeting with
- | | |
|--------------------------------------|---|
| <input type="checkbox"/> Planning | <input type="checkbox"/> Engineering |
| <input type="checkbox"/> Solid Waste | <input type="checkbox"/> Parks and Recreation |
| <input type="checkbox"/> Fire Dept. | |

REVISE AND PROCEED (see below)

- A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.
- Submit plans for a building permit between the hours of 8:30 a.m. and 4:30 p.m., Monday through Friday.
- Your plans must be reviewed by:
- | | |
|--|--|
| <input type="checkbox"/> CITY COUNCIL | <input type="checkbox"/> REDEVELOPMENT |
| <input type="checkbox"/> PLANNING COMMISSION | <input type="checkbox"/> PARK/RECREATION |
| <input type="checkbox"/> HISTORIC PRESERVATION | <input type="checkbox"/> OTHER _____ |

ADDITIONAL COMMENTS _____

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

Site Plan Review Committee



MEETING DATE 2/13/13
SITE PLAN NO. 13-031
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.



RESUBMIT Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.

- During site plan design/policy concerns were identified, schedule a meeting with
 Planning Engineering prior to resubmittal plans for Site Plan Review.
 Solid Waste Parks and Recreation Fire Dept.

REVISE AND PROCEED (see below)

A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.

Submit plans for a building permit between the hours of 8:30 a.m. and 4:30 p.m., Monday through Friday.

Your plans must be reviewed by:

- | | |
|--|--|
| <input type="checkbox"/> CITY COUNCIL | <input type="checkbox"/> REDEVELOPMENT |
| <input type="checkbox"/> PLANNING COMMISSION | <input type="checkbox"/> PARK/RECREATION |
| <input type="checkbox"/> HISTORIC PRESERVATION | <input type="checkbox"/> OTHER _____ |

ADDITIONAL COMMENTS _____

If you have any questions or comments, please call Jason Huckieberry at (559) 713-4259.

Site Plan Review Committee

City of Visalia
Building: Site Plan
Review Comments

ITEM NO: 8 DATE: February 13, 2013
SITE PLAN NO: SPR13031
PROJECT TITLE: MIXED USE CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER WITH 6,500 SF
BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED)
BRIDGECOURT HOMES
APPLICANT: BRIDGECOURT DEVELOPMENT LLC
PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN(S): 091-010-040

NOTE:
These are general comments and DO NOT constitute a complete plan check for your specific project.
Please refer to the applicable California Code & local ordinance for additional requirements.

- Business Tax certification is required. *For information call (559)713-4326*
- A building permit will be required *For information call (559)713-4444*
- Submit 3 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)
- Submit 5 sets of plans signed by an architect or engineer. Must comply with 2010 California Building Code Sec. 2308 for conventional light-frame construction or submit 2 sets of engineered calculations.
- Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:
- Meet State and Federal requirements for accessibility for persons with disabilities.
- A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- All accessible units required to be adaptable for persons with disabilities.
- Maintain sound transmission control between units minimum of 50 STC.
- Maintain fire-resistive requirements at property lines.
- A demolition permit & deposit is required. *For information call (559)713-4444*
- Obtain required permits from San Joaquin Valley Air Pollution Board. *For information call (559)230-6000*
- Location of cashier must provide clear view of gas pump island
- Treatment connection charge to be assessed based on use. Credits _____
- Plans must be approved by the Tulare County Health Department. *For information call (559)624-8011*
- Project is located in flood zone _____ * Hazardous materials report.
- Arrange for an on-site inspection. *For information call (559)713-4444* (Fee for inspection \$146.40)
- School Development fees. Commercial \$0.47 per square foot. Residential \$2.97 per square foot.
- Park Development fee \$ _____, per unit collected with building permits.
- Existing address must be changed to be consistent with city address p *For information call (559)713-4320*
- Acceptable as submitted

Special comments: No Comments

 G. FERRERO

Signature

Date: 2-12-13

Site Plan Review Comments For:

Visalia Fire Department
Kurtis Brown, Assistant Fire Marshal
707 W Acequia
Visalia, CA 93291
559-713-4261 *office*
559-713-4808 *fax*

ITEM NO: 8

DATE: February 13, 2013

SITE PLAN NO:

SPR13031

PROJECT TITLE:

MIXED USE CENTER

DESCRIPTION:

MIXED USE COMMERCIAL CENTER WITH 6,500 SF BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED)

APPLICANT:

BRIDGECOURT HOMES

PROP OWNER:

BRIDGECOURT DEVELOPMENT LLC

LOCATION:

SEC DINUBA BLVD & RIGGIN AVE


APN(S):

091-010-040

The following comments are applicable when checked:

- Refer to previous comments dated _____.
- More information is needed before a Site Plan Review can be conducted. Please submit plans with more detail.
- The Site Plan Review comments in this document are not all encompassing, but a general overview of the California Fire Code, and City of Visalia Municipal Codes. Additional requirements may come during the plan review process.
- No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire protection requirements.
- Address numbers must be placed on the exterior of the building in such a position as to clearly and plainly visible from the street. Numbers will be at least six inches (6") high and shall be of a color to contrast with their background. If multiple addresses served by a common driveway, the range of numbers shall be posted at the roadway/driveway.
- No additional fire hydrants are required for this project; however, additional fire hydrants may be required for any future development.
- There is/are 4 fire hydrants required for this project. (See marked plans for fire hydrant locations.)
- The turning radius for emergency fire apparatus is 20 feet inside radius and 43 feet outside radius. Ensure that the turns identified to you during site plan comply with the requirements. An option is a hammer-head constructed to City standards.
- An access road is required and shall be a minimum of 20 feet wide. The road shall be an all-weather driving surface accessible prior to and during construction.
- Buildings or portions of buildings or facilities exceeding 30 feet in height above the lowest level of fire department vehicle access shall be provided with an approved fire apparatus access roads capable of accommodating fire department aerial apparatus. Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders. Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building.

- A fire lane is required for this project. The location will be given to you during the site plan meeting.
- A Knox Box key lock system is required. Applications are available at the Fire Department Administrative Office. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Acequia Ave. Please allow adequate time for shipping and installation.)
- The security gates, if to be locked, shall be locked with a typical chain and lock that can be cut with a common bolt cutter, or the developer may opt to provide a Knox Box key lock system. Applications are available at the Fire Department Administrative Office.
- That portion of the building that is built upon a property line shall be constructed as to comply with Section 503.4 and Table 5-A of the California Building Code.
- Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system.
- If you handle hazardous material in amounts that exceed the exempt amounts listed on Table 3-D of the California Building Code, you are required to submit an emergency response plan to the Tulare County Health Department. Prior to the building final inspection, we will want a copy of the plan and any Material Safety Data Sheets.
- An automatic fire sprinkler system will be required for this building. A fire hydrant is required within 50 feet of the fire department connection. The fire hydrant, fire department connection and the PIV valve should be located together and minimum 25' from the building, if possible. The caps on the FDC shall be Knox locking caps.
- All hardware on exit doors shall comply with Chapter 10 of the California Fire Code. This includes all locks, latches, dolt locks, and panic and fire exit hardware.
- Provide Illuminated exit signs and emergency lighting though-out building.
- All Fire and Life Safety systems located within the building shall be maintained.
- An automatic fire extinguishing system for protection of the kitchen grease hood and ducts is required.
- Fire Department Impact Fee - \$1601.01 per acre.
- Fire Department Permit Fee - complete application during Building Department permit process.
- Special comments:


 Kurtis Brown, Assistant Fire Marshal

CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4500

COMMERCIAL BIN SERVICE

ITEM NO: 8

DATE: February 13, 2013

SITE PLAN NO:

SPR13031

PROJECT TITLE:

MIXED USE CENTER

DESCRIPTION:

MIXED USE COMMERCIAL CENTER WITH 6,500 SF
BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED)
BRIDGECOURT HOMES

APPLICANT:

BRIDGECOURT DEVELOPMENT LLC

PROP OWNER:

SEC DINUBA BLVD & RIGGIN AVE

LOCATION:

APN(S):

091-010-040

No comments.

Same comments as as

Revisions required prior to submitting final plans. See comments below.

Resubmittal required. See comments below.

Customer responsible for all cardboard and other bulky recyclables to be broken down
before disposing of in recycle containers.

ALL refuse enclosures must be R-3 or R-4

Customer must provide combination or keys for access to locked gates/bins

Type of refuse service not indicated.

Location of bin enclosure not acceptable. See comments below.

Bin enclosure not to city standards double.

Inadequate number of bins to provide sufficient service. See comments below.

Drive approach too narrow for refuse trucks access. See comments below.

Area not adequate for allowing refuse truck turning radius of :
Commercial (X) 50 ft. outside 36 ft. inside; Residential () 35 ft. outside, 20 ft. inside.

Paved areas should be engineered to withstand a 55,000 lb. refuse truck.

Bin enclosure gates are required

Hammerhead turnaround must be built per city standards.

Cul - de - sac must be built per city standards.

Bin enclosures are for city refuse containers only. Grease drums or any other
items are not allowed to be stored inside bin enclosures.

Area in front of refuse enclosure must be marked off indicating no parking

Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS)

Customer will be required to roll container out to curb for service.

Must be a concrete slab in front of enclosure as per city standards

[]

The width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.

Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.

[]

Bin enclosure gates must open 180 degrees and also hinges must be mounted in front of post
see page 2 for instructions

NEED A R-3 BIN ENCLOSURE ON OFFICE BUILDING SITE PLAN.

Javier Hernandez, Solid Waste Front Load Supervisor 713-4338

[]

QUALITY ASSURANCE DIVISION
SITE PLAN REVIEW COMMENTS

ITEM NO: 8 DATE: February 13, 2013
SITE PLAN NO: SPR13031
PROJECT TITLE: MIXED USE CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER WITH 6,500 SF
BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN(S): 091-010-040

YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS:

- WASTEWATER DISCHARGE PERMIT APPLICATION) FOR CAR WASH AND WENDY'S RESTAURANT
- SAND AND GREASE INTERCEPTOR - 3 COMPARTMENT FOR CAR WASH
- GREASE INTERCEPTOR min. 1000 GAL FOR WENDY'S RESTAURANT
- GARBAGE GRINDER - 3/4 HP. MAXIMUM _____
- SUBMISSION OF A DRY PROCESS DECLARATION _____
- NO SINGLE PASS COOLING WATER IS PERMITTED _____
- OTHER _____
- SITE PLAN REVIEWED - NO COMMENTS

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA
PUBLIC WORKS DEPARTMENT
QUALITY ASSURANCE DIVISION
7579 AVENUE 288
VISALIA, CA 93277

[Signature]
AUTHORIZED SIGNATURE

2-12-13



CITY OF VISALIA NONSIGNIFICANT WASTEWATER DISCHARGE PERMIT APPLICATION

Agency Use:
Permit No: _____
Code No: _____
Data Entry By: _____

PLEASE PRINT OR TYPE

APPLICANT BUSINESS NAME: _____ PHONE: _____

BUSINESS ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

BUSINESS OWNER: _____ PHONE: _____

MAILING ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

CONTACT PERSON: _____ TITLE: _____

NATURE OF BUSINESS: (auto repair, car wash, machine shop, painting, battery dealer, etc.)

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

Does your facility conduct automotive servicing operations that involve the exchange or replacement of fluids (e.g. oil, transmission or brake fluid, radiator coolant etc.)? YES NO

Does your facility have any floor drains? YES NO

Does your facility have a steam cleaner? YES NO

Does your facility wash vehicles on site? YES NO

If generated, how do you dispose of the following:

Grease, oil and sand interceptor contents _____

I hereby affirm that all information furnished is true and correct to the best of my knowledge.

Signature Date

Public Works Department
Quality Assurance Division
7579 Ave. 288
Visalia CA 93277
(559) 713-4487

SPA 13031 for WR003



CITY OF VISALIA
NONSIGNIFICANT WASTEWATER
DISCHARGE PERMIT APPLICATION

Agency Use:
Permit No: _____
Code No: _____
Data Entry By: _____

PLEASE PRINT OR TYPE

APPLICANT BUSINESS NAME: _____ PHONE: _____

BUSINESS ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

BUSINESS OWNER: _____ PHONE: _____

MAILING ADDRESS: _____ CITY: _____ STATE: _____ ZIP: _____

CONTACT PERSON: _____ TITLE: _____

NATURE OF BUSINESS: (restaurant, market, convenience store, yogurt shop, walk-up, etc.)

NO. OF EMPLOYEES: _____ HOURS OPEN: _____ AM/PM TO _____ AM/PM

DAYS PER WEEK BUSINESS OPEN: MON TUES WED THUR FRI SAT SUN

Does your facility have a grease, oil or grit trap installed before discharge to sewer? YES NO

If yes, Name of trap: _____ Size of trap: _____ How often is trap cleaned: _____

I hereby affirm that all information furnished is true and correct
to the best of my knowledge.

Signature Date

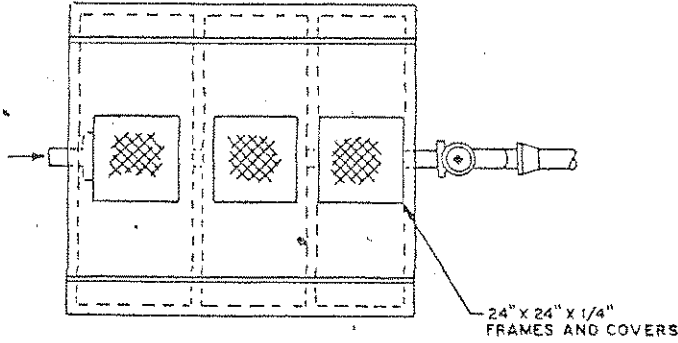
Public Works Department
Quality Assurance Division
7579 Ave 288
Visalia CA 93277
(559) 713-4487

TYPICAL PRECAST 1000 GAL.

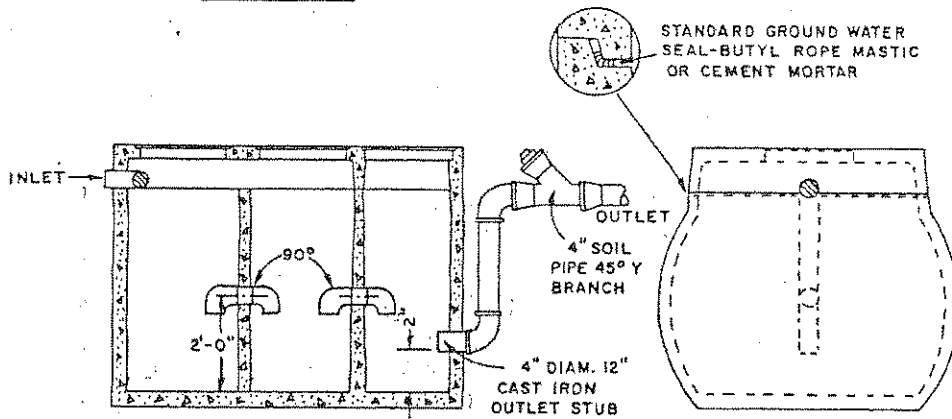
NO SCALE

(SEE DETAIL S-9A FOR NOTES)

SPB 13031 FOR GAR WASH

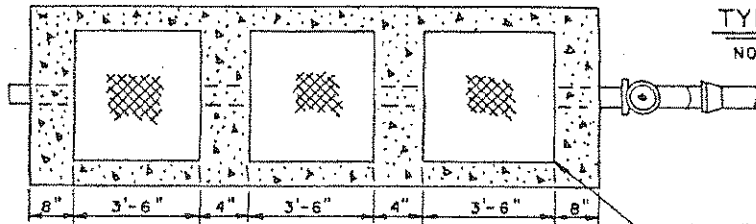


PLAN VIEW



LONGITUDINAL SECTION

END VIEW



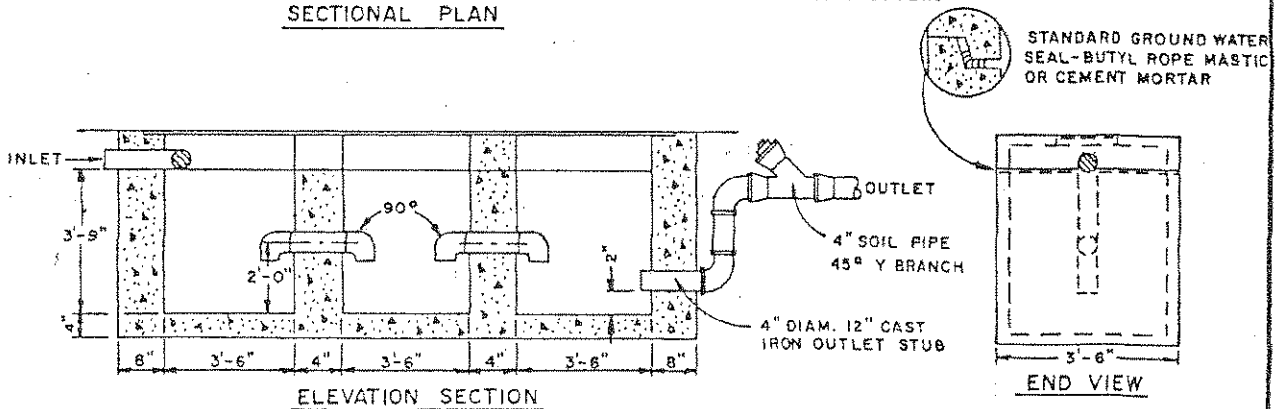
TYPICAL POUR-IN-PLACE

NO SCALE

1000 GAL.

(SEE DETAIL S-9A FOR NOTES)

SECTIONAL PLAN



ELEVATION SECTION

END VIEW

Approved by Visalia City Council on _____ DATE _____

**City of Visalia
Improvement Standards**

**Standard Outside Industrial Sand,
Grease & Oil Interceptor**

REVISIONS

S-9

**CITY OF VISALIA
ORDINANCE 13.08**

13.08.570 Traps required.

Grease, oil and sand traps shall be provided when, in the opinion of the City, they are necessary for the protection of the sewerage system from liquid wastes containing grease in excessive amounts, or any flammable wastes, sand and other harmful ingredients; except that such traps shall not be required for buildings used solely for residential purposes. Such traps shall be required for example, on discharges from all service stations, automotive repair garages, car washes, restaurants, eating establishments and food preparation establishments, and such other commercial or industrial establishments as the city may designate. (Prior code § 4254)

13.08.580 Construction of traps.

All traps shall be of a type and capacity approved by the city, and shall be so located as to be readily and easily accessible for cleaning and inspection. Restaurant traps shall be gas-tight, of a type approved for restaurant use by the division of building safety. Traps for all other facilities, including service stations and garages, shall be in accordance with the adopted plan of the city for such traps or shall be the approved equal thereof as determined by the director. (Prior code § 4255)

13.08.590 Maintenance of traps.

When installed, all grease, oil and sand traps shall be maintained by the owner, at owner's expense, in continuously efficient operation at all times. (Prior code § 4256)

**BUILDING/DEVELOPMENT PLAN
REQUIREMENTS
ENGINEERING DIVISION**

- Jason Huckleberry 713-4259
- Ken McSheehy 713-4447
- Adrian Rubalcaba 713-4271

ITEM NO: 8 DATE: FEBRUARY 13, 2013

SITE PLAN NO.: 13-031
PROJECT TITLE: MIXED USE CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER WITH 6,500 SF BUILDING IN PHASE 1 ON 9.8 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP OWNER: BRIDGECOURT DEVELOPMENT INC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN: 091-010-040

SITE PLAN REVIEW COMMENTS

- REQUIREMENTS (indicated by checked boxes)
- Install curb return with ramp, with _____ radius;
- Install curb; gutter
- Drive approach size: **36' MIN.** Use radius return;
- Sidewalk: **6'** width; **10'** parkway width at _____
- Repair and/or replace any sidewalk across the public street frontage(s) of the subject site that has become uneven, cracked or damaged and may constitute a tripping hazard.
- Replace any curb and gutter across the public street frontage(s) of the subject site that has become uneven and has created areas where water can stand.
- Right-of-way dedication required. A title report is required for verification of ownership.
- Deed required prior to issuing building permit;
- City Encroachment Permit Required.
Insurance certificate with general & auto liability (\$1 million each) and workers compensation (\$1 million), valid business license, and appropriate contractor's license must be on file with the City, and valid Underground Service Alert # provided prior to issuing the permit.
- CalTrans Encroachment Permit required. CalTrans comments required prior to issuing building permit.
Contacts: David Deel (planning) 488-4088;
- Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map. Contact Doug Damko, 713-4268, 315 E. Acequia Ave.
- Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. Prepared by registered civil engineer or project architect. All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) directed to the City's existing storm drainage system; b) directed to a permanent on-site basin; or c) directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site basin: _____; _____ maximum side slopes, perimeter fencing required, provide access ramp to bottom for maintenance.
- Grading permit is required for clearing and earthwork performed prior to issuance of the building permit.
- Show finish elevations. (Minimum slopes: A.C. pavement = 1%, Concrete pavement = 0.25%. Curb & Gutter = 0.020%, V-gutter = 0.25%)
- Show adjacent property grade elevations. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- Traffic indexes per city standards:

- Install street striping as required by the City Engineer.
- Install landscape curbing (typical at parking lot planters).
- Minimum paving section for parking: 2" asphalt concrete paving over 4" Class 2 Agg. Base, or 4" concrete pavement over 2" sand.
- Design Paving section to traffic index of 5.0 min. for solid waste truck travel path.
- Provide "R" value tests: each at
- Written comments required from ditch company Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditch; Jerry Hill 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- Access required on ditch bank, 15' minimum Provide wide riparian dedication from top of bank.
- Show Oak trees with drip lines and adjacent grade elevations. Protect Oak trees during construction in accordance with City requirements.
- A permit is required to remove oak trees. Contact David Pendergraft at 713-4295 for an Oak tree evaluation or permit to remove. A pre-construction conference is required.
- Relocate existing utility poles and/or facilities.
- Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- Subject to existing Reimbursement Agreement to reimburse prior developer.
- Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- If the project meets the one acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- Comply with prior comments. Resubmit with additional information. Redesign required.

Additional Comments:

1. Refer to prior comments with SPR 10-135.

2. Full Court street improvements will be made part of Parcel Map. Under the current Transportation Impact Fee program, a portion of the street improvements is reimbursable as fee credit. See Tentative Parcel Map site plan review comments for further details.

SUMMARY OF APPLICABLE DEVELOPMENT IMPACT FEES

Site Plan No: 13-031
Date: 2/20/2013

Summary of applicable Development Impact Fees to be collected at the time of building permit:

(Preliminary estimate only! Final fees will be based on the development fee schedule in effect at the time of building permit issuance.)

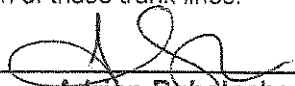
(Fee Schedule Date:8/17/2012)
(Project type for fee rates:MIXED USE)

Existing uses may qualify for credits on Development Impact Fees.

<u>FEE ITEM</u>	<u>FEE RATE</u>
<input checked="" type="checkbox"/> Groundwater Overdraft Mitigation Fee	
<input checked="" type="checkbox"/> Transportation Impact Fee	
<input checked="" type="checkbox"/> Trunk Line Capacity Fee	
<input checked="" type="checkbox"/> Sewer Front Foot Fee	
<input checked="" type="checkbox"/> Storm Drain Acq/Dev Fee	
<input type="checkbox"/> Park Acq/Dev Fee	
<input type="checkbox"/> Northeast Specific Plan Fees	
<input checked="" type="checkbox"/> Waterways Acquisition Fee	
<input checked="" type="checkbox"/> Public Safety Impact Fee: Police	
<input checked="" type="checkbox"/> Public Safety Impact Fee: Fire	
<input checked="" type="checkbox"/> Public Facility Impact Fee	
<input type="checkbox"/> Parking In-Lieu	

Reimbursement:

- 1.) No reimbursement shall be made except as provided in a written reimbursement agreement between the City and the developer entered into prior to commencement of construction of the subject facilities.
- 2.) Reimbursement is available for the development of arterial/collector streets as shown in the City's Circulation Element and funded in the City's transportation impact fee program. The developer will be reimbursed for construction costs and right of way dedications as outlined in Municipal Code Section 16.44. Reimbursement unit costs will be subject to those unit costs utilized as the basis for the transportation impact fee.
- 3.) Reimbursement is available for the construction of storm drain trunk lines and sanitary sewer trunk lines shown in the City's Storm Water Master Plan and Sanitary Sewer System Master Plan. The developer will be reimbursed for construction costs associated with the installation of these trunk lines.



Adrian Rubalcaba

SITE PLAN REVIEW COMMENTS

Brandon Smith, Planning Division (559) 713-4636

Date: February 13, 2012

SITE PLAN NO: 13-031
PROJECT TITLE: MIXED USE CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER WITH 6,500 SF BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED)
APPLICANT TITLE: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT DEVELOPMENT INC
LOCATION: SEC CORNER OF DINUBA AND RIGGIN
APN TITLE: 091-010-040

General Plan: RLD – Residential Low Density
Existing Zoning: R-1-6 – Single-Family Residential 6,000 sq. ft. min. site area

Planning Division Recommendation:

- Revise and Proceed
 Resubmit

Project Requirements

- General Plan Amendment
- Change of Zone
- Conditional Use Permit – for conditionally-allowed uses in commercial zones (restaurants with drive-through service, service (gas) station, convenience store), and for parcels without access to public right-of-way
- Tentative Parcel Map (see SPR No. 2013-032 comments)

PROJECT SPECIFIC INFORMATION: 02/13/2013

1. Identify and label all areas to be developed with the initial phase of development (Phase 1) and the latter phase of development (Phase 2). Staff's understanding is that the east-west access drive and Court Street will be built with Phase 1.
2. The "Conceptual" label shown with the "Future Development: should be removed from the plan since this phase is intended to be built as shown, and since the construction of the east-west access drive will dictate the layout of the second phase of commercial.
3. Emergency vehicular access shall be shown between the Wendy's fast food restaurant and the office based on the previous direction of the Visalia Planning Commission. Staff recommends that full vehicular cross-access between the Wendy's fast food restaurant and the office be provided.
4. Add more pedestrian crosswalks, sidewalks, and pathways throughout the commercial center so that all buildings are accessible and connected via a network of pedestrian paths.
5. The following items will need to be submitted with or prior to submittal of the Conditional Use Permit. The items may be submitted together as part of a Master Plan document. All items are subject to City staff review and comment prior to the application being deemed complete.
 - Building elevations with detail call-outs
 - Wall detail
 - Sign program
 - Landscaping
6. Provide ten site plan exhibits with the Conditional Use Permit application.

7. The 2,500 sq. ft. office shows ten parking spaces on-site. This meets the parking requirements for non-medical office. Use of the building for medical office requires 13 parking stalls.
8. The trash enclosure associated with the office shall be located 15 feet from the north property line of the adjoining R-1-6 zoned parcel. Staff recommends relocating this trash enclosure next to the trash enclosure proposed for the Wendy's fast food restaurant, and placing more parking spaces in the former trash enclosure site.
9. Cross-Access agreement is required and shall be recorded with the final parcel map.
10. Comments for the previous Site Plan Review submittal for this site can be found with Site Plan Review No. 2010-135.

CITY GENERAL PLAN CONSISTENCY

Staff's initial finding is that the proposed project MAY NOT BE CONSISTENT with the City General Plan. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

Design Standards: The property will be assigned a Design District upon approval of a Change of Zone. Staff recommends that Design District "B" standards be considered for this property since it currently applies to other commercial designations on Dinuba Blvd. between Houston Avenue and Robin Avenue. The design standards for this district are as follows:

Design District: "B" [Section 17.30.170 of Visalia Municipal Code]

Maximum Building Height: 50 Feet

Minimum Setbacks:

	Building	Landscaping
➤ Front	15 Feet	15 Feet
➤ Side	0 Feet	5 Feet*
➤ Street side on corner lot	10 Feet	10 Feet
➤ Side abutting residential zone	15 Feet	5 Feet
➤ Rear	0 Feet	5 Feet*
➤ Rear abutting residential zone	20 Feet	5 Feet

*(Except where building is on property line)

Minimum Site Area: 5 acres

Parking:

1. Parking spaces are required in accordance with Zoning Ordinance Section 17.34.020 based on the types of land uses in the center.
 - For restaurants, provide 1 space per 100 square feet of gross floor area.
 - For retail, provide 1 space per 300 square feet of gross floor area.
 - For non-medical offices, provide 1 space per 250 square feet of gross floor area.
2. 30% of the required parking stalls may be compact and shall be evenly distributed in the lot (Zoning Ordinance Section 17.34.030.I).
3. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.34.040.D & 17.30.130.C).
4. A planter is required every other row. Said planter shall be 6 feet in width and shall contain trees on twenty (20) foot centers (Zoning Ordinance Section 17.30.130.C.6).
5. It is highly recommended that bicycle rack(s) be provided on site plan.
6. No parking shall be permitted in a required front/rear/side yard (Zoning Ordinance Section 17.34.030.F).
7. The parking lot shall be screened from view by a 3-foot tall solid wall or shrubs when located adjacent to a public street.

8. The project should provide preferential parking spaces for carpools and vanpools to decrease the number of single occupant vehicle work trips. The preferential treatment could include covered parking spaces or close-in parking spaces, or designated free parking, or a guaranteed space for the vehicle.

Fencing and Screening:

1. Provide screening for roof mounted equipment (Zoning Ordinance Section 17.30.130.F).
2. Provide second-story screening for all windows that may intrude into adjacent residential properties. Details and cross-sections will be required to be reviewed and approved prior to issuance of building permits (Zoning Ordinance Section 17.30.130.F).
3. Provide screened trash enclosure with solid screening gates (Zoning Ordinance Section 17.30.130.F).
4. Provide solid screening of all outdoor storage areas. Outdoor storage to be screened from public view with solid material (Zoning Ordinance Section 17.30.130.F).
5. Outdoor retail sales prohibited.
6. Cross Sections need to be provided for site Plan Review if there is greater than an 18-inch difference between the elevation of the subject site and the adjacent properties, and the sections would be required for the public hearing process also.
7. All outdoor storage areas are to be identified on the site plan and they are to be shown with screening (fencing). No materials may be stored above the storage area fence heights (Zoning Ordinance Section 17.30.130.F).
8. If there is an anticipated grade difference of more than 12-inches between this site and the adjacent sites, a cross section of the difference and the walls must be provided as a part of the Subdivision and/or CUP application package.
9. NOTE: The maximum height of block walls and fences is 7-feet in the appropriate areas; this height is measured on the tallest side of the fence. If the height difference is such that the fence on the inside of the project site is not of sufficient height, the fence height should be discussed with Planning Staff prior to the filing of applications to determine if an Exception to fence/wall height should also be submitted.

Landscaping:

1. Provide street trees at an average of 20-feet on center along street frontages. All trees to be 15-gallon minimum size (Zoning Ordinance Section 17.30.130.C).
2. All landscape areas to be protected with 6-inch concrete curbs (Zoning Ordinance Section 17.30.130.F).
3. All parking lots to be designed to provide a tree canopy to provide shade in the hot seasons and sunlight in the winter months.
4. An 80 sq. ft. minimum landscape well is required every 10 contiguous parking stalls (Zoning Ordinance Section 17.30.130.C).
5. Provide a detailed landscape and irrigation plan for review prior to issuance of building permits. Please review Zoning Ordinance section 17.30.130-C for current landscaping and irrigation requirements.
6. Locate existing oak trees on site and provide protection for all oak trees greater than 2" diameter (see Oak Tree Preservation Ordinance).
7. On September 30, 2009, the State Model Water Efficient Landscape Ordinance (MWELo) was finalized by the State Department of Water Resources to comply with AB 1881. AB 1881 along with the MWELo became effective on January 1, 2010. As of January 1, 2010, the State Model Water Efficient Landscape Ordinance became effective by adoption of a City urgency ordinance on December 21, 2009. The ordinance applies to projects installing 2,500 square feet or more of landscaping. It requires that landscaping and irrigation plans be certified by a qualified entity (i.e., Landscape Architect) as meeting the State water conservation requirements. The City's implementation of this new State law will be accomplished by self-certification of the final landscape and irrigation plans by a California

licensed landscape architect or other qualified entity with sections signed by appropriately licensed or certified persons as required by the ordinance.

Lighting:

1. All lighting is to be designed and installed so as to prevent any significant direct or indirect light or glare from falling upon any adjacent residential property. This will need to be demonstrated in the building plans and prior to final on the site.
2. Parking lot and drive aisle lighting adjacent to residential units or designated property should consider the use of 15-foot high light poles, with the light element to be completely recessed into the can. A reduction in the height of the light pole will assist in the reduction/elimination of direct and indirect light and glare which may adversely impact adjacent residential areas.
3. Building and security lights need to be shielded so that the light element is not visible from the adjacent residential properties, if any new lights are added or existing lights relocated.
4. NOTE: Failure to meet these lighting standards in the field will result in no occupancy for the building until the standards are met.
5. In no case shall more than 0.5 lumens be exceeded at any property line, and in cases where the adjacent residential unit is very close to the property line, 0.5 lumens may not be acceptable.

Noise: Must meet City noise standards as prescribed by the Noise Element.

Signage: All signs require a separate building permit.

The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments.

Signature  _____

SITE PLAN REVIEW COMMENTS

TRANSIT DIVISION

ITEM NO. 8 DATE: February 13, 2013

SITE PLAN NO.: SPR13031
MIXED USE CENTER
MISED USE COMMERCIAL CENTER WITH 6,500 SF
BUILIDNG IN PHASE 1 ON 9.8 ACRES (R-16 ZONED)
BRIDGECOURT HOMES
BRIDGECOURT DEVELOPMENT LLC


LOCATION: SEC DINUBA DLVD & RIGGIN AVE
APN: 091-010-040

NOTES

Please be advised that a new bus stop will need to be installed with ADA pad at the following location:

- ◆ On the South side of Riggin Ave, between 0' and 150' east of Dinuba Blvd.
- ◆ On the west side of Court St., between 240' and 390' south of Riggin Rd.

For more information, please contact the Transit Division at 713-4100.



Monty A. Cox
Transit Manager

City of Visalia
Police Department
303 S. Johnson St.
Visalia, Ca. 93292
(559) 713-4573

ITEM NO: 8 DATE: February 13, 2013
SITE PLAN NO: SPR13031
PROJECT TITLE: MIXED USE CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER WITH 6,500 SF BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED) BRIDGECOURT HOMES
APPLICANT: BRIDGECOURT DEVELOPMENT LLC
PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN(S): 091-010-040

Site Plan Review Comments

- No Comment at this time.
- Request opportunity to comment or make recommendations as to safety issues as plans are developed.
- Public Safety Impact fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001

Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.

- Not enough information provided. Please provide additional information pertaining to:

- Territorial Reinforcement: Define property lines (private/public space).

- Access Controlled / Restricted etc:

- Lighting Concerns:

- Landscaping Concerns:

- Traffic Concerns:

- Surveillance Issues:

- Line of Sight Issues:

- Other Concerns:



Visalia Police Department

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

February 13, 2013

ITEM NO: 8
SITE PLAN NO: SPR13031
PROJECT TITLE: MIXED USE CENTER
DESCRIPTION: MIXED USE COMMERCIAL CENTER WITH 6,500 SF BUILDING IN PHASE I ON 9.8 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN(S): 091-010-040

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- No Comments
- See Previous Site Plan Comments
- Install Street Light(s) per City Standards.
- Install Street Name Blades at Locations.
- Install Stop Signs at each exit location.
- Construct parking per City Standards PK-1 through PK-4. Include the appropriate handicap parking with all the pavement markings and signs.
- Construct drive approach per City Standards.
- Traffic Impact Study required.

Additional Comments:


Eric Bons



MEETING DATE 2/13/13
SITE PLAN NO. 13-032
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

- RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.
- During site plan design/policy concerns were identified, schedule a meeting with
- Planning Engineering prior to resubmittal plans for Site Plan Review.
- Solid Waste Parks and Recreation Fire Dept.

REVISE AND PROCEED (see below)

- A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.
- Submit plans for a building permit between the hours of 8:30 a.m. and 4:30 p.m., Monday through Friday.
- Your plans must be reviewed by:
- CITY COUNCIL REDEVELOPMENT
- PLANNING COMMISSION PARK/RECREATION
- HISTORIC PRESERVATION OTHER _____

ADDITIONAL COMMENTS Conditional Use Permit for parcels with no public right of way access.

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

Site Plan Review Committee



MEETING DATE 2/13/13
SITE PLAN NO. 13032
PARCEL MAP NO.
SUBDIVISION
LOT LINE ADJUSTMENT NO.

Enclosed for your review are the comments and decisions of the Site Plan Review committee. Please review all comments since they may impact your project.

- RESUBMIT** Major changes to your plans are required. Prior to accepting construction drawings for building permit, your project must return to the Site Plan Review Committee for review of the revised plans.
- During site plan design/policy concerns were identified, schedule a meeting with
- Planning Engineering prior to resubmittal plans for Site Plan Review.
- Solid Waste Parks and Recreation Fire Dept.
-

REVISE AND PROCEED (see below)

- A revised plan addressing the Committee comments and revisions must be submitted for Off-Agenda Review and approval prior to submitting for building permits or discretionary actions.
- Submit plans for a building permit between the hours of 8:30 a.m. and 4:30 p.m., Monday through Friday.

Your plans must be reviewed by:

- | | |
|--|--|
| <input type="checkbox"/> CITY COUNCIL | <input type="checkbox"/> REDEVELOPMENT |
| <input checked="" type="checkbox"/> PLANNING COMMISSION <i>CUP</i> | <input type="checkbox"/> PARK/RECREATION |
| <input type="checkbox"/> HISTORIC PRESERVATION | <input type="checkbox"/> OTHER _____ |
-

ADDITIONAL COMMENTS _____

If you have any questions or comments, please call Jason Huckleberry at (559) 713-4259.

Site Plan Review Committee

City of Visalia
Building: Site Plan
Review Comments

DATE: February 13, 2013
SITE PLAN NO: SPR13032
PROJECT TITLE: PARCEL MAP
DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES LP
PROF OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN(S): 091-010-040

NOTE:

These are general comments and DO NOT constitute a complete plan check for your specific project. Please refer to the applicable California Code & local ordinance for additional requirements.

- Business Tax certification is required. For information call (559)713-4326
- A building permit will be required For information call (559)713-4444
- Submit 3 sets of professionally prepared plans and 2 sets of calculations. (Small Tenant Improvements)
- Submit 5 sets of plans signed by an architect or engineer. Must comply with 2010 California Building Code Sec. 2308 for conventional light-frame construction or submit 2 sets of engineered calculations.
- Indicate abandoned wells, septic systems and excavations on construction plans.
- You are responsible to ensure compliance with the following checked items:
- Meet State and Federal requirements for accessibility for persons with disabilities.
- A path of travel, parking and common area must comply with requirements for access for persons with disabilities.
- All accessible units required to be adaptable for persons with disabilities.
- Maintain sound transmission control between units minimum of 50 STC.
- Maintain fire-resistive requirements at property lines.
- A demolition permit & deposit is required. For information call (559)713-4444
- Obtain required permits from San Joaquin Valley Air Pollution Board. For information call (559)230-6000
- Location of cashier must provide clear view of gas pump island
- Treatment connection charge to be assessed based on use. Credits _____
- Plans must be approved by the Tulare County Health Department. For information call (559)624-8011
- Project is located in flood zone _____ * Hazardous materials report.
- Arrange for an on-site inspection. For information call (559)713-4444 (Fee for inspection \$146.40)
- School Development fees. Commercial \$0.47 per square foot. Residential \$2.97 per square foot.
- Park Development fee \$ _____, per unit collected with building permits.
- Existing address must be changed to be consistent with city address p For information call (559)713-4320
- Acceptable as submitted

Special comments: No Comments

G. Ferrero
Signature

Date: 2-12-13

Site Plan Review Comments For:

Visalia Fire Department
Kurtis Brown, Assistant Fire Marshal
707 W Acequia
Visalia, CA 93291
559-713-4261 *office*
559-713-4808 *fax*

ITEM NO: 9

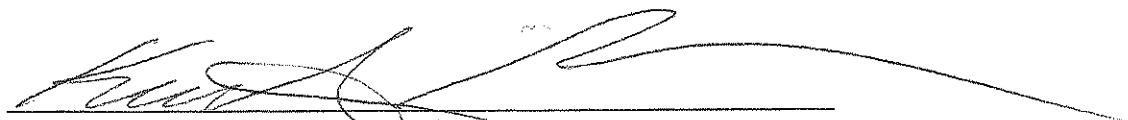
DATE: February 13, 2013

SITE PLAN NO: SPR13032
PROJECT TITLE: PARCEL MAP
DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES LP
PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN(S): 091-010-040

The following comments are applicable when checked:

- Refer to previous comments dated .
- More information is needed before a Site Plan Review can be conducted. Please submit plans with more detail.
- The Site Plan Review comments in this document are not all encompassing, but a general overview of the California Fire Code, and City of Visalia Municipal Codes. Additional requirements may come during the plan review process.
- No fire protection items required for parcel map or lot line adjustment; however, any future projects will be subject to fire protection requirements.
- Address numbers must be placed on the exterior of the building in such a position as to clearly and plainly visible from the street. Numbers will be at least six inches (6") high and shall be of a color to contrast with their background. If multiple addresses served by a common driveway, the range of numbers shall be posted at the roadway/driveway.
- No additional fire hydrants are required for this project; however, additional fire hydrants may be required for any future development.
- There is/are fire hydrants required for this project. (See marked plans for fire hydrant locations.)
- The turning radius for emergency fire apparatus is 20 feet inside radius and 43 feet outside radius. Ensure that the turns identified to you during site plan comply with the requirements. An option is a hammer-head constructed to City standards.
- An access road is required and shall be a minimum of 20 feet wide. The road shall be an all-weather driving surface accessible prior to and during construction.
- Buildings or portions of buildings or facilities exceeding 30 feet in height above the lowest level of fire department vehicle access shall be provided with an approved fire apparatus access roads capable of accommodating fire department aerial apparatus. Access roads shall have a minimum unobstructed width of 26 feet, exclusive of shoulders. Access routes shall be located within a minimum of 15 feet and maximum of 30 feet from the building, and shall be positioned parallel to one entire side of the building.

- A fire lane is required for this project. The location will be given to you during the site plan meeting.
- A Knox Box key lock system is required. Applications are available at the Fire Department Administrative Office. (Note: Knox boxes shall be ordered using an approved application that can be found at Fire Administration Office located at 707 W. Acequia Ave. Please allow adequate time for shipping and installation.)
- The security gates, if to be locked, shall be locked with a typical chain and lock that can be cut with a common bolt cutter, or the developer may opt to provide a Knox Box key lock system. Applications are available at the Fire Department Administrative Office.
- That portion of the building that is built upon a property line shall be constructed as to comply with Section 503.4 and Table 5-A of the California Building Code.
- Commercial dumpsters with 1.5 cubic yards or more shall not be stored or placed within 5 feet of combustible walls, openings, or a combustible roof eave line except when protected by a fire sprinkler system.
- If you handle hazardous material in amounts that exceed the exempt amounts listed on Table 3-D of the California Building Code, you are required to submit an emergency response plan to the Tulare County Health Department. Prior to the building final inspection, we will want a copy of the plan and any Material Safety Data Sheets.
- An automatic fire sprinkler system will be required for this building. A fire hydrant is required within 50 feet of the fire department connection. The fire hydrant, fire department connection and the PIV valve should be located together and minimum 25' from the building, if possible. The caps on the FDC shall be Knox locking caps.
- All hardware on exit doors shall comply with Chapter 10 of the California Fire Code. This includes all locks, latches, bolt locks, and panic and fire exit hardware.
- Provide Illuminated exit signs and emergency lighting through-out building.
- All Fire and Life Safety systems located within the building shall be maintained.
- An automatic fire extinguishing system for protection of the kitchen grease hood and ducts is required.
- Fire Department Impact Fee - \$1601.01 per acre.
- Fire Department Permit Fee - complete application during Building Department permit process.
- Special comments:


 Kurtis Brown, Assistant Fire Marshal

CITY OF VISALIA
SOLID WASTE DIVISION
336 N. BEN MADDOX
VISALIA CA. 93291
713 - 4500

COMMERCIAL BIN SERVICE

ITEM NO. 2 DATE: FEBRUARY 13, 2013
 SITE PLAN NO: SPR13032
 PROJECT TITLE: PARCEL MAP
 DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS
 AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED)
 APPLICANT: BRIDGECOURT HOMES LP
 PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
 LOCATION: SEC DINUBA BLVD & RIGGIN AVE
 APN(S): 091-010-040

- No comments.
- Same comments as as
- Revisions required prior to submitting final plans. See comments below.
- Resubmittal required. See comments below.
- Customer responsible for all cardboard and other bulky recyclables to be broken down
be fore disposing of in recycle containers.
- ALL refuse enclosures must be R-3 or R-4
- Customer must provide combination or keys for access to locked gates/bins
- Type of refuse service not indicated.
- Location of bin enclosure not acceptable. See comments below.
- Bin enclosure not to city standards double.
- Inadequate number of bins to provide sufficient service. See comments below.
- Drive approach too narrow for refuse trucks access. See comments below.
- Area not adequate for allowing refuse truck turning radius of :
Commercial (X) 50 ft. outside 36 ft. inside; Residential () 35 ft. outside, 20 ft. inside.
- Paved areas should be engineered to withstand a 55,000 lb. refuse truck.
- Bin enclosure gates are required
- Hammerhead turnaround must be built per city standards.
- Cul - de - sac must be built per city standards.
- Bin enclosures are for city refuse containers only. Grease drums or any other
items are not allowed to be stored inside bin enclosures.
- Area in front of refuse enclosure must be marked off indicating no parking
- Enclosure will have to be designed and located for a STAB service (DIRECT ACCESS)
- Customer will be required to roll container out to curb for service.
- Must be a concrete slab in front of enclosure as per city standards

[]

The width of the enclosure by ten(10) feet, minimum of six(6) inches in depth.

Roll off compactor's must have a clearance of 3 feet from any wall on both sides and there must be a minimum of 53 feet clearance in front of the compactor to allow the truck enough room to provide service.

[]

Bin enclosure gates must open 180 degrees and also hinges must be mounted in front of post see page 2 for instructions

Javier Hernandez, Solid Waste Front Load Supervisor 713-4338

[]

QUALITY ASSURANCE DIVISION SITE PLAN REVIEW COMMENTS

ITEM NO: 9 DATE: February 13, 2013
 SITE PLAN NO: SPR13032
 PROJECT TITLE: PARCEL MAP
 DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS
 AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED)
 APPLICANT: BRIDGECOURT HOMES LP
 PROP OWNER: BRIDGECOURT DEVELOPMENT LLC
 LOCATION: SEC DINUBA BLVD & RIGGIN AVE
 APN(S): 091-010-040

YOU ARE REQUIRED TO COMPLY WITH THE CITY OF VISALIA WASTEWATER ORDINANCE 13.08 RELATIVE TO CONNECTION TO THE SEWER, PAYMENT OF CONNECTION FEES AND MONTHLY SEWER USER CHARGES. THE ORDINANCE ALSO RESTRICTS THE DISCHARGE OF CERTAIN NON-DOMESTIC WASTES INTO THE SANITARY SEWER SYSTEM.

YOUR PROJECT IS ALSO SUBJECT TO THE FOLLOWING REQUIREMENTS:

- WASTEWATER DISCHARGE PERMIT APPLICATION
- SAND AND GREASE INTERCEPTOR - 3 COMPARTMENT _____
- GREASE INTERCEPTOR min. 1000 GAL
- GARBAGE GRINDER - ¼ HP. MAXIMUM _____
- SUBMISSION OF A DRY PROCESS DECLARATION _____
- NO SINGLE PASS COOLING WATER IS PERMITTED _____
- OTHER _____
- SITE PLAN REVIEWED - NO COMMENTS

CALL THE QUALITY ASSURANCE DIVISION AT (559) 713-4529 IF YOU HAVE ANY QUESTIONS.

CITY OF VISALIA
 PUBLIC WORKS DEPARTMENT
 QUALITY ASSURANCE DIVISION
 7579 AVENUE 288
 VISALIA, CA 93277



 AUTHORIZED SIGNATURE

2-13-13

**SUBDIVISION & PARCEL MAP
REQUIREMENTS
ENGINEERING DIVISION**

- Jason Huckleberry 713-4259
- Ken McSheehy 713-4447
- Adrian Rubalcaba 713-4164

ITEM NO: 9 DATE: FEBRUARY 13, 2013

SITE PLAN NO.: 13-032
PROJECT TITLE: PARCEL MAP
DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED)
APPLICANT: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT DEVELOPMENT LLC
LOCATION: SEC DINUBA BLVD & RIGGIN AVE
APN: 091-010-040

SITE PLAN REVIEW COMMENTS

- REQUIREMENTS (Indicated by checked boxes)
- Submit improvements plans detailing all proposed work; Subdivision Agreement will detail fees & bonding requirements
- Bonds, certificate of insurance, cash payment of fees/inspection, and approved map & plan required prior to approval of Final Map.
- The Final Map & Improvements shall conform to the Subdivision Map Act, the City's Subdivision Ordinance and Standard Improvements.
- A preconstruction conference is required prior to the start of any construction.
- Right-of-way dedication required. A title report is required for verification of ownership. by map by deed
- City Encroachment Permit Required which shall include an approved traffic control plan.
- CalTrans Encroachment Permit Required. CalTrans comments required prior to tentative parcel map approval. CalTrans contacts: David Deel (planning) 488-4088
- Landscape & Lighting District/Home Owners Association required prior to approval of Final Map. Landscape & Lighting District will maintain common area landscaping, street lights, street trees and local streets as applicable. Submit completed Landscape and Lighting District application and filing fee a min. of 75 days before approval of Final Map. Contact Doug Damko, 713-4268, 315 E. Acequia Ave.
- Landscape & irrigation improvement plans to be submitted for each phase. Landscape plans will need to comply with the City's street tree ordinance. The locations of street trees near intersections will need to comply with Plate SD-1 of the City improvement standards. A street tree and landscape master plan for all phases of the subdivision will need to be submitted with the initial phase to assist City staff in the formation of the landscape and lighting assessment district.
- Dedicate landscape lots to the City that are to be maintained by the Landscape & Lighting District.
- Northeast Specific Plan Area: Application for annexation into Northeast District required 75 days prior to Final Map approval.
- Written comments required from ditch company. Contacts: James Silva 747-1177 for Modoc, Persian, Watson, Oakes, Flemming, Evans Ditch and Peoples Ditches; Paul Hendrix 686-3425 for Tulare Irrigation Canal, Packwood and Cameron Creeks; Bruce George 747-5601 for Mill Creek and St. John's River.
- Final Map & Improvements shall conform to the City's Waterways Policy. Access required on ditch bank, 12' minimum. Provide wide riparian dedication from top of bank.
- Sanitary Sewer master plan for the entire development shall be submitted for approval prior to approval of any portion of the system. The sewer system will need to be extended to the boundaries of the development where future connection and extension is anticipated. The sewer system will need to be sized to serve any future developments that are anticipated to connect to the system.
- Grading & Drainage plan required. If the project is phased, then a master plan is required for the entire project area that shall include pipe network sizing and grades and street grades. Prepared by registered civil engineer or project architect. All elevations shall be based on the City's benchmark network. Storm run-off from the project shall be handled as follows: a) directed to the City's existing storm drainage system; b) directed to a permanent on-site basin; or c) directed to a temporary on-site basin is required until a connection with adequate capacity is available to the City's storm drainage system. On-site

basin: : maximum side slopes; perimeter fencing required, provide access ramp to bottom for maintenance.

- Show Oak trees with drip lines and adjacent grade elevations. Protect Oak trees during construction in accordance with City requirements. A permit is required to remove oak trees. The City will evaluate Oak trees with removal permit applications. Oak tree evaluations by a certified arborist are required to be submitted to the City in conjunction with the tentative map application. A pre-construction conference is required. Contact: David Pendergraft, City Arborist, 713-4295
- Show adjacent property grade elevations on improvement plans. A retaining wall will be required for grade differences greater than 0.5 feet at the property line.
- Relocate existing utility poles and/or facilities.
- Underground all existing overhead utilities within the project limits. Existing overhead electrical lines over 50kV shall be exempt from undergrounding.
- Provide "R" value tests: each at
- Traffic indexes per city standards: **Court St. T.I. of 8.0**
- All public streets within the project limits and across the project frontage shall be improved to their full width, subject to available right of way, in accordance with City policies, standards and specifications.
- All lots shall have separate drive approaches constructed to City Standards.
- Install street striping as required by the City Engineer.
- Install sidewalk: ft. wide, with ft. wide parkway on **Refer SPR 13-031**
- Cluster mailbox supports required at 1 per 2 lots, or use postal unit (contact the Postmaster at 732-8073).
- Subject to existing Reimbursement Agreement to reimburse prior developer:
- Abandon existing wells per City of Visalia Code. A building permit is required.
- Remove existing irrigation lines & dispose off-site. Remove existing leach fields and septic tanks.
- Fugitive dust will be controlled in accordance with the applicable rules of San Joaquin Valley Air District's Regulation VIII. Copies of any required permits will be provided to the City.
- If the project requires discretionary approval from the City, it may be subject to the San Joaquin Valley Air District's Rule 9510 Indirect Source Review per the rule's applicability criteria. A copy of the approved AIA application will be provided to the City.
- If the project meets the one-acre of disturbance criteria of the State's Storm Water Program, then coverage under General Permit Order 2009-0009-DWQ is required and a Storm Water Pollution Prevention Plan (SWPPP) is needed. A copy of the approved permit and the SWPPP will be provided to the City.
- Comply with prior comments Resubmit with additional information Redesign required

Additional Comments:

1. Public street improvements, Court St, shall be submitted with filing of Tentative Parcel Map. All bonds, cash payments, insurance, disclosures, and agreements shall be completed and submitted to the City prior to improvement plan approval and filing for recordation of the Tentative Parcel Map.

2. In accordance with the City's current Transportation Impact Fee program, a portion of Court Street improvements is available for reimbursement through fee credits towards future Commercial development. The City and Developer shall enter into an agreement as part of Tentative Map improvements.

SITE PLAN REVIEW COMMENTS

Brandon Smith, Planning Division (559) 713-4636

Date: February 13, 2012

SITE PLAN NO: 13-032
PROJECT TITLE: PARCEL MAP
DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS AND 2
REMAINDERS ON 9.8 ACRES (R16 ZONED)
APPLICANT TITLE: BRIDGECOURT HOMES
PROP. OWNER: BRIDGECOURT DEVELOPMENT INC
LOCATION: SEC CORNER OF DINUBA AND RIGGIN
APN TITLE: 091-010-040

General Plan: RLD – Residential Low Density
Existing Zoning: R-1-6 – Single-Family Residential 6,000 sq. ft. min. site area

Planning Division Recommendation:

- Revise and Proceed
 Resubmit

Project Requirements

- Tentative Parcel Map
- CUP for parcels with no public right-of-way access

PROJECT SPECIFIC INFORMATION: 02/13/2013

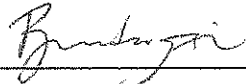
1. A CUP is required for this parcel map due to the creation of parcels that do not have access to a public right-of-way.
2. Lot "A" will need to provide shared access for the benefit of Parcels 1, 2, and 3.
3. A Lot Line Adjustment may be needed to re-align the southern boundary of Parcel 3 with the existing fence belonging to the adjacent residence.

CITY GENERAL PLAN CONSISTENCY

Staff initial finding is that the proposed site plan IS CONSISTENT with the City General Plan if a General Plan Amendment and Change of Zone toward a commercial designation is approved. Because this project requires discretionary approval by the City Council and/or Planning Commission the final determination of consistency will be made by the Planning Commission and/or City Council.

The comments found on this document pertain to the site plan submitted for review on the above referenced date. Any changes made to the plan submitted must be submitted for additional review.

NOTE: Staff recommendations contained in this document are not to be considered support for a particular action or project unless otherwise stated in the comments.

Signature 

City of Visalia
Police Department
303 S. Johnson St.
Visalia, Ca. 93292
(559) 713-4573

ITEM NO: 9
SITE PLAN NO: SPR13032
PROJECT TITLE: PARCEL MAP
DESCRIPTION: TENTATIVE PARCEL MAP TO CREATE 3 PARCELS AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED) BRIDGECOURT HOMES LP
APPLICANT: BRIDGECOURT DEVELOPMENT LLC
PROP OWNER: SEC DINUBA BLVD & RIGGIN AVE
LOCATION: 091-010-040
APN(S):

Site Plan Review Comments

- No Comment at this time.
- Request opportunity to comment or make recommendations as to safety issues as plans are developed.

Public Safety Impact fee:
Ordinance No. 2001-11 Chapter 16.48 of Title 16 of the Visalia Municipal Code
Effective date - August 17, 2001

Impact fees shall be imposed by the City pursuant to this Ordinance as a condition of or in conjunction with the approval of a development project. "New Development or Development Project" means any new building, structure or improvement of any parcels of land, upon which no like building, structure of improvement previously existed. *Refer to Engineering Site Plan comments for fee estimation.

- Not enough information provided. Please provide additional information pertaining to:

- Territorial Reinforcement: Define property lines (private/public space).

- Access Controlled / Restricted etc:

- Lighting Concerns:

- Landscaping Concerns:

- Traffic Concerns:

- Surveillance Issues:

- Line of Sight Issues:

- Other Concerns:

Visalia Police Department

SITE PLAN REVIEW COMMENTS

CITY OF VISALIA TRAFFIC SAFETY DIVISION

February 13, 2013

ITEM NO: 9	
SITE PLAN NO:	SPR13032
PROJECT TITLE:	PARCEL MAP
DESCRIPTION:	TENTATIVE PARCEL MAP TO CREATE 3 PARCELS AND 2 REMAINDERS ON 9.8 ACRES (R16 ZONED)
APPLICANT:	BRIDGECOURT HOMES LP
PROP. OWNER:	BRIDGECOURT DEVELOPMENT LLC
LOCATION:	SEC DINUBA BLVD & RIGGIN AVE
APN(S):	091-010-040

THE TRAFFIC DIVISION WILL PROHIBIT ON-STREET PARKING AS DEEMED NECESSARY

- No Comments
- See Previous Site Plan Comments
- Install Street Light(s) per City Standards.
- Install Street Name Blades at Locations.
- Install Stop Signs at each exit location.
- Construct parking per City Standards PK-1 through PK-4. Include the appropriate handicap parking with all the pavement markings and signs.
- Construct drive approach per City Standards.
- Traffic Impact Study required.

Additional Comments:


Eric Bons

SITE PLAN REVIEW COMMENTS

TRANSIT DIVISION

ITEM NO. 89 DATE: February 13, 2013

SITE PLAN NO.: SPR13031
MIXED USE CENTER
MISED USE COMMERCIAL CENTER WITH 6,500 SF
BUILDING IN PHASE 1 ON 9.8 ACRES (R-16 ZONED)
BRIDGECOURT HOMES
BRIDGECOURT DEVELOPMENT LLC

LOCATION: SEC DINUBA DLVD & RIGGIN AVE
APN: 091-010-040

NOTES

Please be advised that a new bus stop will need to be installed with ADA pad at the following location:

- ◆ On the South side of Riggins Ave, between 0' and 150' east of Dinuba Blvd.
- ◆ On the west side of Court St., between 240' and 390' south of Riggins Rd.

For more information, please contact the Transit Division at 713-4100.



Monty A. Cox
Transit Manager

Rhonda Haynes

From: Tracy Robertshaw
Sent: Monday, July 29, 2013 8:38 AM
To: Doug Elliott; Steve Luna; Steven Rocha; Tommy Contreras
Cc: Rhonda Haynes
Subject: FW: 1047 N Manzanita- City property break in.

Hi Everyone:

Check to see which of these houses are in your assigned areas so that you can keep an eye on them when you are in the area.

Thank you,

Tracy Robertshaw
Code Enforcement Officer
City of Visalia
(559)713-4187

*“Be more concerned with your character than your reputation,
because your character is what you really are,
while your reputation is merely what others think you are.”*

John Wooden

From: Rhonda Haynes
Sent: Monday, July 29, 2013 8:33 AM
To: Tracy Robertshaw; Steven Phillips
Cc: Chris Young; Christopher Tavarez; Charlotte Dunn; Jeff Dearborn
Subject: 1047 N Manzanita- City property break in.

I am making you aware of a City purchased property, where there was a break in. I wanted to make you are aware, then, if you or staff, are in the area, can keep an eye on it. The sliding door is being replaced since the old slider was reversed, making it easy to enter. This is the second entry. The first, there was no damage, only leaving windows and doors open. This time, they punched holes in the wall above the fire place and left windows open. (minor cost to repair) Jeff Dearborn is the project manager on the city owned properties. In addition to this property, the remaining homes we own, preparing for rehabilitation and resell are:

3650 S Chatham	sold- awaiting close of escrow
1820 S West	sold- awaiting close of escrow
2728 E Vine	sold- awaiting close of escrow
2817 N Willis	rehab near completion- placing on the market for resell
340 W Wren	rehab near completion- placing on the market for resell
2406 W Prospect	preparing for rehab
2301 W Prospect	preparing for rehab
1027 N Manzanita-	mold remediation to begin in the next two weeks

If you have any questions, please contact me. I appreciate your help, as always, in keeping an eye on these properties.

Rhonda Haynes

City of Visalia, Community Development Department

Housing Specialist

Ph. (559) 713-4460

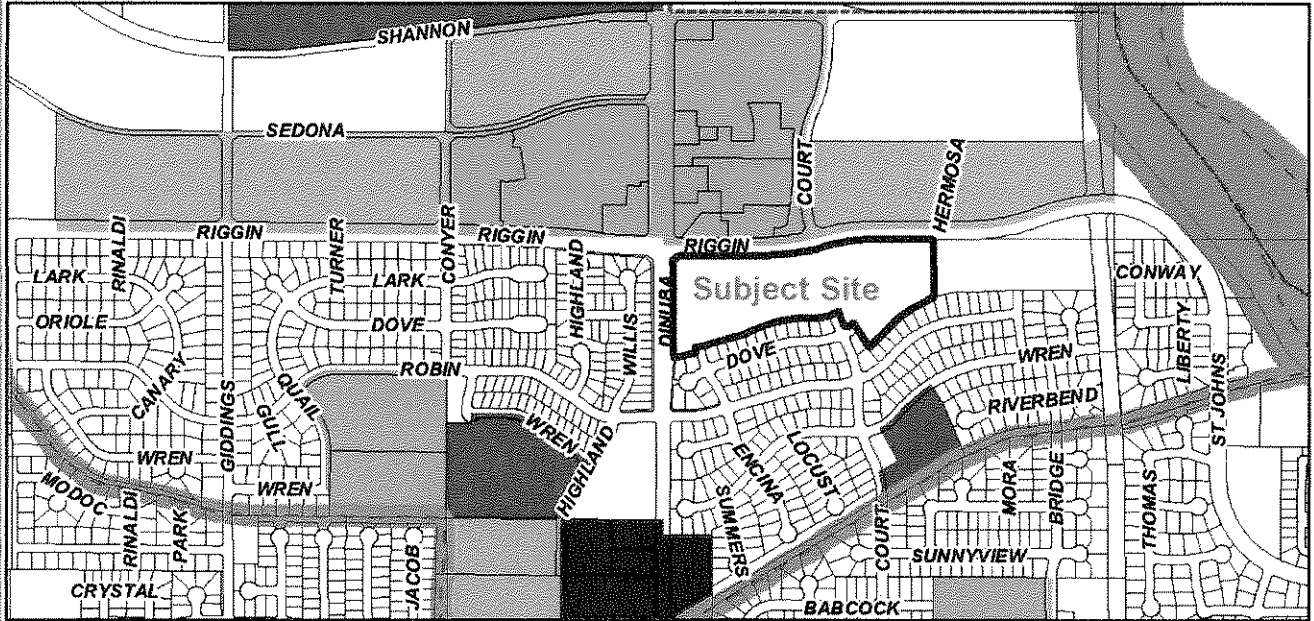
Fx. (559) 713-4814

Email: rhaynes@ci.visalia.ca.us

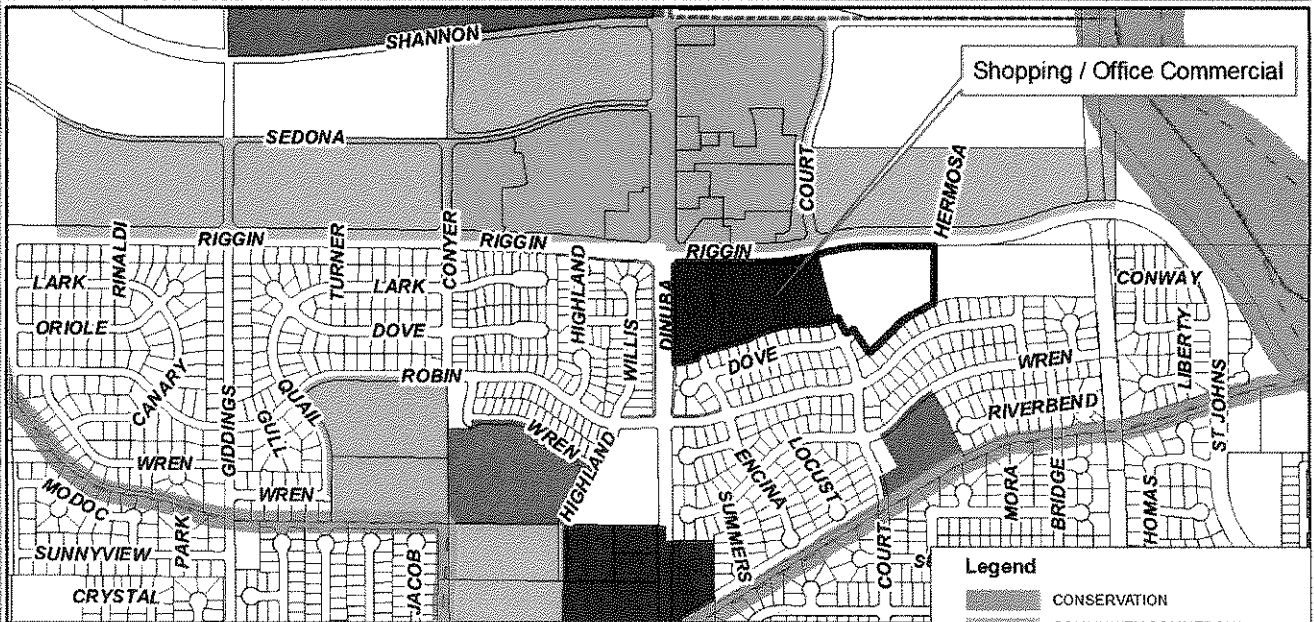
**General Plan Amendment No. 2011-14, Change of Zone No. 2011-15,
Tentative Parcel Map No. 2013-01, Conditional Use Permit No. 2013-25**

Location: Southeast corner of Dinuba Boulevard and Riggins Avenue. (APN: 091-010-040)

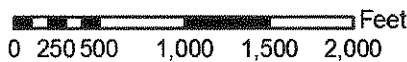
EXISTING GENERAL PLAN



PROPOSED GENERAL PLAN



General Plan Land Use Map

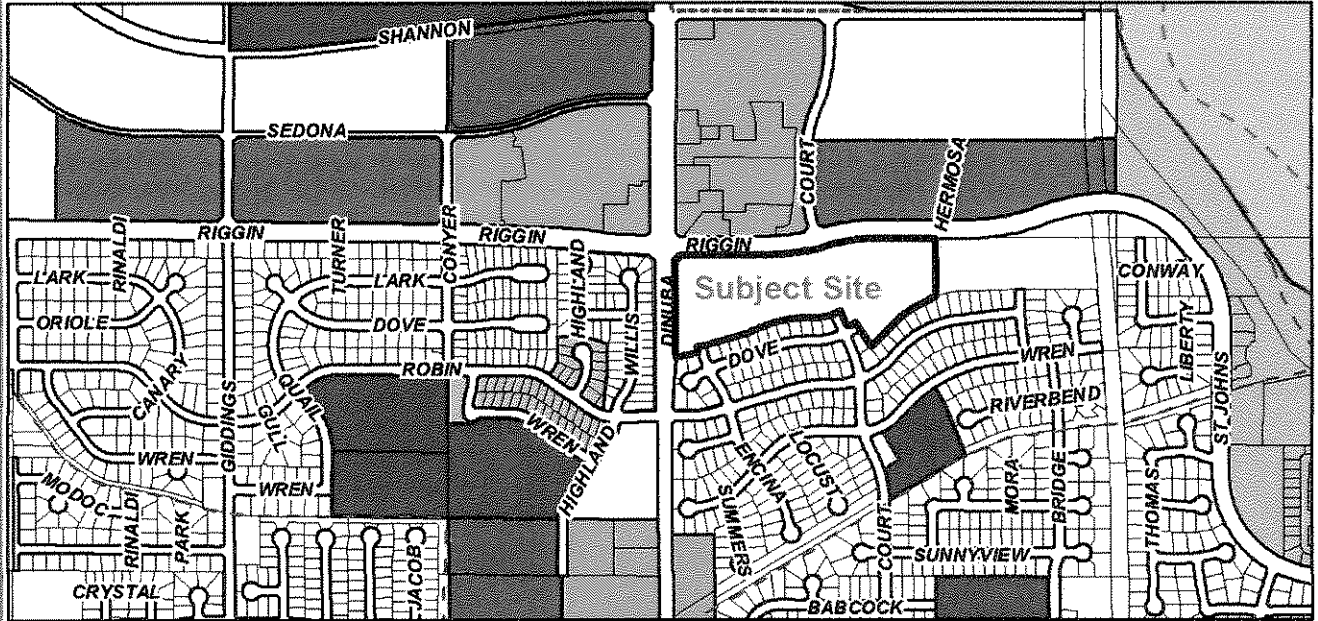


- Legend**
- CONSERVATION
 - COMMUNITY COMMERCIAL
 - SHOPPING / OFFICE COMMERCIAL
 - PARK
 - PUBLIC INSTITUTIONAL
 - RESIDENTIAL LOW DENSITY
 - RESIDENTIAL MEDIUM DENSITY
 - URBAN RESERVE

**General Plan Amendment No. 2011-14, Change of Zone No. 2011-15,
Tentative Parcel Map No. 2013-01, Conditional Use Permit No. 2013-25**

Location: Southeast corner of Dinuba Boulevard and Riggan Avenue. (APN: 091-010-040)








EXISTING ZONING DESIGNATION



PROPOSED ZONING DESIGNATION

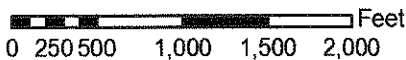


Legend

-  COMMUNITY COMMERCIAL
-  SHOPPING / OFFICE COMMERCIAL
-  QUASI-PUBLIC
-  R-1-4.5 (SINGLE/MULTI-FAMILY RESIDENTIAL)
-  R-1-6 (SINGLE-FAMILY RESIDENTIAL)
-  R-M-2 (MULTI-FAMILY RESIDENTIAL)
-  COUNTY

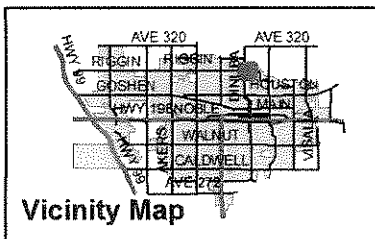


Zoning Map



**General Plan Amendment No. 2011-14, Change of Zone No. 2011-15,
Tentative Parcel Map No. 2013-01, Conditional Use Permit No. 2013-25**

Location: Southeast corner of Dinuba Boulevard and Riggini Avenue. (APN: 091-010-040)



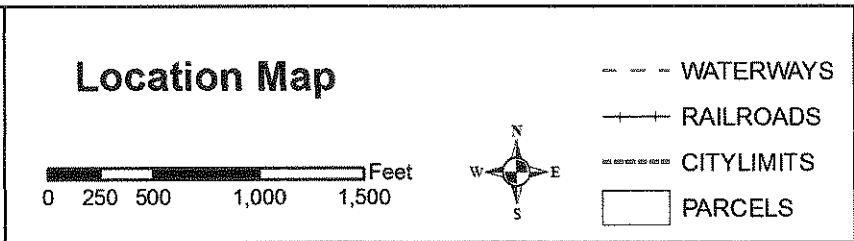
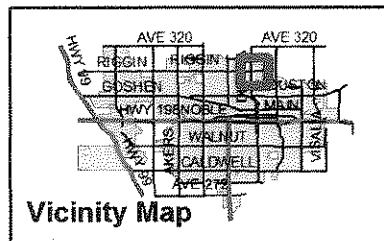
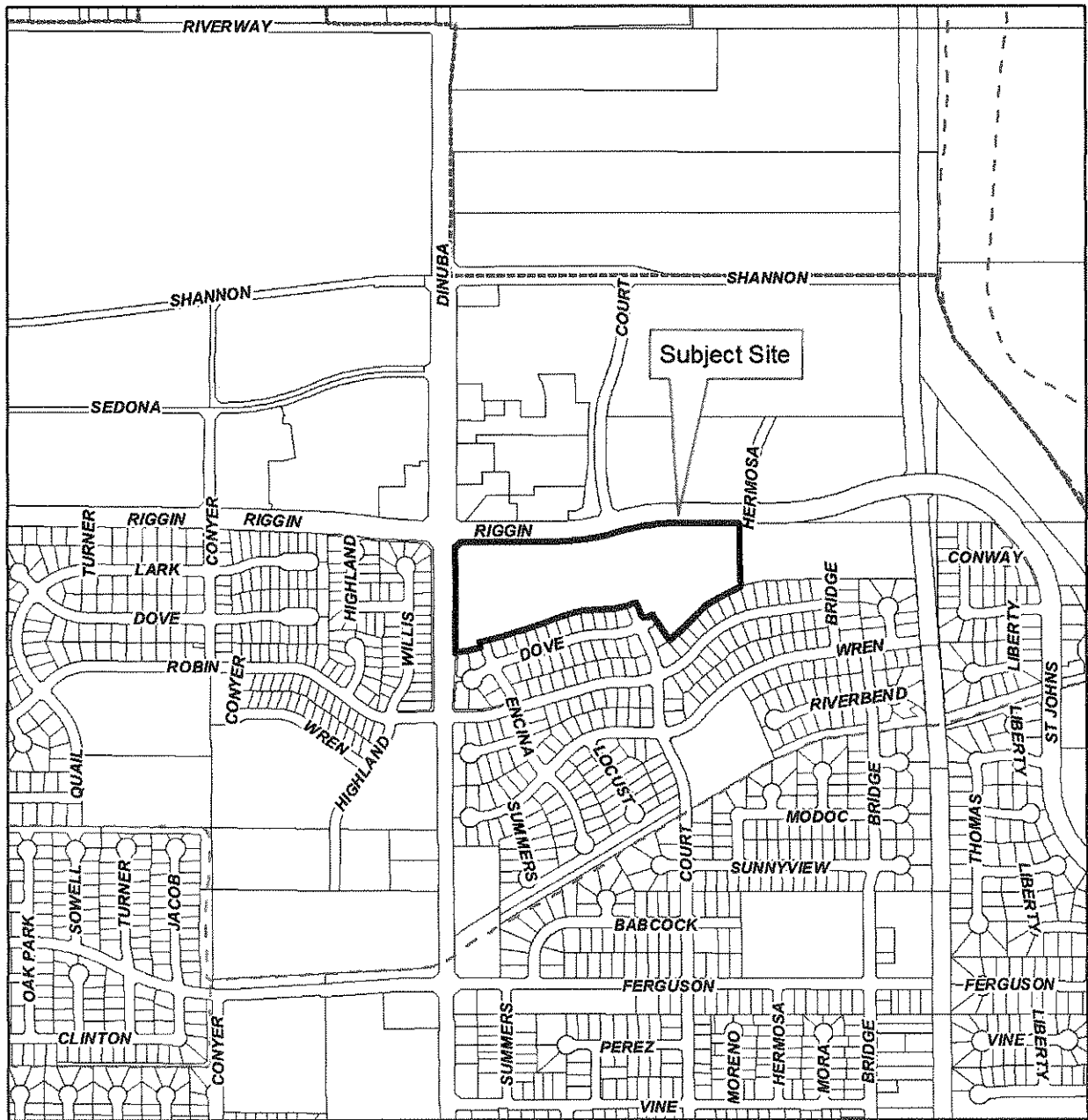
Aerial Photo
Photo Taken March 2010

0 100 200 400 600 800 Feet

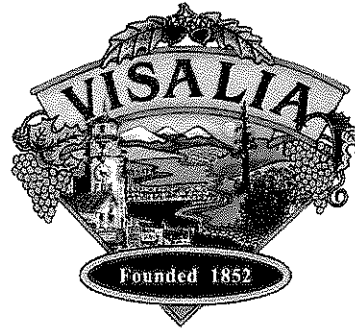
A compass rose with a central circle and four points labeled N (North), S (South), E (East), and W (West).

General Plan Amendment No. 2011-14, Change of Zone No. 2011-15, Tentative Parcel Map No. 2013-01, Conditional Use Permit No. 2013-25

Location: Southeast corner of Dinuba Boulevard and Riggin Avenue. (APN: 091-010-040)



City of Visalia



To: Visalia Planning Commission

From: Josh McDonnell, City Planner
(713-4364)

Paul Bernal, Senior Planner
(713-4025)

Date: August 12, 2013

Re: Work Program to Amend the Zoning Ordinance Regulating Telecommunication Tower Development Standards and Locations.

Background

During the February 2011 Joint Worksession, the City Council and Planning Commission discussed the merits of developing an ordinance to address telecommunication tower (i.e., cell tower) development standards and locational options. This report describes the manner in which current cell tower applications are currently processed and outlines options for amending the Zoning Ordinance to establish new procedures and standards for future cell tower applications.

Staff requests the Planning Commission review the options outlined in this memorandum and provide additional comments or concerns regarding staff's recommendation to prepare a zone text amendment to address cell tower design and location criteria.

Current Regulations for "Telecommunication Towers"

Zone Locations

Staff classifies "telecommunication towers" (cell towers) as "communication equipment buildings" pursuant to Line Item No. 67 of the Zoning Matrix. Table 1 outlines the Commercial, Office and Industrial zones which a tower is either a Permitted or Conditional Permitted use using this determination

Cell towers are either permitted or conditionally permitted in a number of the City's zoning designations. Table 1 depicts these zone districts:

Zone District	Allowed
Residential	Yes
Agricultural	CUP required
Rural Residential	CUP required
Single-Family Residential	CUP required
Multi-Family Residential	CUP required
Commercial/Office*	Yes
CC – Convenience Commercial	CUP required
CN – Neighborhood Commercial	CUP required
CSO – Shopping / Office Commercial	CUP required
CCM – Community Commercial	Permitted

CR – Regional Retail Commercial	Not allowed
CDT – Central Business District	CUP required
CH – Highway Commercial	Not allowed
CS – Service Commercial	Permitted
OG – Office Garden	Not allowed
PA – Professional/Administrative	CUP required
BRP – Business Research Park	CUP required
OC – Office Conversion	Not allowed
Other*	Yes
IL/IH – Industrial Zones	Permitted
Quasi-Public	CUP required

*Staff classifies cell phone towers as “communication equipment buildings” pursuant to Line No. 67 of the Zoning Matrix

Current Height Regulations

The height of telecommunication towers is determined by both the zone and design district for which a proposed structure is located. Section 17.32.020 (Special Provisions) of the Zoning Ordinance provides an exception to the height restrictions for towers, spires, cupolas, chimneys, etc. This special provision allows telecommunication towers to be erected to a height of not more than 25-feet above the height limit prescribed by the regulations for that zone in which the site/structure is located. For example, Design District “A” has a maximum building height of 50 feet. A telecommunication tower in Design District “A” would be allowed to be constructed to a maximum height of 75 feet.

Setbacks

The setbacks that regulate the location of structures are established by the Design District for commercial, office and industrial zone properties. Staff, on prior cell tower submittals, has requested the approval of conditions that establish a “separation area” from the cell tower location to residential zone properties. This separation “setback” area is determined by the height of the tower. As an example, a 70-foot tower would require a 70-foot setback from property zoned and/or planned residential.

With these issues in mind, staff will analyze current ordinance requirements and make recommendations to change the ordinance by citing appropriate zone districts, establishing height requirements, and correcting zoning inconsistencies for cell towers that have the potential of creating land use conflicts when locating these types of facilities in the community.

City Council/Planning Commission Feedback

At the February 2011 Worksession, the Council and Commission discussed the following specific telecommunication tower related concerns:

- A preference for having telecommunication towers constructed on City-owned properties
- Prohibiting telecommunication towers on residential zoned property
- Requiring stealth technologies when feasible/necessary

- Requiring the removal of the telecommunication tower and equipment when no longer in use

Options for Addressing City Council / Planning Commission Feedback

Zone Text Amendment Process – Recommended

Staff could prepare a Zone Text Amendment to address telecommunication towers. This would entail amending the portions of the Zoning Ordinance that currently regulate telecommunication tower development standards and zone locations. The text amendment process is not as cumbersome or resource intensive as the preparation of an entire Telecommunication Ordinance. The text amendment will serve to better clarify the City's current vision for the location and design criteria for new cell phone towers. The amendment will be restricted to those items of concern noted by the City Council and Planning Commission at their worksession specifically:

1. Specify "Telecommunication Towers" as its own line item in the Zoning Matrix;
2. Prohibit "Telecommunication Towers" in all residential zones;
3. Establish "Telecommunication Towers" as either "Permitted" or "Conditionally Permitted" in certain commercial/office zones.
4. Increase public disclosure and input in the entitlement process by requiring public hearing by the Planning Commission for all "Telecommunication Towers" in certain commercial/office zone or when in close proximity to residential planned/zoned property;
5. Correct minor inconsistencies among the various Zoning Code provisions regarding Communication Equipment Buildings; and,
6. Set minimum physical siting requirements for telecommunication towers, including: stealth technologies, location criteria, and substantial setbacks for all zoning and design districts.
7. Work within the limitations of the 1996 Telecommunications Act.

The Zone Text Amendment process is less comprehensive than a preparation of a full Telecommunication Ordinance and would include:

- Preparing the revisions to the existing text;
- Circulating the revised text to interested parties, including the business community and cellular phone carriers and revise as appropriate;
- Hold adoption hearings with the Planning Commission and City Council.

New Ordinance

Staff could prepare an entire new section of the Zoning Ordinance to codify a “stand-alone” wireless telecommunication Ordinance. This process would be resource intensive and would include:

1. Establishing a citizens advisory committee, including members from both the City Council and Planning Commission, local business community, and the cell phone carrier industry;
2. Review the existing cell tower approval process and existing ordinances from other cities;
3. Formulating development and geographic location standards;
4. Community outreach; and,
5. Adoption hearings before the Planning Commission and City Council.

In addition to the items outlined above, the new ordinance would establish terminology, zoning locations, design standards (height, setbacks, stealth technologies, etc.) spacing criteria, co-location considerations, submittal requirements, and any additional requirements deemed necessary by the Planning Commission and City Council.

1996 Telecommunications Act

Both the amendment process or a new ordinance will work within the limitations imposed by the 1996 Telecommunications Act. These limitations include:

- May not prohibit or have the effect of prohibiting the provision of personal wireless services (i.e., telecommunication equipment).
- May not discriminate among functionally equivalent services or service providers.
- Prohibit or inhibit any request to construct or modify personal wireless service facilities beyond a reasonable time period of time after the request is made.
- Regulate these facilities on the basis of environmental effects such as health risk of radio frequency emissions if the proposed facility complies with FCC guidelines.

Next Steps

Staff will present this information, as well as additional information provided by the Planning Commission to the City Council as a Worksession item at their August 19, 2013 meeting. Based on further direction/information from the Council, staff will proceed with preparing the zone text amendment with an anticipation of scheduling the zone text amendment for public hearing in September 2013.